

TO: HONORABLE CHAIRMAN AND PLANNING COMMISSION

FROM: ED GALLAGHER, COMMUNITY DEVELOPMENT DIRECTOR

SUBJECT: PLANNED DEVELOPMENT 13-006 & CONDITIONAL USE PERMIT 13-008
(MULLAHEY CHRYSLER)

DATE: JANUARY 28, 2014

Needs: For the Planning Commission to consider the following applications filed by Dennis Flynn Architects on behalf of Tim and Mike Mullahey:

- **PD 13-006:** request to establish a new car dealership with automotive repair. Also requested is the ability to place the dirt that will be removed to construct the dealership on to adjacent lots.
- **CUP 13-008:** request to construct 40-foot tall highway oriented pole sign.

Facts:

1. The project is located on the northeast corner of Golden Hill Road and Tractor Street. (see Vicinity Map, Attachment 1).
2. The General Plan designation is Commercial Service (CS). The current zoning designation is C3 (Commercial – Light Industrial) within Sub Area E of the Borkey Area Specific Plan (Specific Plan).
3. New car dealerships are permitted in the C3/CS designated area, and Sub Area E of the Specific Plan anticipates commercial and industrial land uses.
4. The 29,907 square foot building is proposed to be built on Parcels 65-68, which add up to 3.6 acres (see APN map, Attachment 2). Approximately 5,000 cubic yards of dirt will need to be removed from the site for the construction of the dealership. It is being requested that the dirt be spread over parcels that the Mullaheys own on the south side of Tractor Street (Parcels 71 & 74). There is no development proposed on the southerly lots at this time.
5. The applicant is requesting the approval of CUP 13-008 to allow for the installation of a 40-foot tall highway oriented sign. Currently, the project site is not within the area that allows highway oriented signs. The City is processing Code Amendment 13-001, which would expand the boundary where a highway oriented sign could be placed, to include the Mullahey parcels. Approval of the CUP for the sign will be subject to the City Council adopting the sign code amendment.

6. The DRC reviewed the project on December 16, 2013. The DRC was generally in favor of the proposed building, with the revisions that were made to the west elevation. The DRC did indicate that the Planning Commission may discuss whether additional architectural elements are necessary on the west elevation. The DRC recommended approval of the code amendment to expand the highway oriented sign district and the installation of the 40-foot tall highway oriented sign for the Mullahey project.
7. Pursuant to the Statutes and Guidelines of the California Environmental Quality Act (CEQA) and the City's Procedures for Implementing CEQA, an Initial Study and Negative Declaration (ND) was prepared and circulated for public review and comment. Based on the information and analysis contained in the Initial Study (and comments and responses thereto), a determination has been made that the project may be approved with a Negative Declaration.

**Analysis and
Conclusion:**

As mentioned above, new car dealerships are permitted in the C3 zone, and is a use that would be consistent with the anticipated uses for the Specific Plan (Sub Area E). The Paso Robles Ford dealership is located near this area and is also in Sub Area E.

Architecture:

City Staff met with the applicants early in the development review process, prior to the project being submitted to the City. There was discussion regarding site planning and storm water requirements, with the main focus of the discussions being focused on the architecture of the building.

The building is oriented to the south, with the front elevation of the building facing Tractor Street. The west elevation of the building (street side) is oriented to Golden Hill Road. Staff requested that the architect provide additional architectural elements to the west elevation since it is very prominent along Golden Hill Road.

The plans that have been provided to the Planning Commission have been revised from the original to improve the west elevation by adding a stucco parapet along the entire length of the building, replacing the metal siding with a stucco finish, and extending the use of glass around the corner of the building. City Staff requested that a taller arch element, similar to the entry element be added to the west elevation. That request was reviewed with the Chrysler Corporation and not accepted.

With the architectural additions described above, along with the installation of the decorative wrought iron fencing and the 10-foot of landscape planter along the parking lot on Golden Hill Road, the DRC was satisfied with the building architecture as proposed.

Highway Oriented Sign:

Along with the project, the applicants are requesting the City's approval of CUP 13-008 allowing for the installation of a 40-foot tall highway-oriented sign. The Mullahey properties are not located within the current area that allows for highway-oriented signs. Code Amendment 13-001 is being processed by the City which proposes to expand the existing boundary to incorporate the Mullahey properties and the Paso Robles Waste site.

With the approval of the Code Amendment, signs would be subject to the current regulations in terms of height and square footage. Based on the Mullahey site being larger than 3 acres, it is possible to apply for a 40-foot tall sign.

The height and design of the sign was reviewed by the DRC, where it was discussed that the design and the height of the sign seems appropriate, given the size and location of the site.

If the Planning Commission approves CUP 13-008 to allow for the highway oriented sign, the final approval of the sign will be subject to the City Council approving the Code Amendment 13-001.

Pre-Grading:

It is necessary to remove approximately 5,000 cubic yards of dirt in order to provide for the new building and parking lot and provide for the necessary drainage and low impact design measures. The applicants are requesting that the Planning Commission allow them to place the dirt on the parcels they own on the south side of Tractor Street. The zoning code allows what is referred to as Pre-project Grading for grading of an area over 20,000 square feet, without an accompanying development. Since the 5,000 cubic yards of dirt will take up an area larger than 20,000 square feet, an approval of the pre-grading needs Planning Commission approval.

The applicants have provided a "stock pile" plan which indicates that the stored dirt will not exceed beyond 6-feet in height and dust control measures will be provided.

Conclusion:

It would seem that the proposed dealership along with the highway-oriented sign would be consistent with other similar uses in this area. The project would meet the intent of the Zoning Code, General Plan, and Borkey Specific Plan by providing clean, attractive highway oriented uses.

Policy

Reference: Zoning Code, General Plan Land Use Element, Borkey Area Specific Plan, and 2006 Economic Strategy.

Fiscal

Impact: There are no specific fiscal impacts associated with approval of this Planned Development.

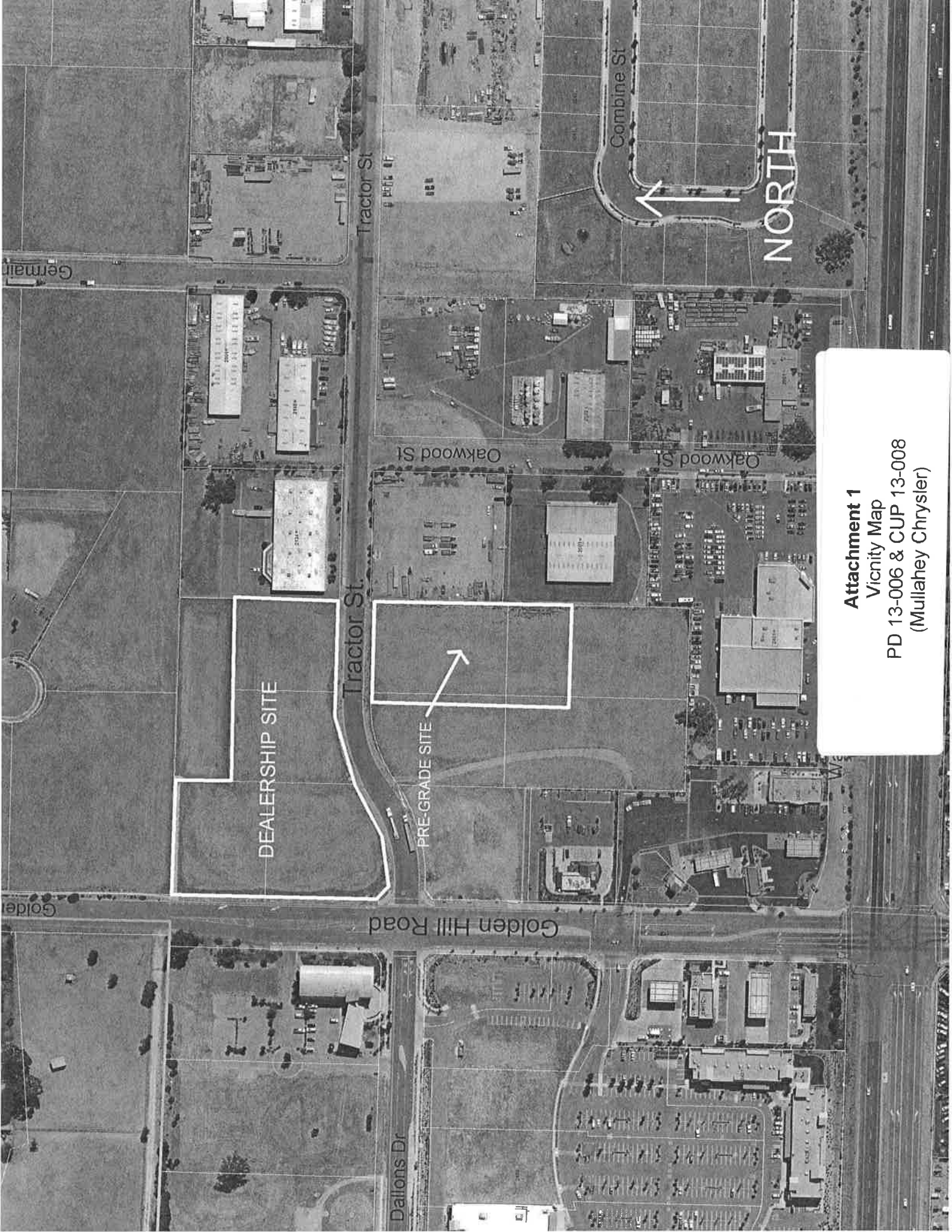
Options:

After consideration of all public testimony, that the Planning Commission may choose the following options:

- A.
 1. Adopt a Resolution approving a Negative Declaration for the project;
 2. Adopt the attached Resolution approving Planned Development 13-006 & CUP 13-008, including the establishment of the new car dealership and the proposed pre-project grading and 40 foot tall highway-oriented sign, subject to standard and site specific conditions of approval.
 3. Adopt the attached Resolution approving CUP 13-008, allowing the installation of the 40-foot tall highway oriented sign, subject to standard and site specific conditions of approval and subject to the City Council adopting Code Amendment 13-001.
- B. Amend, modify, or reject the above-listed action;

Attachments:

1. Vicinity Map
2. APN Map
3. City Engineer's Memo
4. Draft Resolution to approve a Negative Declaration
5. Draft Resolution to approve PD 13-006
6. Draft Resolution to approve CUP 13-008
7. Mail and Newspaper Affidavits

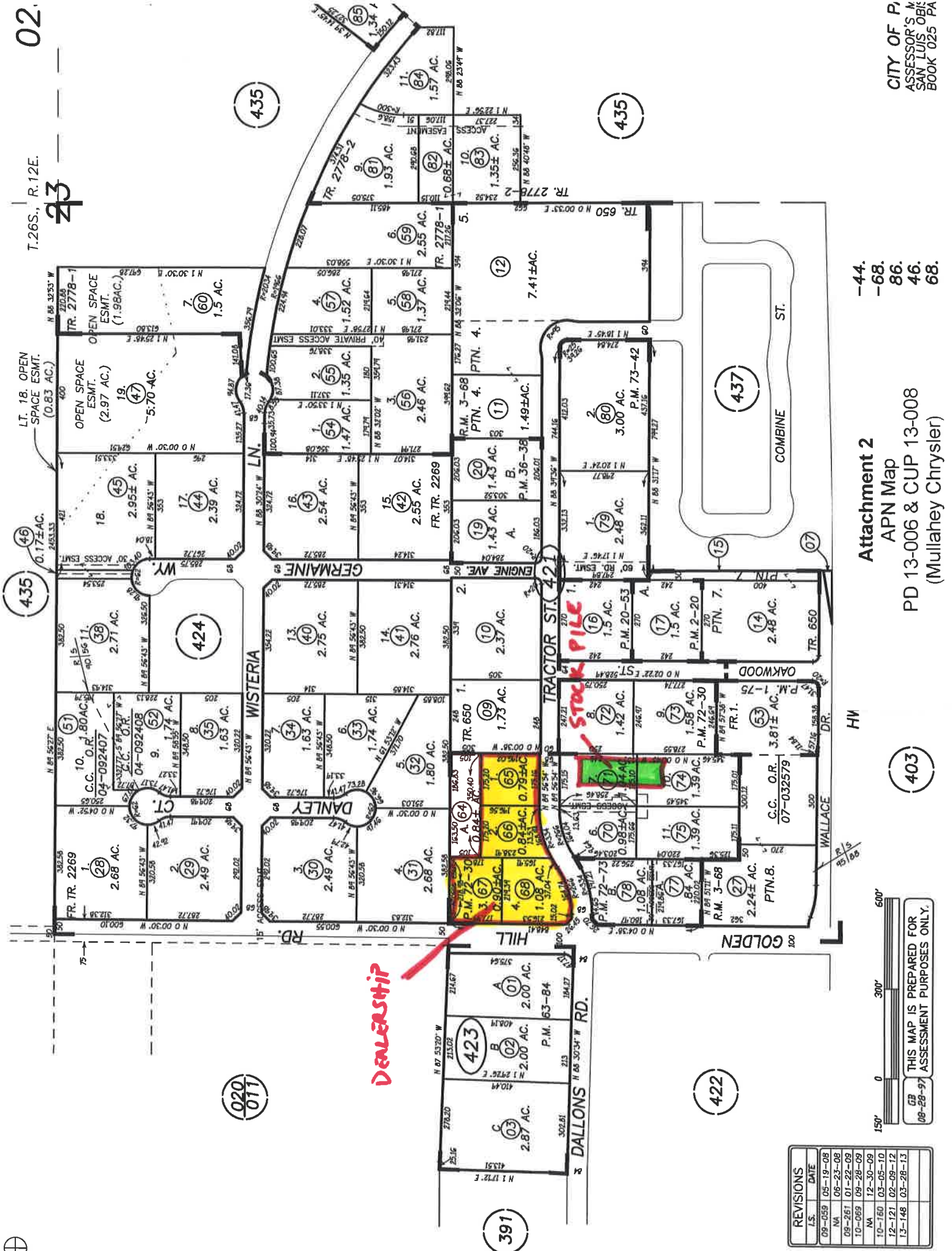


DEALERSHIP SITE

PRE-GRADE SITE

NORTH

Attachment 1
Vicinity Map
PD 13-006 & CUP 13-008
(Mullahey Chrysler)



REVISIONS	I.S.	DATE
	09-059	05-19-08
	NA	05-23-08
	09-261	01-22-09
	10-069	09-28-09
	NA	12-30-09
	10-160	03-05-10
	12-121	02-09-12
	13-148	03-28-13

Attachment 2
 APN Map
 PD 13-006 & CUP 13-008
 (Mullahey Chrysler)

-44.
 -68.
 86.
 46.
 68.

THIS MAP IS PREPARED FOR
 ASSESSMENT PURPOSES ONLY.

08-29-97
 GB

0403

CITY OF P...
 ASSESSOR'S M...
 SAN LUIS OBIS...
 BOOK 025 PA

MEMORANDUM

TO: Darren Nash
FROM: John Falkenstien
SUBJECT: PD 13-0063, Mullahey
DATE: January 16, 2014

Streets

The project fronts on Golden Hill Road and Tractor Way. Both streets were constructed with the subdivision of the Weyrick property. Sidewalks need to be completed along with development of the property.

Grading, Drainage and Storm Water Quality

On July 12, 2013, the Regional Water Quality Control Board adopted storm water management requirements for development projects in the Central Coast region. Upon the Board's direction, the City has adopted a Storm Water Ordinance requiring all projects to implement low impact development best management practices to mitigate impacts to the quality of storm water run-off and to limit the increase in the rate and volume of storm water run-off to the maximum extent practical. The best management practices include strict numeric criteria.

The applicant has prepared a storm water control plan offering a site assessment of constraints and opportunities and corresponding storm water management strategies in compliance with the new regulations.

Sewer and Water

Sewer and Water services were provided to the property with the development of the Weyrick subdivision. Sewer is available in Golden Hill Road from an existing lateral to an 18-inch main. Water services are available on both Golden Hill Road and Tractor Way. There is an 8-inch water main in Tractor Way and a 12-inch water main in Golden Hill Road.

Conditions

Prior to occupancy, sidewalks shall be completed along the frontage of the property on both Golden Hill Road and Tractor Way.

Low impact development best management practices as outlined in the project submittals shall be incorporated into the project grading and drainage plans.

RESOLUTION NO. _____
A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF PASO ROBLES
APPROVING A NEGATIVE DECLARATION FOR
PLANNED DEVELOPMENT 13-006
(MULLAHEY CHRYSLER DEALERSHIP)

WHEREAS, PD 13-006 has been submitted by Dennis J. Flynn Architects on behalf of Tim and Mike Mullahey, to establish a 29,907 square foot new car dealership with automotive repair; and

WHEREAS, the project also consists of the request to replace 5,000 cubic yards of dirt from the project site to adjacent parcels under the same ownership, without the request for development (Pre Project Grading); and

WHEREAS, the dealership is proposed to be located on the 3.6-acre site on the northeast corner of Golden Hill Road and Tractor Street, along with the pre project grading proposed to be located on Parcels 71 and 74, on the south side of Tractor Street; and

WHEREAS, in conjunction with PD 13-006, Conditional Use Permit (CUP) 13-008 has been applied for requesting to construct a 40-foot tall highway oriented sign on the dealership site; and

WHEREAS, an Initial Study was prepared for this project (attached as Exhibit A), which concludes that the project as proposed will not have significant impacts on the environment; and

WHEREAS, Public Notice of the proposed Negative Declaration was given as required by Section 21092 of the Public Resources Code; and

WHEREAS, public hearings were conducted by the Planning Commission on January 28, 2014 to consider the Initial Study prepared for this application, and to accept public testimony regarding this proposed environmental determination for the proposed zoning modification, and

WHEREAS, based on General Plan Land Use Designation, the 2003 General Plan Environmental Impact Report, information contained in the Initial Study prepared for this zoning modification, the staff report and testimony received as a result of the public notice, the City Council finds no substantial evidence that the project would have a significant impact on the environment.

NOW, THEREFORE, BE IT RESOLVED:

1. That the above Recitals are true and correct and incorporated herein by reference.
2. That based on the City's independent judgment, the City Council of the City of El Paso de Robles does hereby approve a Negative Declaration for PD 13-006, in accordance with the California Environmental Quality Act.

PASSED AND ADOPTED THIS 28th day of January, 2014 by the following roll call vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

VINCE VANDERLIP, CHAIRMAN

ATTEST:

ED GALLAGHER, COMMISSION SECRETARY

ENVIRONMENTAL INITIAL STUDY CHECKLIST FORM
CITY OF PASO ROBLES

- 1. PROJECT TITLE:** **Planned Development PD 13-006 & Conditional Use Permit 13-008**
- Concurrent Entitlements:** **PD, CUP & Pre Grading**
- 2. LEAD AGENCY:** City of Paso Robles
1000 Spring Street
Paso Robles, CA 93446
- Contact:**
Phone: (805) 237-3970
- 3. PROJECT LOCATION:** Northeast corner of Golden Hill Road and Tractor Street, Paso Robles, CA (APN: 025-421-065, 066, 067, 068, 071 & 074)
- 4. PROJECT PROPONENT:** Mullahey Chrysler
- Contact Person:** Tim Mullahey
- Phone:** (714) 501-0893
Email: tjmullahey@yahoo.com
- 5. GENERAL PLAN DESIGNATION:** CS (Commercial Service)
- 6. ZONING:** C3 (Commercial- Light Industrial)
Sub Area E, Borkey Area Specific Plan
- 7. PROJECT DESCRIPTION:** The project would consist of the following:
- PD 13-006:** Request to construct a 29,907 square foot new car dealership with automotive repair. The dealership is proposed to be built on a vacant 3.6 acre site located on the northeast corner of Golden Hill Road and Tractor Street.
- Also included with the project is a request for pre-grading (grading on a site for future development) which includes spreading out approximately 5,000 cubic yards of dirt on to parcels 025-421-071 & 074 that would be taken from the 3.6 acre site for the construction of the dealership.
- CUP 13-008:** Request for a 40-foot tall Highway Oriented Sign.

8. **ENVIRONMENTAL SETTING:** The parcels are flat, have been previously graded, and all street improvements and utilities have been installed and stubbed on to the lots. The lots have no biological resources.

This Initial Study incorporates by reference the City of El Paso de Robles General Plan Environmental Impact Report (EIR) (SCH#2003011123).

This site was included in an earlier environmental review process, where a Negative Declaration was approved for the commercial/industrial subdivision that created this lot (Tentative Parcel Map PR 04-0310, Resolution No. 04-0139).

9. **OTHER AGENCIES WHOSE APPROVAL IS REQUIRED (AND PERMITS NEEDED):** Air Pollution Control District.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|---|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture and Forestry Resources | <input type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology /Soils |
| <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality |
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Noise |
| <input type="checkbox"/> Population / Housing | <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation/Traffic | <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance |

DETERMINATION: (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Signature: _____

Date _____

EVALUATION OF ENVIRONMENTAL IMPACTS:

1. A brief explanation is required for all answers except “No Impact” answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A “No Impact” answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. All answers must take account of the whole action involved. Answers should address off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. “Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. “Potentially Significant Impact” is appropriate if there is substantial evidence that an effect may be significant. If there are one or more “Potentially Significant Impact” entries when the determination is made, an EIR is required.
4. “Negative Declaration: Less Than Significant With Mitigation Incorporated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less Than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from “Earlier Analyses,” as described in (5) below, may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
 - a. Earlier Analysis Used. Identify and state where they are available for review.
 - b. Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c. Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
7. Supporting Information Sources: A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.
8. The explanation of each issue should identify:
 - a. the significance criteria or threshold, if any, used to evaluate each question; and
 - b. the mitigation measure identified, if any, to reduce the impact to less than significance

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
--	---	---	---	----------------------

I. AESTHETICS: Would the project:

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|-------------------------------------|
| a. Have a substantial adverse effect on a scenic vista? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c. Substantially degrade the existing visual character or quality of the site and its surroundings? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?
(Sources: 1, 2, 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Discussion (a-d):

The project site is not located on a scenic vista and does not include scenic resources. The project is located in an area where there is existing commercial, light industrial, gas services stations and drive-through fast food uses and will be consistent in terms of use and form to the existing surrounding uses, therefore the project will not be an impact to existing visual character. Standard conditions of approval have been applied to the project that requires adequately shielded lighting to prevent off-site glare and as a result will be less than significant.

II. AGRICULTURE AND FOREST RESOURCES: In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. Would the project:

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Conflict with existing zoning for, or cause rezoning of, forest, land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 5114(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion (a-e): The project site is not located on land that is considered agricultural or forest land. There will be no impact from the project on this environmental factor.

III. AIR QUALITY: Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

a. Conflict with or obstruct implementation of the applicable air quality plan? (Source: Attachment 5)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Violate any air quality standard or contribute substantially to an existing or projected air quality violation? (Source: 11)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
c. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? (Source: Attachment 4)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Expose sensitive receptors to substantial pollutant concentrations? (Source: Attachment 4)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion (a-d):

The San Luis Obispo County area is a non-attainment area for the State standards for ozone and suspended particulate matter. The SLO County Air Pollution Control District (APCD) administers a permit system to ensure that stationary sources do not collectively create emissions which would cause local and state standards to be exceeded. The potential for future project development to create adverse air quality impacts falls generally into two categories: Short term and Long term impacts.

Short term impacts are associated with the grading and development portion of a project where earth work generates dust, but the impact ends when construction is complete. Long term impacts are related to the ongoing operational characteristics of a project and are generally related to vehicular trip generation and the level of offensiveness of the onsite activity being developed.

There will be short term impacts associated with grading for the proposed construction, standard conditions required by the City as well as the APCD will be implemented.

When reviewing the 29,907 square foot building with the APCD CEQA Handbook (April 2012), the project would produce less than the 25 lbs/day of ROG+NOx and therefore be considered less than significant and no mitigation is required for operational or long-term impacts based on Retail – Auto Center land use.

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
e. Create objectionable odors affecting a substantial number of people? (Source: 11) Discussion: It is not anticipated that there will be any objection odors as a result of the operation of the new car dealership.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

IV. BIOLOGICAL RESOURCES: Would the project:

a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with any local policies or ordinances protecting biological	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
--	---	---	---	----------------------

resources, such as a tree preservation policy or ordinance?

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

(Source:)

Discussion (a-f):

All parcels within the project site, including the lots where the pre-grading is proposed have been previously graded, and are located within an area that has development on all sides. There is no visible vegetation beyond native grasses on the site. Based on previous disturbance, there could not be potential impacts to endangered, threatened or rare species or their habitats.

V. CULTURAL RESOURCES: Would the project:

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a. Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d. Disturb any human remains, including those interred outside of formal cemeteries? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion (a-d): There are no historic resources (as defined), located on the site. There are also no archaeological or paleontological resources known to be present on the site or in the near vicinity. Since the property has been previously developed and has been graded, there is no impact to cultural resources.

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
---	---	---	----------------------

VI. GEOLOGY AND SOILS: Would the project:

a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:

i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42. (Sources: 1, 2, & 3)

Discussion: The potential for and mitigation of impacts that may result from fault rupture in the project area are identified and addressed in the General Plan EIR, pg. 4.5-8. There are two known fault zones on either side of the Salinas Rivers valley. The Rinconada Fault system runs on the west side of the valley, and grazes the City on its western boundary. The San Andreas Fault is on the east side of the valley and is situated about 30 miles east of Paso Robles. The City of Paso Robles recognizes these geologic influences in the application of the California Building Code (CBC) to all new development within the City. Review of available information and examinations indicate that neither of these faults is active with respect to ground rupture in Paso Robles. Soils and geotechnical reports and structural engineering in accordance with local seismic influences would be applied in conjunction with any new development proposal. Based on standard conditions of approval, the potential for fault rupture and exposure of persons or property to seismic hazards is not considered significant. There are no Alquist-Priolo Earthquake Fault Zones within City limits.

ii. Strong seismic ground shaking? (Sources: 1, 2, & 3)

Discussion: The proposed project will be constructed to current CBC codes. The General Plan EIR identified impacts resulting from ground shaking as less than significant and provided mitigation measures that will be incorporated into the design of this project including adequate structural design and not constructing over active or potentially active faults. Therefore, impacts that may result from seismic ground shaking are considered less than significant.

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
iii. Seismic-related ground failure, including liquefaction? (Sources: 1, 2 & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Discussion: Per the General Plan EIR, the project site is located in an area with soil conditions that have a low potential for liquefaction or other type of ground failure due to seismic events and soil conditions. To implement the EIR's mitigation measures to reduce this potential impact, the City has a standard condition to require submittal of soils and geotechnical reports, which include site-specific analysis of liquefaction potential for all building permits for new construction, and incorporation of the recommendations of said reports into the design of the project.				
iv. Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Discussion: Per the General Plan Safety Element, the project site is in an area that is designated a low-risk area for landslides. Therefore, potential impacts due to landslides is less than significant.				
b. Result in substantial soil erosion or the loss of topsoil? (Sources: 1, 2, & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: Per the General Plan EIR the soil condition is not erosive or otherwise unstable. As such, no significant impacts are anticipated. A geotechnical/ soils analysis will be required prior to issuance of grading permit that will evaluate the site specific soil stability and suitability of grading and retaining walls proposed. This study will determine the necessary grading techniques that will ensure that potential impacts due to soil stability will not occur. An erosion control plan shall be required to be approved by the City Engineer prior to commencement of site grading.				
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: See response to item a.iii, above.				
d. Be located on expansive soil, as defined in Table 18-1-B of the California Building Code, creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: See response to item a.iii, above.				

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
--	---	---	---	----------------------

- e. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?

	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion (a-d): The development will be connected to the City’s municipal wastewater system, therefore there would not be impacts related use of septic tanks.

VII. GREENHOUSE GAS EMISSIONS: Would the project:

- a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
--	--------------------------	--------------------------	-------------------------------------	--------------------------

- b. Conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gasses?

	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
--	--------------------------	--------------------------	-------------------------------------	--------------------------

Discussion (a-b):

When reviewing the 29,907 square foot building with the APCD CEQA Handbook (April 2012), the project would not exceed the 11.50 mt CO2e/year threshold and therefore be considered less than significant and no mitigation is required for greenhouse gas emissions, based on Retail – Auto Center land use.

VIII. HAZARDS AND HAZARDOUS MATERIALS: Would the project:

- a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?

	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
--	--------------------------	--------------------------	-------------------------------------	--------------------------

Discussion: All fluids and materials related to the automotive repair activities for this project will be handled by companies that specialize in their transport and disposal/recycling. Impacts related to the handling and disposal of hazardous materials will be less than significant.

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: See VIII a. above.				
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: The proposed auto dealership will not emit hazardous materials. There are no schools near this project site.				
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: The project site is not identified as a hazardous site per state Codes.				
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: (e. & f.) The project site is not located within an airport safety zone.

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
g. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: The project will not impair or interfere with adopted emergency response routes or plans.

h. Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The project is not in the vicinity of wildland fire hazard areas.

IX. HYDROLOGY AND WATER QUALITY: Would the project:

a. Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
---	--------------------------	--------------------------	-------------------------------------	--------------------------

Discussion: The proposed project is designed to retain storm water on-site through installation of various low-impact development (LID) features. The project was been designed to reduce impervious surfaces, and promote groundwater recharge by employing bioretention and underground storage wells through implementation of these measures. Thus, water quality standards will be maintained and discharge requirements will be in compliance with State and local regulations. Therefore, impacts to water quality and discharge will be less than significant.

b. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., Would the production rate of pre-existing nearby wells drop to a level which would not support existing land uses or planned uses for which permits have been granted)? Would decreased rainfall infiltration or groundwater recharge reduce stream baseflow? (Source: 7)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
---	--------------------------	--------------------------	-------------------------------------	--------------------------

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
---	---	---	----------------------

Discussion: The proposed project would be on the City’s municipal water supply system, therefore it could not individually impact nearby well production. The site is designed to reduce impervious surfaces where possible and to direct surface drainage to onsite retention systems to facilitate groundwater recharge.

The City has sufficient groundwater resource capacity in combination with surface water resources to adequately serve this project. The General Plan accounts for water resource demand for a combination of resort and residential land uses on this property. Therefore, this project would not substantially deplete groundwater supplies or interfere with groundwater recharge such that there would be a net deficit in aquifer volume or lowering of the groundwater basin, and impacts to groundwater resources would be less than significant.

- c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site? (Source: 10)
- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: The drainage pattern on the site would not be substantially altered with development of this project since the project largely maintains the existing, historic drainage pattern of the property, and drainage will be maintained on the project site. Additionally, surface flow would be directed to the existing detention basin at the northeast corner of the site. There are no streams, creeks or rivers on or near the project site that could be impacted from this project or result in erosion or siltation on- or off-site. Therefore, impacts to drainage patterns and facilities would less than significant.

- d. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site? (Source: 10)
- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: See IX c. above. Drainage resulting from development of this property will be maintained onsite and will not contribute to flooding on- or off-site. Thus, flooding impacts from the project are considered less than significant.

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
e. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff? (Source: 10)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion: As noted in IX a. above, surface drainage will be managed onsite and will not add to offsite drainage facilities. Additionally, onsite LID drainage facilities will be designed to clean pollutants before they enter the groundwater basin. Therefore, drainage impacts that may result from this project would be less than significant.

f. Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
---	--------------------------	--------------------------	-------------------------------------	--------------------------

Discussion: See answers IX a. – e. This project will result in less than significant impacts to water quality.

g. Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: There is no housing associated with this project nor is there any housing in the near vicinity downstream from the site and the site is not within or near a flood hazard area. Therefore this project could not result in flood related impacts to housing.

h. Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
---	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: See IX h. above.

i. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: See IX h. above. Additionally, there are no levees or dams in the City.

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
j. Inundation by mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: In accordance with the Paso Robles General Plan, there is no mudflow hazards located on or near the project site. Therefore, the project could not result in mudflow inundation impacts.

k. Conflict with any Best Management Practices found within the City's Storm Water Management Plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
---	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The project will implement the City's Storm Water Management Plan - Best Management Practices, and would therefore not conflict with these measures.

l. Substantially decrease or degrade watershed storage of runoff, wetlands, riparian areas, aquatic habitat, or associated buffer zones?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The project will incorporate all feasible means to manage water runoff on the project site. There is no wetland or riparian areas in the near vicinity, and the project could not result in impacts to aquatic habitat. Therefore, the project will not result in significant impacts to these resources.

X. LAND USE AND PLANNING: Would the project:

a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The project is proposed to be developed on existing vacant parcels that were created for commercial uses. The proposed auto dealership and associated pre-grading are permitted uses in the C3 zone and is consistent with the Borkey Area Specific Plan (Sub Area E). The project will not divide an established community.

b. Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
---	--------------------------	--------------------------	--------------------------	-------------------------------------

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
---	---	---	----------------------

Discussion: The property is zoned C-3 (Commercial – Light Industrial) as is within Subarea E of the Borkey Area Specific Plan. The C3 zoning district allows new car dealerships with accessory auto repair as a permitted use. The Zoning Code requires the approval of a Conditional Use Permit for highway oriented signs. The dealership project complies with the Specific Plan and would meet the intent of the Commercial Service (CS) land use designation by providing highway related commercial services, and therefore there is no impact to land use and zoning.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| c. Conflict with any applicable habitat conservation plan or natural community conservation plan? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion (c): There are no conservation plans associated with this property.

XI. MINERAL RESOURCES: Would the project:

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state? (Source: 1) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: There are no known mineral resources at this project site.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| b. Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? (Source: 1) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: There are no known mineral resources at this project site.

XII. NOISE: Would the project result in:

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies? (Source: 1) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
---	---	---	----------------------

Discussion: There will be noise created from the addition of the project related to the auto repair functions of this facility however, all air compressors and associated equipment will be located within the building. The nearest residential property to this site is across Golden Hill Road in the County. The new building will be located approximately 100 feet to the County property line and the new building will be approximately 480 feet from the existing residence on the site. Since all activities will be happening indoors, since the repair operations will be limited to 7am to 7pm daily, it is anticipated that noise from the repair operations to neighboring properties will be less than significant.

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| b. Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: The project may result in short term construction noise and vibration from machinery, however, the construction noise is not anticipated to be excessive nor operate in evening hours. Therefore, impacts from groundborne vibration noise would be considered less than significant.

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| c. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: See discussion on Section a. above.

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| d. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: See discussion on Section a. above.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels? (Sources: 1, 4) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion : The project is located within Airport Safety Zone 5 of the Airport Land Use Plan. The Plan indicates that vehicles and parts sales are permitted within Zone 5, without any special conditions related to airport safety or noise as a result of aircraft, and will thus not be impacted by airport related noise.

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
--------------------------------	--	------------------------------	-----------

XIII. POPULATION AND HOUSING: Would the project:

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a. Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)? (Source: 1) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion (a-c): The project site is currently undeveloped, vacant land and jobs created can be absorbed by the local and regional employment market, and will not create the demand for new housing or population growth or displace housing or people.

XIV. PUBLIC SERVICES: Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|-------------------------------------|
| a. Fire protection? (Sources: 1,10) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b. Police protection? (Sources: 1,10) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c. Schools? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d. Parks? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e. Other public facilities? (Sources: 1,10) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
---	---	---	----------------------

Discussion (a-e): The proposed project will not result in a significant demand for additional new services since it is not proposing to include new neighborhoods or a significantly large scale development, and the incremental impacts to services can be mitigated through payment of development impact fees. Therefore, impacts that may result from this project on public services are considered less than significant.

XV. RECREATION

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion (a&b):

As a commercial development project that will not encourage new housing demands and use of recreational facilities, it will not result in impacts to recreational facilities.

XVI. TRANSPORTATION/TRAFFIC: Would the project:

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a. Conflict with an applicable plan, ordinance or policy establishing measures or effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
---	---	---	----------------------

Discussion: The proposed project is on an existing lot that has existing improved street frontages. The project is located near existing regional commercial area and is in proximity of within the downtown area and is in walking distance to many commercial areas in the vicinity. A transit stop is located within one block from the project site on Dallons Drive. The project is consistent with the policies of the City’s 2011 Circulation Element by providing facilities for multiple modes of transportation.

- b. Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------	-------------------------------------	--------------------------

(Source: Attachment 8)

Discussion: Based on the project being consistent with the C3 and CS zoning and land use designations, and based on auto dealerships with auto repair not being considered high traffic generators, the project impacts to traffic and circulation will be less than significant.

As required by all development projects with the approval of a building permit, the applicant shall be required to pay transportation impact fees established by City Council in affect at the time of occupancy to mitigate future impacts with planned improvements by the City.

- c. Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The project is located within Airport Safety Zone 5 of the Airport Land Use Plan. The Plan indicates that vehicles and parts sales are permitted within Zone 5, without any special conditions related to airport safety, and therefore, the project will not change air traffic patterns.

- d. Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: There are no hazardous design features associated with, planned for or will result from this project.

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
e. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: The project will not impede emergency access, and is designed in compliance with all emergency access safety features and to City emergency access standards.

f. Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The project would be built within an existing commercial subdivision which incorporates multi-modal transportation facilities and access such as bike lanes, sidewalks, walkways and is located near a transit stop. Therefore, it does not conflict with policies and plans regarding these facilities.

XVII. UTILITIES AND SERVICE SYSTEMS: Would the project:

a. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
---	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The project will comply with all applicable wastewater treatment requirements required by the City, RWQCB and the State. Therefore, there will be no impacts resulting from wastewater treatment from this project.

b. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: Per the City's General Plan EIR, Urban Water Management Plan, and Sewer System Management Plan, the City's water and wastewater treatment facilities are adequately sized, including planned facility upgrades, to provide water needed for this project and treat effluent resulting from this project. Therefore, this project will not result in the need to construct new facilities.

c. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
---	--------------------------	--------------------------	--------------------------	-------------------------------------

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
---	---	---	----------------------

Discussion: All new stormwater resulting from this project will be managed on the project site, and will not enter existing storm water drainage facilities or require expansion of new drainage facilities. Therefore, the project will not impact the City’s storm water drainage facilities.

- d. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?

Discussion: The auto dealership project is a permitted use in the current land use and zoning designations; therefore the project can be served with existing water resource entitlements available and will not require expansion of new water resource entitlements.

- e. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the projects projected demand in addition to the providers existing commitments?

Discussion: Per the City’s SSMP The City’s wastewater treatment facility has adequate capacity to serve this project as well as existing commitments.

- f. Be served by a landfill with sufficient permitted capacity to accommodate the project’s solid waste disposal needs?

Discussion: Per the City’s Landfill Master Plan, the City’s landfill has adequate capacity to accommodate construction related and operational solid waste disposal for this project.

- g. Comply with federal, state, and local statutes and regulations related to solid waste?

Discussion: The project will comply with all federal, state, and local solid waste regulations.

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
---	---	---	----------------------

XVIII. MANDATORY FINDINGS OF SIGNIFICANCE

- a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: As noted within this environmental document, and based on this site being an infill site that has been previously graded, and surrounded by development there will be no impact to fish habitat as well as no impact to fish and wildlife populations. The site is routinely maintained and mowed, so there will be no impact to fish, wildlife, of plant habitat.

- b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The project will not have impacts that are individually limited, but cumulatively considerable.

- c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The project will not cause substantial adverse effects on human beings, either directly or indirectly.

EARLIER ANALYSIS AND BACKGROUND MATERIALS.

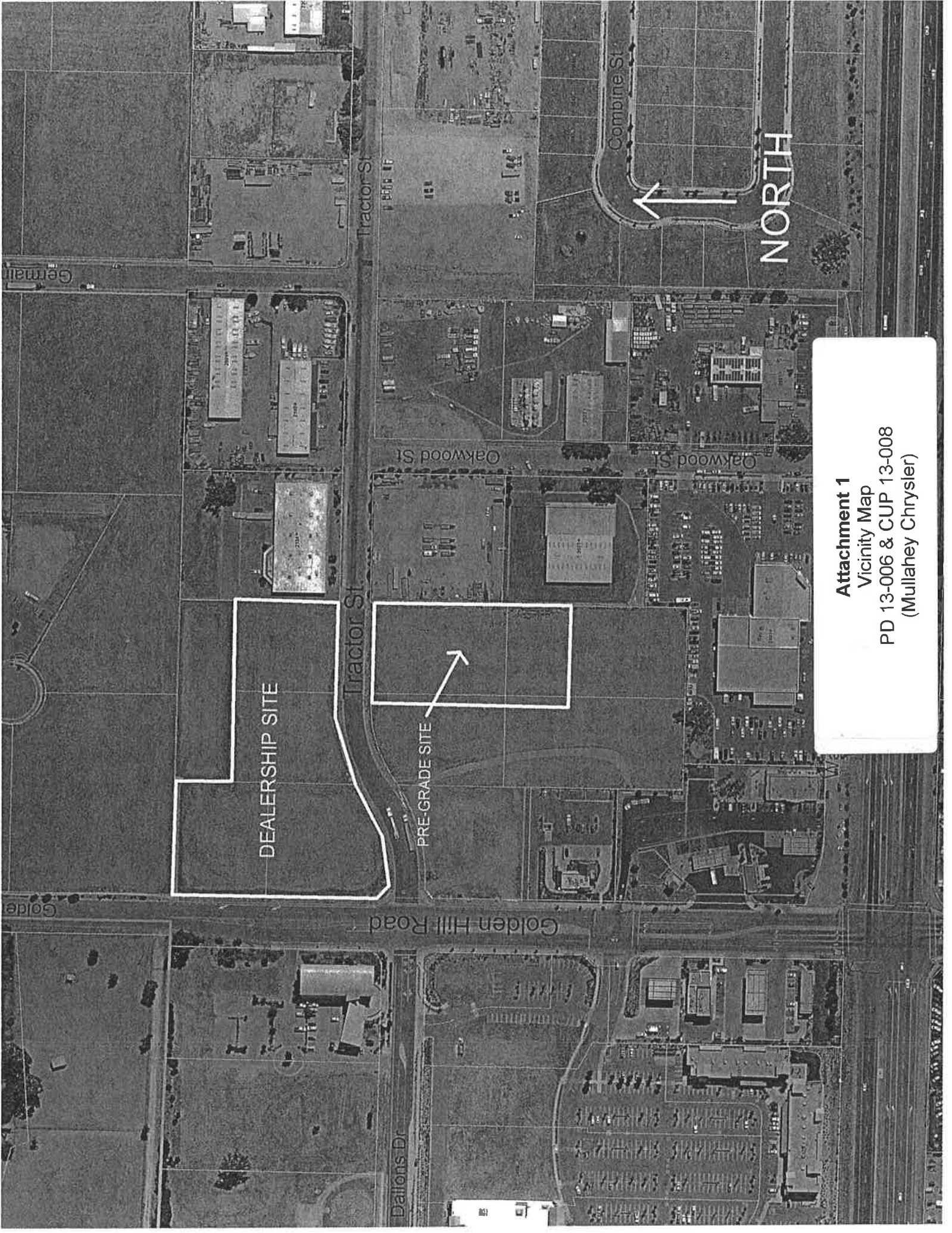
Earlier analyses may be used where, pursuant to tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or negative declaration. Section 15063 (c)(3)(D).

Earlier Documents Prepared and Utilized in this Analysis and Background / Explanatory Materials

<u>Reference #</u>	<u>Document Title</u>	<u>Available for Review at:</u>
1	City of Paso Robles General Plan	City of Paso Robles Community Development Department 1000 Spring Street Paso Robles, CA 93446
2	City of Paso Robles Zoning Code	Same as above
3	City of Paso Robles Environmental Impact Report for General Plan Update	Same as above
4	2005 Airport Land Use Plan	Same as above
5	City of Paso Robles Municipal Code	Same as above
6	City of Paso Robles Water Master Plan	Same as above
7	City of Paso Robles Urban Water Management Plan 2005	Same as above
8	City of Paso Robles Sewer Master Plan	Same as above
9	City of Paso Robles Housing Element	Same as above
10	City of Paso Robles Standard Conditions of Approval for New Development	Same as above
11	San Luis Obispo County Air Pollution Control District Guidelines for Impact Thresholds	APCD 3433 Roberto Court San Luis Obispo, CA 93401
12	San Luis Obispo County – Land Use Element	San Luis Obispo County Department of Planning County Government Center San Luis Obispo, CA 93408
13	USDA, Soils Conservation Service, Soil Survey of San Luis Obispo County, Paso Robles Area, 1983	Soil Conservation Offices Paso Robles, Ca 93446

Attachments:

- 1. Vicinity Map**
- 2. Site Plan**



Attachment 1
Vicinity Map
PD 13-006 & CUP 13-008
(Mullahey Chrysler)



DENNIS J. FLYNN
ARCHITECTS, INC.

300 NORTH BROAD STREET
FULLERTON, CALIFORNIA 92631
TEL: 714.771.1100
WWW.DJFLYNNARCHITECTS.COM

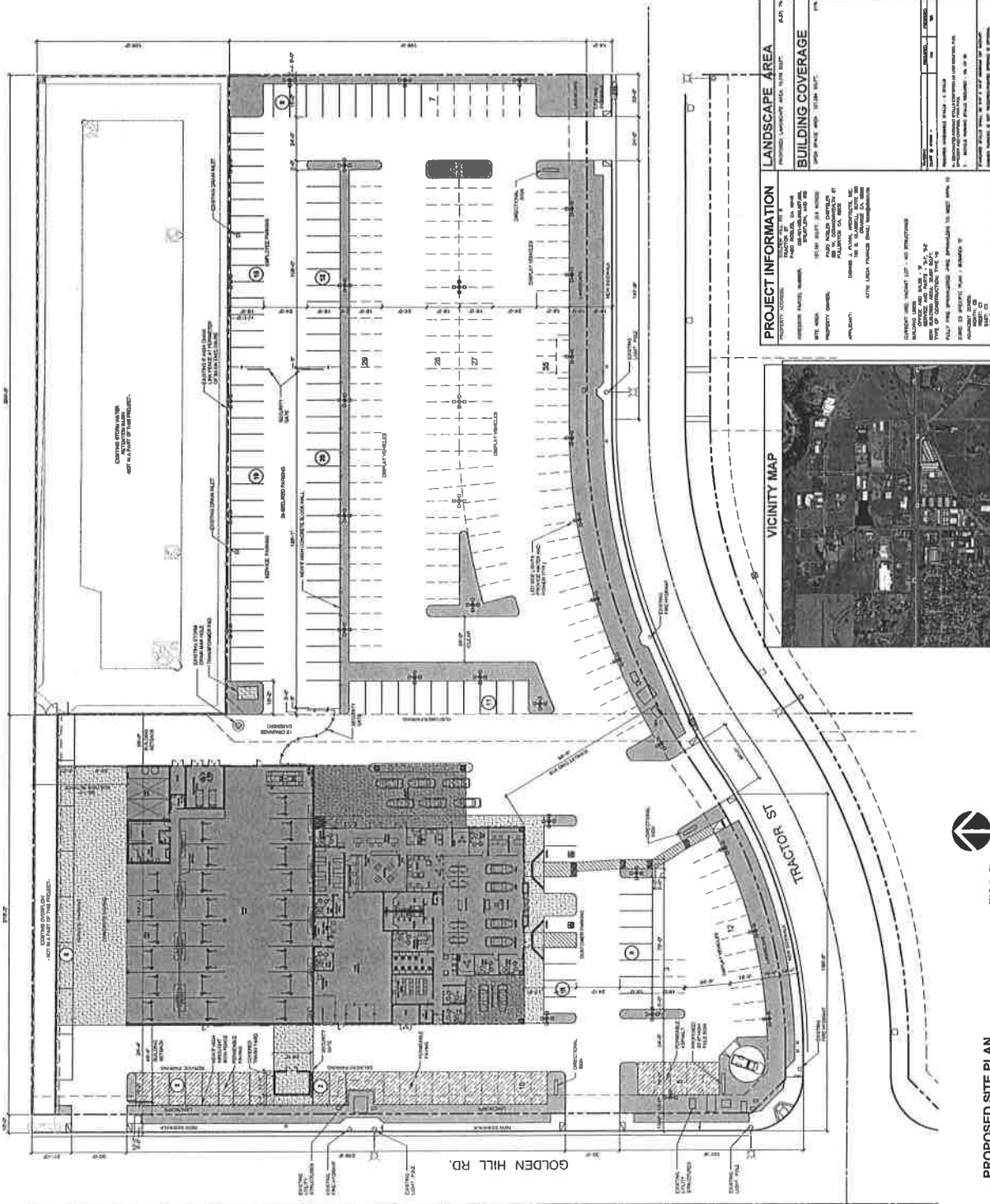
DESIGNER	DENNIS J. FLYNN ARCHITECTS, INC.
DATE	08/15/2013
PROJECT	CHRYSLER JEEP DODGE RAM PROPOSED SITE PLAN
CLIENT	PASO ROBLES
LOCATION	TRACTOR ST & GOLDEN HILL RD, PASO ROBLES, CA 94388
SCALE	AS SHOWN
STATUS	PROPOSED SITE PLAN
REVISIONS	
NO.	DESCRIPTION
1	ISSUED FOR PERMIT
2	ISSUED FOR PERMIT
3	ISSUED FOR PERMIT
4	ISSUED FOR PERMIT
5	ISSUED FOR PERMIT
6	ISSUED FOR PERMIT
7	ISSUED FOR PERMIT
8	ISSUED FOR PERMIT
9	ISSUED FOR PERMIT
10	ISSUED FOR PERMIT
11	ISSUED FOR PERMIT
12	ISSUED FOR PERMIT
13	ISSUED FOR PERMIT
14	ISSUED FOR PERMIT
15	ISSUED FOR PERMIT
16	ISSUED FOR PERMIT
17	ISSUED FOR PERMIT
18	ISSUED FOR PERMIT
19	ISSUED FOR PERMIT
20	ISSUED FOR PERMIT
21	ISSUED FOR PERMIT
22	ISSUED FOR PERMIT
23	ISSUED FOR PERMIT
24	ISSUED FOR PERMIT
25	ISSUED FOR PERMIT
26	ISSUED FOR PERMIT
27	ISSUED FOR PERMIT
28	ISSUED FOR PERMIT
29	ISSUED FOR PERMIT
30	ISSUED FOR PERMIT
31	ISSUED FOR PERMIT
32	ISSUED FOR PERMIT
33	ISSUED FOR PERMIT
34	ISSUED FOR PERMIT
35	ISSUED FOR PERMIT
36	ISSUED FOR PERMIT
37	ISSUED FOR PERMIT
38	ISSUED FOR PERMIT
39	ISSUED FOR PERMIT
40	ISSUED FOR PERMIT
41	ISSUED FOR PERMIT
42	ISSUED FOR PERMIT
43	ISSUED FOR PERMIT
44	ISSUED FOR PERMIT
45	ISSUED FOR PERMIT
46	ISSUED FOR PERMIT
47	ISSUED FOR PERMIT
48	ISSUED FOR PERMIT
49	ISSUED FOR PERMIT
50	ISSUED FOR PERMIT

CHRYSLER JEEP DODGE RAM
PROPOSED SITE PLAN

PASO ROBLES
TRACTOR ST
PASO ROBLES, CA 94388

PROJECT NO. 13-006
DATE: 08/15/2013
SCALE: AS SHOWN
SHEET NO. 13-006-01
SHEET TOTAL: 13-006-01
PROJECT OWNER: PASO ROBLES
ARCHITECT: DENNIS J. FLYNN ARCHITECTS, INC.
PLANNING: DENNIS J. FLYNN ARCHITECTS, INC.
ENGINEER: DENNIS J. FLYNN ARCHITECTS, INC.
LANDSCAPE ARCHITECT: DENNIS J. FLYNN ARCHITECTS, INC.
CITY: PASO ROBLES, CA
COUNTY: BUTTE, CA
STATE: CALIFORNIA

A1.01



PROJECT INFORMATION	LANDSCAPE AREA
PROJECT NO. 13-006	PROPOSED LANDSCAPE AREA: 10,000 SQ. FT.
PROJECT OWNER: PASO ROBLES	BUILDING COVERAGE
ARCHITECT: DENNIS J. FLYNN ARCHITECTS, INC.	PROPOSED BUILDING COVERAGE: 10,000 SQ. FT.
PLANNING: DENNIS J. FLYNN ARCHITECTS, INC.	EXISTING BUILDING COVERAGE: 0 SQ. FT.
ENGINEER: DENNIS J. FLYNN ARCHITECTS, INC.	
LANDSCAPE ARCHITECT: DENNIS J. FLYNN ARCHITECTS, INC.	
CITY: PASO ROBLES, CA	
COUNTY: BUTTE, CA	
STATE: CALIFORNIA	



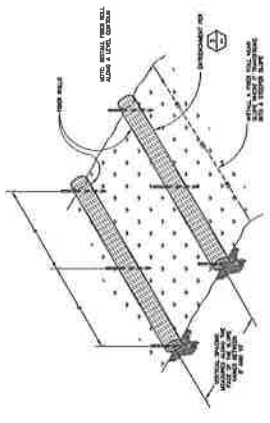
MADE IN U.S.A.

PROPOSED SITE PLAN

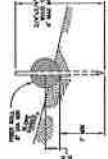
Attachment 2
Site Plan - Dealership
PD 13-006 & CUP 13-008
(Mullahey Chrysler)

GRADING AND DRAINAGE / UTILITY LEGEND

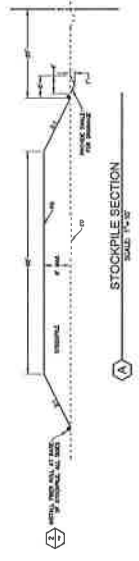
UTILITY	
1" DIA. WATER	2" DIA. WATER
4" DIA. WATER	6" DIA. WATER
8" DIA. WATER	12" DIA. WATER
16" DIA. WATER	24" DIA. WATER
36" DIA. WATER	48" DIA. WATER
60" DIA. WATER	72" DIA. WATER
12" DIA. SEWER	18" DIA. SEWER
24" DIA. SEWER	36" DIA. SEWER
48" DIA. SEWER	60" DIA. SEWER
72" DIA. SEWER	96" DIA. SEWER
12" DIA. GAS	18" DIA. GAS
24" DIA. GAS	36" DIA. GAS
48" DIA. GAS	60" DIA. GAS
72" DIA. GAS	96" DIA. GAS
12" DIA. ELECTRIC	18" DIA. ELECTRIC
24" DIA. ELECTRIC	36" DIA. ELECTRIC
48" DIA. ELECTRIC	60" DIA. ELECTRIC
72" DIA. ELECTRIC	96" DIA. ELECTRIC
12" DIA. TELEPHONE	18" DIA. TELEPHONE
24" DIA. TELEPHONE	36" DIA. TELEPHONE
48" DIA. TELEPHONE	60" DIA. TELEPHONE
72" DIA. TELEPHONE	96" DIA. TELEPHONE
12" DIA. CABLE TV	18" DIA. CABLE TV
24" DIA. CABLE TV	36" DIA. CABLE TV
48" DIA. CABLE TV	60" DIA. CABLE TV
72" DIA. CABLE TV	96" DIA. CABLE TV
12" DIA. FIBER OPTIC	18" DIA. FIBER OPTIC
24" DIA. FIBER OPTIC	36" DIA. FIBER OPTIC
48" DIA. FIBER OPTIC	60" DIA. FIBER OPTIC
72" DIA. FIBER OPTIC	96" DIA. FIBER OPTIC
12" DIA. RAINWATER	18" DIA. RAINWATER
24" DIA. RAINWATER	36" DIA. RAINWATER
48" DIA. RAINWATER	60" DIA. RAINWATER
72" DIA. RAINWATER	96" DIA. RAINWATER
12" DIA. STORMWATER	18" DIA. STORMWATER
24" DIA. STORMWATER	36" DIA. STORMWATER
48" DIA. STORMWATER	60" DIA. STORMWATER
72" DIA. STORMWATER	96" DIA. STORMWATER



2 TYPICAL FIBER ROLL INSTALLATION



3 TYPICAL FIBER ROLL ENTRENCHMENT



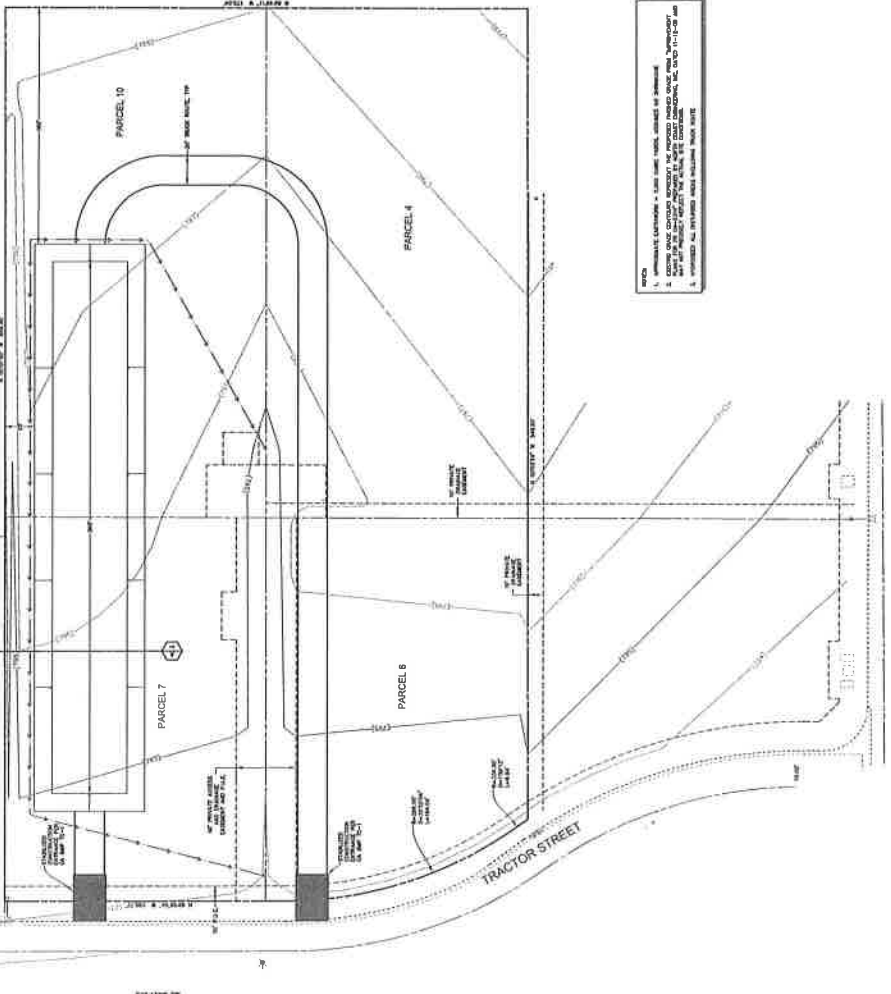
A STOCKPILE SECTION



**CHRYSLER JEEP DODGE RAM
OFFSITE STOCKPILE AND
EROSION CONTROL PLAN**



DATE: 12/20/11



NOTES
1. APPROXIMATE QUANTITIES - QUANTITIES ARE APPROXIMATE AND SUBJECT TO CHANGE.
2. QUANTITIES ARE BASED ON A 10% EROSION RISK FACTOR. QUANTITIES MAY VARY BASED ON ACTUAL SITE CONDITIONS.
3. QUANTITIES ARE BASED ON A 10% EROSION RISK FACTOR. QUANTITIES MAY VARY BASED ON ACTUAL SITE CONDITIONS.

**Attachment 3
Site Plan - Pre grading
PD 13-006 & CUP 13-008
(Mullahey Chrysler)**

RESOLUTION NO.:

A RESOLUTION OF THE PLANNING COMMISSION
OF THE CITY OF EL PASO DE ROBLES
APPROVING PLANNED DEVELOPMENT 13-006 & CONDITIONAL USE PERMIT 13-008
MULLAHEY CHRYSLER DEALERSHIP
(APN: 025-421-065, 066, 067, 068, 071 & 074)

WHEREAS, PD 13-006 has been submitted by Dennis J. Flynn Architects on behalf of Tim and Mike Mullahey, to establish a 29,907 square foot new car dealership with automotive repair; and

WHEREAS, the project also consists of the request to replace 5,000 cubic yards of dirt from the project site to adjacent parcels under the same ownership, without the request for development (Pre Project Grading); and

WHEREAS, the dealership is proposed to be located on the 3.6-acre site on the northeast corner of Golden Hill Road and Tractor Street, along with the pre project grading proposed to be located on Parcels 71 and 74, on the south side of Tractor Street; and

WHEREAS, in conjunction with PD 13-006, Conditional Use Permit (CUP) 13-008 has been applied for requesting to construct a 40-foot tall highway oriented sign on the dealership site; and

WHEREAS, a public hearing was conducted by the Planning Commission on January 28, 2014, to consider facts as presented in the staff report prepared for this project, and to accept public testimony regarding this proposed Development Plan, and associated Negative Declaration; and

WHEREAS, a resolution was adopted by the Planning Commission approving a Negative Declaration status for this project, and a Negative Declaration was prepared for the proposed Planned Development and Rezone applications in accordance with the California Environmental Quality Act; and

WHEREAS, based upon the facts and analysis presented in the staff report and the attachments thereto, the public testimony received, and subject to the Conditions of Approval listed below, the Planning Commission makes the following findings:

Section 1. Findings

In accordance with Sections 21.23.250 and 21.23B.050 of the Zoning Code, based on facts and analysis set forth in the staff report for this item, and taking into consideration comments received from the public and/or other governmental agencies having purview in the subject development plan application, the Planning Commission hereby makes the following findings:

- a. The design and intensity (density of the proposed development is consistent with the following):
- b. The Mullahey Chrysler project, is consistent with the adopted codes, policies, standards and plans of the City; since the project has gone through the development review process including,

environmental review as required by Section 21.23.B of the Zoning Code related to buildings over 10,000 square feet; and

- c. The Mullahey Chrysler project, will not be detrimental to the health, safety, morals, comfort, convenience and general welfare of the residents and or businesses in the surrounding area, or be injurious or detrimental to property and improvements in the neighborhood or to the general welfare of the City; since the project will be required to comply with the recommended conditions of approval, including any environmental mitigation measures, and comply with any building and fire codes; and
- d. The Mullahey Chrysler project accommodates the aesthetic quality of the City as a whole, especially where development will be visible from the gateways to the City, scenic corridors and the public right-of-way; in this particular case, based on the site plan, building architecture and landscaping, the proposed development will accommodate the aesthetic quality of the City as a whole; and
- e. The Mullahey Chrysler project is compatible with, and is not detrimental to, surrounding land uses and improvements, provides an appropriate visual appearance, and contributes to the mitigation of any environmental and social impacts, as a result of the site planning and building architecture included with this project.
- f. The Mullahey Chrysler project is compatible with existing scenic and environmental resources such as hillsides, oak trees, vistas, etc. as a result of the project site being flat, and located in an area of the City where there is existing commercial and light-industrial development similar to what is being proposed by this project; and
- g. The establishment, maintenance or operation of the Mullahey Chrysler project, will not, under the circumstances of the particular case, be detrimental to the health, safety, morals, comfort, convenience and general welfare of the persons residing or working in the neighborhood of such proposed use, since the project has gone through the development review process including, environmental review as required by Section 21.23.B of the Zoning Code related to buildings over 10,000 square feet; and
- h. The Mullahey Chrysler project contributes to the orderly development of the City as a whole, since the project will utilize the existing infrastructure in Golden Hill Road and Tractor Street, consisting of sewer water and other utilities; and
- i. The Mullahey Chrysler project as conditioned would meet the intent of the General Plan and Zoning Ordinance by providing clean attractive highway oriented businesses in the C3 zoning district.
- j. The Mullahey Chrysler project would be consistent with the Economic Strategy, since it would allow for retail commercial growth.
- k. The Mullahey Chrysler project would be consistent with the Borkey Area Specific Plan, since it provide for retail commercial uses in the Sub Area E.

1. The proposed pre grading would be acceptable since the site where the dirt would be placed is large enough to accept the dirt and the dirt will be spread in a manner that blends in with the site.

Section 2. Conditions of Approval

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission of the City of El Paso de Robles approves Planned Development 13-006 & Conditional Use Permit 13-008 subject to the following conditions:

PLANNING:

1. This PD 13-006 allows for the development of the 29,907 square foot new car dealership with automotive repair on the 3.6 acre site. PD 13-006 also allows for the pre project grading to allow for the placement of 5,000 cubic yards of dirt on Parcels 025-421-071 & 074.
2. CUP 13-008 allows for the construction of the highway oriented sign consistent with Exhibit I of this resolution. The approval of CUP 13-008 is subject to the City Council adopting Code Amendment 13-001, if it is not adopted the highway oriented sign is not approved. Please note that the maximum square footage the sign can be is 150 square feet. Exhibit I indicates the sign is 159 square feet, so the sign will need to be reduced to 150 square feet to comply with the Ordinance (if Code Amendment adopted).
3. The project shall be constructed in substantial conformance with the Conditions of Approval established by this Resolution and it shall be constructed in substantial conformance with the following Exhibits:

EXHIBIT	DESCRIPTION
A	Standard Conditions
B	Cover Sheet – Project Data
C	Site Plan
D	Floor Plan
E	Roof Plan
F	Architectural Elevations
G	Preliminary Landscape Plan
H	Preliminary Grading and Drainage
I	Site Cross Sections
J	Stock Pile – Pre Project Grading Plan
K	Highway Oriented Sign

4. Prior to the issuance of a building permit, Planning Staff shall review the following items to insure substantial compliance with the above listed Exhibits:
 - Final site details such as landscaping, exterior lighting and any other site planning details;
 - Architectural elevations, including final materials, colors and details;
 - Equipment such as back flow devices, transformers, a/c condensers and appropriate screening methods for both views and noise;
 - Final grading and drainage plans;
 - Signage
5. The project landscape plan is subject to the requirements within the City’s Landscape Ordinance.

6. All on-site operations shall be in conformance with the City's performance standards contained in Section 21.21.040 and as listed below:
 - a. Fire and Explosion Hazards. All activities involving, and all storage of, inflammable and explosive materials shall be provided with adequate safety devices against the hazard of fire and explosion and adequate firefighting and fire-suppression equipment and devices standard in industry and as approved by the fire department. All incineration is prohibited.
 - b. Radioactivity or Electrical Disturbance. Devices that radiate radio-frequency energy shall be so operated as not to cause interference with any activity carried on beyond the boundary line of the property upon which the device is located. Further, no radiation of any kind shall be emitted which is dangerous to humans. All radio transmissions shall occur in full compliance with Federal Communications Commission (FCC) and other applicable regulations.
 - c. Noise. No land use shall increase the ambient noise level as measured at the nearest residentially zoned property line to a level that constitutes a public nuisance.
 - d. Vibration. No vibrations shall be permitted so as to cause a noticeable tremor measurable without instruments at the lot line.
 - e. Smoke. Except for fireplaces and barbecues, no emission shall be permitted at any point from any chimney which would constitute a violation of standards established by the San Luis Obispo County Air Pollution Control District (APCD).
 - f. Odors. Except for fireplaces and barbecues, no emission shall be permitted of odorous gases or other odorous matter in such quantities as to constitute a public nuisance.
 - g. Fly Ash, Dust, Fumes, Vapors, Gases and Other Forms of Air Pollution. No emission shall be permitted which can cause damage to health, animals, vegetations or other forms of property, or which can cause any excessive soiling at any point. No emissions shall be permitted in excess of the standards established by the San Luis Obispo County Air Pollution Control District (APCD).
 - h. Glare. No direct glare, whether produced by floodlight, high-temperature processes such as combustion or welding or other processes, so as to be visible from any boundary line of the property on which the same is produced shall be permitted. Sky-reflected glare from buildings or portions thereof shall be so controlled by reasonable means as are practical to the end that said sky-reflected glare will not inconvenience or annoy persons or interfere with the use and enjoyment of property in and about the area where it occurs.
 - i. Liquid or Solid Wastes. No discharge shall be permitted at any point into any public sewer, private sewage disposal system or stream, or into the ground, of any materials of such nature or temperature as can contaminate any water supply, interfere with bacterial processes in sewage treatment, or otherwise cause the emission of dangerous or offensive elements, except in accord with standards approved by the California Department of Health or such other governmental agency as shall have jurisdiction over such activities. Manufacturing, processing, treatment and other activities involving use of toxic or hazardous materials shall be designed to

incorporate the best available control technologies and wherever technically feasible shall employ a "closed loop" system of containment.

- j. Transportation Systems Impacts. Vehicular, bikeway and/or pedestrian traffic, directly attributable to the proposed land use, shall not increase to a significant extent without implementation of adequate mitigation measures in a form to be approved by the city engineer. In determining significance of impacts, consideration shall be given to cumulative (projected build-out) capacity of streets and highways serving the land use. Mitigation measures required may include but not be limited to curb, gutter, sidewalk, street and/or alley, bikeway, transit related improvements and traffic signalization. Mitigation may be required as pursuant to the California Environmental Quality Act (CEQA), or as a condition of a discretionary review. (Ord. 665 N.S. § 28, 1993; (Ord. 405 N.S. § 2 (part), 1977)

ENGINEERING:

7. Prior to occupancy, sidewalks shall be completed along the frontage of the property on both Golden Hill Road and Tractor Way.
8. Low impact development best management practices as outlined in the project submittals shall be incorporated into the project grading and drainage plans.

Air Quality:

9. For projects with areas of disturbance exceeding 4 acres, the SLOAPCD requires implementation of the following mitigation measures to minimize nuisance impacts and to significantly reduce fugitive dust emissions:
 - a. Reduce the amount of the disturbed area where possible;
 - b. Use of water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site. Increased watering frequency would be required whenever wind speeds exceed 15 mph. Reclaimed (non-potable) water should be used whenever possible;
 - c. All dirt stock pile areas should be sprayed daily as needed;
 - d. Permanent dust control measures identified in the approved project revegetation and landscape plans should be implemented as soon as possible following completion of any soil disturbing activities;
 - e. Exposed ground areas that are planned to be reworked at dates greater than one month after initial grading should be sown with a fast germinating, non-invasive grass seed and watered until vegetation is established;
 - f. All disturbed soil areas not subject to revegetation should be stabilized using approved chemical soil binders, jute netting, or other methods approved in advance by the APCD;
 - g. All roadways, driveways, sidewalks, etc. to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used;
 - h. Vehicle speed for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site;

- i. All trucks hauling dirt, sand, soil, or other loose materials are to be covered or should maintain at least two feet of freeboard (minimum vertical distance between top of load and top of trailer) in accordance with CVC Section 23114;
 - j. Install wheel washers where vehicles enter and exit unpaved roads onto streets, or wash off trucks and equipment leaving the site;
 - k. Sweep streets at the end of each day if visible soil material is carried onto adjacent paved roads. Water sweepers with reclaimed water should be used where feasible;
 - l. All of these fugitive dust mitigation measures shall be shown on grading and building plans; and
 - m. The contractor or builder shall designate a person or persons to monitor the fugitive dust emissions and enhance the implementation of the measures as necessary to minimize dust complaints, reduce visible emissions below 20% opacity, and to prevent transport of dust offsite. Their duties shall include holidays and weekend periods when work may not be in progress. The name and telephone number of such persons shall be provided to the APCD Compliance Division prior to the start of any grading, earthwork or demolition.
10. Portable equipment, 50 horsepower (hp) or greater, used during construction activities may require California statewide portable equipment registration (issued by the California Air Resources Board) or an APCD permit. The following list is provided as a guide to equipment and operations that may have permitting requirements, but should not be viewed as exclusive. For a more detailed listing, refer to the Technical Appendices, page 4-4, in the APCD's 2012 CEQA Handbook.
- Power screens, conveyors, diesel engines, and/or crushers
 - Portable generators and equipment with engines that are 50 hp or greater
 - Electrical generation plants or the use of standby generator
 - Internal combustion engines
 - Rock and pavement crushing
 - Unconfined abrasive blasting operations
 - Tub grinders
 - Trommel screens
 - Portable plants (e.g. aggregate plant, asphalt batch plant, concrete batch plant, etc)

To minimize potential delays, prior to the start of the project, please contact the APCD Engineering Division at (805) 781-5912 for specific information regarding construction phase permitting requirements.

PASSED AND ADOPTED THIS 28th day of January, 2014 by the following Roll Call Vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

VINCE VANDERLIP, CHAIRMAN

ATTEST:

ED GALLAGHER, PLANNING COMMISSION SECRETARY

h:darren/PD/Mullahey /PC Res

EXHIBIT A OF RESOLUTION
CITY OF EL PASO DE ROBLES
STANDARD DEVELOPMENT CONDITIONS

Planned Development _____ Conditional Use Permit _____

Tentative Parcel Map _____ Tentative Tract Map _____

Approval Body: Planning Commission Date of Approval: January 14, 2014

Applicant: Mullahey Chrysler Location: Golden Hill Rd. – Tractor St.

APN: 025-421-065, 066, 067, 068, 071 & 074,

The following conditions that have been checked are standard conditions of approval for the above referenced project. The checked conditions shall be complied with in their entirety before the project can be finalized, unless otherwise specifically indicated. In addition, there may be site specific conditions of approval that apply to this project in the resolution.

COMMUNITY DEVELOPMENT DEPARTMENT - The applicant shall contact the Community Development Department, (805) 237-3970, for compliance with the following conditions:

A. GENERAL CONDITIONS – PD/CUP:

- 1. This project approval shall expire on January 28, 2016 unless a time extension request is filed with the Community Development Department, or a State mandated automatic time extension is applied prior to expiration.
- 2. The site shall be developed and maintained in accordance with the approved plans and unless specifically provided for through the Planned Development process shall not waive compliance with any sections of the Zoning Code, all other applicable City Ordinances, and applicable Specific Plans.
- 3. To the extent allowable by law, Owner agrees to hold City harmless from costs and expenses, including attorney’s fees, incurred by City or held to be the liability of City in connection with City’s defense of its actions in any proceeding brought in any State or Federal court challenging the City’s actions with respect to the project. Owner understands and acknowledges that City is under no obligation to defend any legal actions challenging the City’s actions with respect to the project.

- 4. Any site specific condition imposed by the Planning Commission in approving this project **(Conditional Use Permit)** may be modified or eliminated, or new conditions may be added, provided that the Planning Commission shall first conduct a public hearing in the same manner as required for the approval of this project. No such modification shall be made unless the Commission finds that such modification is necessary to protect the public interest and/or neighboring properties, or, in the case of deletion of an existing condition, that such action is necessary to permit reasonable operation and use for this approval.
- 5. The site shall be kept in a neat manner at all times and the landscaping shall be continuously maintained in a healthy and thriving condition.
- 6. All signs shall be subject to review and approval as required by Municipal Code Section 21.19 and shall require a separate application and approval prior to installation of any sign.
- 7. All walls/fences and exposed retaining walls shall be constructed of decorative materials which include but are not limited to splitface block, slumpstone, stuccoed block, brick, wood, crib walls or other similar materials as determined by the Development Review Committee, but specifically excluding precision block.
- 8. Prior to the issuance of a Building Permit a landscape and irrigation plan consistent with the Landscape and Irrigation Ordinance, shall be submitted for City review and approval. The plan needs to be designed in a manner that utilizes drought tolerant plants, trees and ground covers and minimizes, if not eliminates the use of **turf**. The irrigation plan shall utilize drip irrigation and limit the use of spray **irrigation**. All existing and/or new landscaping shall be installed with automatic irrigation systems.
- 9. A reciprocal parking and access easement and agreement for site access, parking, and maintenance of all project entrances, parking areas, landscaping, hardscape, common open space, areas and site lighting standards and fixtures, shall be recorded prior to or in conjunction with the Final Map. Said easement and agreement shall apply to all properties, and be referenced in the site Covenants, Conditions and Restrictions (CC&Rs).
- 10. All outdoor storage shall be screened from public view by landscaping and walls or fences per Section 21.21.110 of the Municipal Code.
- 11. For commercial, industrial, office or multi-family projects, all refuse enclosures are required to provide adequate space for recycling bins. The enclosure shall be architecturally compatible with the primary building. Gates shall be view obscuring and constructed of durable materials. Check with Paso Robles Waste Disposal to determine the adequate size of enclosure based on the number and size of containers to be stored in the enclosure.

- 12. For commercial, industrial, office or multi-family projects, all existing and/or new ground-mounted appurtenances such as air-conditioning condensers, electrical transformers, backflow devices etc., shall be screened from public view through the use of decorative walls and/or landscaping subject to approval by the Community Development Director or his designee. Details shall be included in the building plans.
- 13. All existing and/or new roof appurtenances such as air-conditioning units, grease hoods, etc. shall be screened from public view. The screening shall be architecturally integrated with the building design and constructed of compatible materials to the satisfaction of the Community Development Director or his designee. Details shall be included in the building plans.
- 14. All existing and/or new lighting shall be shielded so as to be directed downward in such a manner as to not create off-site glare or adversely impact adjacent properties. The style, location and height of the lighting fixtures shall be submitted with the building plans and shall be subject to approval by the Community Development Director or his designee.
- 15. All walls/fences and exposed retaining walls shall be constructed of decorative materials which include but are not limited to splitface block, slumpstone, stuccoed block, brick, wood, crib walls or other similar materials as determined by the Development Review Committee, but specifically excluding precision block.
- 16. It is the property owner's responsibility to insure that all construction of private property improvements occur on private property. It is the owner's responsibility to identify the property lines and insure compliance by the owner's agents.
- 17. Any existing Oak trees located on the project site shall be protected and preserved as required in City Ordinance No.835 N.S., Municipal Code No. 10.01 "Oak Tree Preservation", unless specifically approved to be removed. An Oak tree inventory shall be prepared listing the Oak trees, their disposition, and the proposed location of any replacement trees required. In the event an Oak tree is designated for removal, an approved Oak Tree Removal Permit must be obtained from the City, prior to removal.
- 18. No storage of trash cans or recycling bins shall be permitted within the public right-of-way.
- 19. Prior to recordation of the map or prior to occupancy of a project, all conditions of approval shall be completed to the satisfaction of the City Engineer and Community Developer Director or his designee.
- 20. Two sets of the revised Planning Commission approved plans incorporating all Conditions of Approval, standard and site specific, shall be submitted to the Community Development Department prior to the issuance of building permits.

- 21. Prior to the issuance of building permits, the
 - Development Review Committee shall approve the following:
 - Planning Division Staff shall approve the following:
 - a. A detailed site plan indicating the location of all structures, parking layout, outdoor storage areas, walls, fences and trash enclosures;
 - b. A detailed landscape plan;
 - c. Detailed building elevations of all structures indicating materials, colors, and architectural treatments;
 - d. Other:

B. GENERAL CONDITIONS – TRACT/PARCEL MAP:

- 1. In accordance with Government Section 66474.9, the subdivider shall defend, indemnify and hold harmless the City, or its agent, officers and employees, from any claim, action or proceeding brought within the time period provided for in Government Code section 66499.37, against the City, or its agents, officers, or employees, to attack, set aside, void, annul the City's approval of this subdivision. The City will promptly notify subdivider of any such claim or action and will cooperate fully in the defense thereof.
- 2. The Covenants, Conditions, and Restrictions (CC&Rs) and/or Articles Affecting Real Property Interests are subject to the review and approval of the Community Development Department, the Public Works Department and/or the City Attorney. They shall be recorded concurrently with the Final Map or prior to the issuance of building permits, whichever occurs first. A recorded copy shall be provided to the affected City Departments.
- 3. The owner shall petition to annex residential Tract (or Parcel Map)_____ into the City of Paso Robles Community Facilities District No. 2005-1 for the purposes of mitigation of impacts on the City's Police and Emergency Services Departments.
- 4. Street names shall be submitted for review and approval by the Planning Commission, prior to approval of the final map.
- 5. The following areas shall be permanently maintained by the property owner, Homeowners' Association, or other means acceptable to the City:

ENGINEERING DIVISION- The applicant shall contact the Engineering Division, (805) 237-3860, for compliance with the following conditions:

All conditions marked are applicable to the above referenced project for the phase indicated.

C. PRIOR TO ANY PLAN CHECK:

- 1. The applicant shall enter into an Engineering Plan Check and Inspection Services Agreement with the City.

D. PRIOR TO ISSUANCE OF A GRADING PERMIT:

- 1. Prior to approval of a grading plan, the developer shall apply through the City, to FEMA and receive a Letter of Map Amendment (LOMA) issued from FEMA. The developer's engineer shall provide the required supporting data to justify the application.
- 2. Any existing Oak trees located on the project site shall be protected and preserved as required in City Ordinance No. 553, Municipal Code No. 10.01 "Oak Tree Preservation", unless specifically approved to be removed. An Oak tree inventory shall be prepared listing the Oak trees, their disposition, and the proposed location of any replacement trees required. In the event an Oak tree is designated for removal, an approved Oak Tree Removal Permit must be obtained from the City, prior to its removal.
- 3. A complete grading and drainage plan shall be prepared for the project by a registered civil engineer and subject to approval by the City Engineer. The project shall conform to the City's Storm Water Discharge Ordinance.
- 4. A Preliminary Soils and/or Geology Report providing technical specifications for grading of the site shall be prepared by a Geotechnical Engineer.
- 5. A Storm Water Pollution Prevention Plan per the State General Permit for Storm Water Discharges Associated with Construction Activity shall be provided for any site that disturbs greater than or equal to one acre, including projects that are less than one acre that are part of a larger plan of development or sale that would disturb more than one acre.

E. PRIOR TO ISSUANCE OF A BUILDING PERMIT:

- 1. All off-site public improvement plans shall be prepared by a registered civil engineer and shall be submitted to the City Engineer for review and approval. The improvements shall be designed and placed to the Public Works Department Standards and Specifications.

- 2. The applicant shall submit a composite utility plan signed as approved by a representative of each public utility.
- 3. Landscape and irrigation plans for the public right-of-way shall be incorporated into the improvement plans and shall require approval by the Streets Division Supervisor and the Community Development Department.
- 4. In a special Flood Hazard Area as indicated on a Flood Insurance Rate Map (FIRM) the owner shall provide an Elevation Certificate in accordance with the National Flood Insurance program. This form must be completed by a land surveyor or civil engineer licensed in the State of California.

F. PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY OR RECORDATION OF THE FINAL MAP:

The Planning Commission has made a finding that the fulfillment of the construction requirements listed below are a necessary prerequisite to the orderly development of the surrounding area.

- 1. The applicant shall pay any current and outstanding fees for Engineering Plan Checking and Construction Inspection services.
- 2. All public improvements are completed and approved by the City Engineer, and accepted by the City Council for maintenance.
- 3. The owner shall offer to dedicate and improve the following street(s) to the standard indicated:

Street Name	City Standard	Standard Drawing No.
-------------	---------------	----------------------

- 4. If, at the time of approval of the final map, any required public improvements have not been completed and accepted by the City the owner shall be required to enter into a Subdivision Agreement with the City in accordance with the Subdivision Map Act.

Bonds required and the amount shall be as follows:
 Performance Bond.....100% of improvement costs.
 Labor and Materials Bond.....50% of performance bond.

- 5. If the existing City street adjacent to the frontage of the project is inadequate for the traffic generated by the project, or will be severely damaged by the construction, the applicant shall excavate the entire structural section and replace it with a standard half-width street plus a 12' wide travel lane and 8' wide graded shoulder adequate to provide for two-way traffic.

6. If the existing pavement and structural section of the City street adjacent to the frontage of the project is adequate, the applicant shall provide a new structural section from the proposed curb to the edge of pavement and shall overlay the existing paving to centerline for a smooth transition.
7. Due to the number of utility trenches required for this project, the City Council adopted Pavement Management Program requires a pavement overlay on _____ along the frontage of the project.
8. The applicant shall install all utilities underground. Street lights shall be installed at locations as required by the City Engineer. All existing overhead utilities adjacent to or within the project shall be relocated underground except for electrical lines 77 kilovolts or greater. All utilities shall be extended to the boundaries of the project.
9. The owner shall offer to dedicate to the City the following easement(s). The location and alignment of the easement(s) shall be to the description and satisfaction of the City Engineer:
- a. Public Utilities Easement;
 - b. Water Line Easement;
 - c. Sewer Facilities Easement;
 - d. Landscape Easement;
 - e. Storm Drain Easement.
10. The developer shall annex to the City's Landscape and Lighting District for payment of the operating and maintenance costs of the following:
- a. Street lights;
 - b. Parkway/open space landscaping;
 - c. Wall maintenance in conjunction with landscaping;
 - d. Graffiti abatement;
 - e. Maintenance of open space areas.
11. For a building with a Special Flood Hazard Area as indicated on a Flood Insurance Rate Map (FIRM), the developer shall provide an Elevation Certificate in accordance with the National Flood Insurance Program. This form must be completed by a lands surveyor or civil engineer licensed in the State of California.
12. All final property corners shall be installed.
13. All areas of the project shall be protected against erosion by hydro seeding or landscaping.
14. All construction refuse shall be separated (i.e. concrete, asphalt concrete, wood gypsum board, etc.) and removed from the project in accordance with the City's Source Reduction and Recycling Element.

- 15. Clear blackline mylars and paper prints of record drawings, signed by the engineer of record, shall be provided to the City Engineer prior to the final inspection. An electronic autocad drawing file registered to the California State Plane – Zone 5 / NAD83 projected coordinate system, units in survey feet, shall be provided.

PASO ROBLES DEPARTMENT OF EMERGENCY SERVICES- The applicant shall contact the Department of Emergency Services, (805) 227-7560, for compliance with the following conditions:

G. GENERAL CONDITIONS

- 1. Prior to the start of construction:
 - Plans shall be reviewed, approved and permits issued by Emergency Services for underground fire lines.
 - Applicant shall provide documentation to Emergency Services that required fire flows can be provided to meet project demands.
 - Fire hydrants shall be installed and operative to current, adopted edition of the California Fire Code.
 - A based access road sufficient to support the department's fire apparatus (HS-20 truck loading) shall be constructed and maintained for the duration of the construction phase of the project.
 - Access road shall be at least twenty (20) feet in width with at least thirteen (13) feet, six (6) inches of vertical clearance.

- 2. Provide central station monitored fire sprinkler system for all residential, commercial and industrial buildings that require fire sprinklers in current, adopted edition of the California Building Code, California Fire Code and Paso Robles Municipal Code.
 - Plans shall be reviewed, approved and permits issued by Emergency Services for the installation of fire sprinkler systems.

- 3. Provide central station monitored fire alarm system for all residential, commercial and industrial buildings that require fire alarm system in current, adopted edition of the California Building Code, California Fire Code and Paso Robles Municipal Code.

- 4. If required by the Fire Chief, provide on the address side of the building if applicable:
 - Fire alarm annunciator panel in weatherproof case.
 - Knox box key entry box or system.
 - Fire department connection to fire sprinkler system.

5. Provide temporary turn-around to current City Engineering Standard for phased construction streets that exceed 150 feet in length.
6. Project shall comply with all requirements in current, adopted edition of California Fire Code and Paso Robles Municipal Code.
7. Prior to the issuance of Certificate of Occupancy:
 - Final inspections shall be completed on all underground fire lines, fire sprinkler systems, fire alarm systems and chemical hood fire suppression systems.
 - Final inspections shall be completed on all buildings.

RECEIVED

JAN 10 2014

City of Paso Robles
Community Development Dept




CONSULTANT LIST		NOTES	 DENNIS J. FLYNN ARCHITECTS, INC. 190 S. GLASSSELL STREET, STE. 200 ORANGE, CALIFORNIA 92866	 NEW DEALERSHIP CHRYSLER / DODGE / JEEP / RAM GOLDEN HILL ROAD, PASO ROBLES, CA. 93446	PROPERTY OWNER: PASO ROBLES CHRYSLER 600 W. COMMONWEALTH ST FULLERTON CA, 92832	CIVIL PLANNING SUBMITTAL 12/30/13	CHRYSLER / DODGE / JEEP / RAM PASO ROBLES, CA 93446	DATE: [REDACTED] PERIOD: [REDACTED]	21031	T-1
CONSULTANT LIST GENERAL CONTRACTOR ARCHITECT: [REDACTED] CIVIL ENGINEER: [REDACTED] ELECTRICAL ENGINEER: [REDACTED] MECHANICAL ENGINEER: [REDACTED] PLUMBING ENGINEER: [REDACTED] PEAK ENGINEERING 5200 S. MESA BLVD., STE. 100 PASO ROBLES, CA 93446 805-238-3300 805-238-3301 805-238-3302 805-238-3303 805-238-3304						SHEET INDEX ARCHITECTURAL PLANS CIVIL PLANS LANDSCAPE PLANS	D J FLYNN ARCHITECTS, INC. 190 SOUTH GLASSSELL STREET, SUITE 200, ORANGE, CALIFORNIA 92866 TEL: 714-602-9300 FAX: 714-602-9309	CITY PLANNING SUBMITTAL 12/30/13		
LEGAL DESCRIPTION [REDACTED]		VICINITY MAP 	PROJECT GENERAL INFORMATION PROPERTY ADDRESS: [REDACTED] SUBJECT: [REDACTED] PREPARED BY: [REDACTED] DATE: [REDACTED]	DATE: [REDACTED] PERIOD: [REDACTED]						

Exhibit B
Cover Sheet
PD 13-006 & CUP 13-008
(Mullahey Chrysler)



DENNIS J. FLYNN ARCHITECTS, INC.

300 SOUTH STATE STREET
FACILITY: CHRYSLER RAM
300 SOUTH STATE STREET
10000 DALLAS, TEXAS 75219

CHRYSLER JEEP DODGE RAM
PROPOSED SITE PLAN

PASO ROBLES
300 SOUTH STATE STREET
DALLAS, TEXAS 75219

DATE: 08/12/10
PROJECT NO: 10-100-10000

A1.01

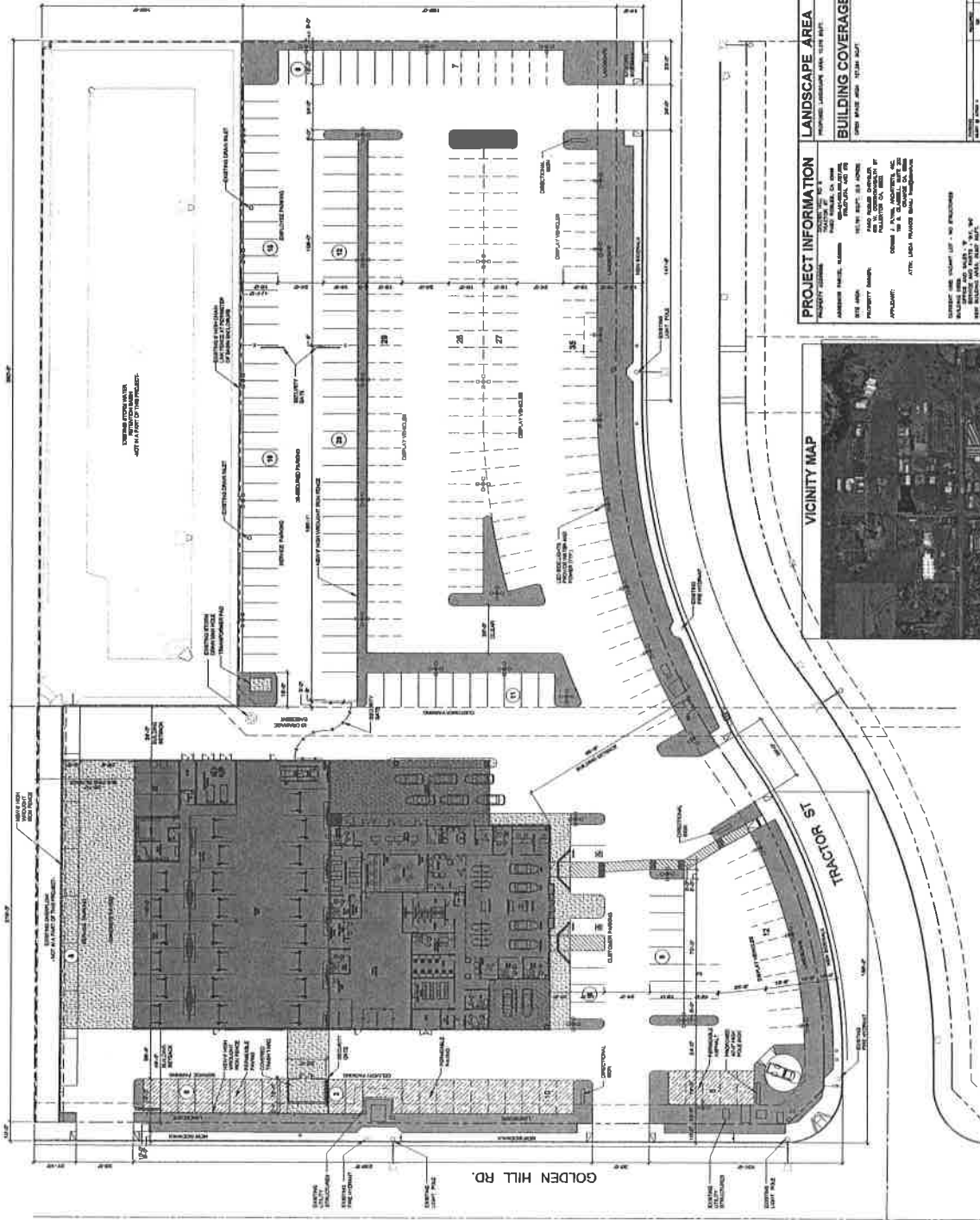


Table with 2 columns: Project Information and Landscape Area. Contains details about building coverage, project address, and site specifications.

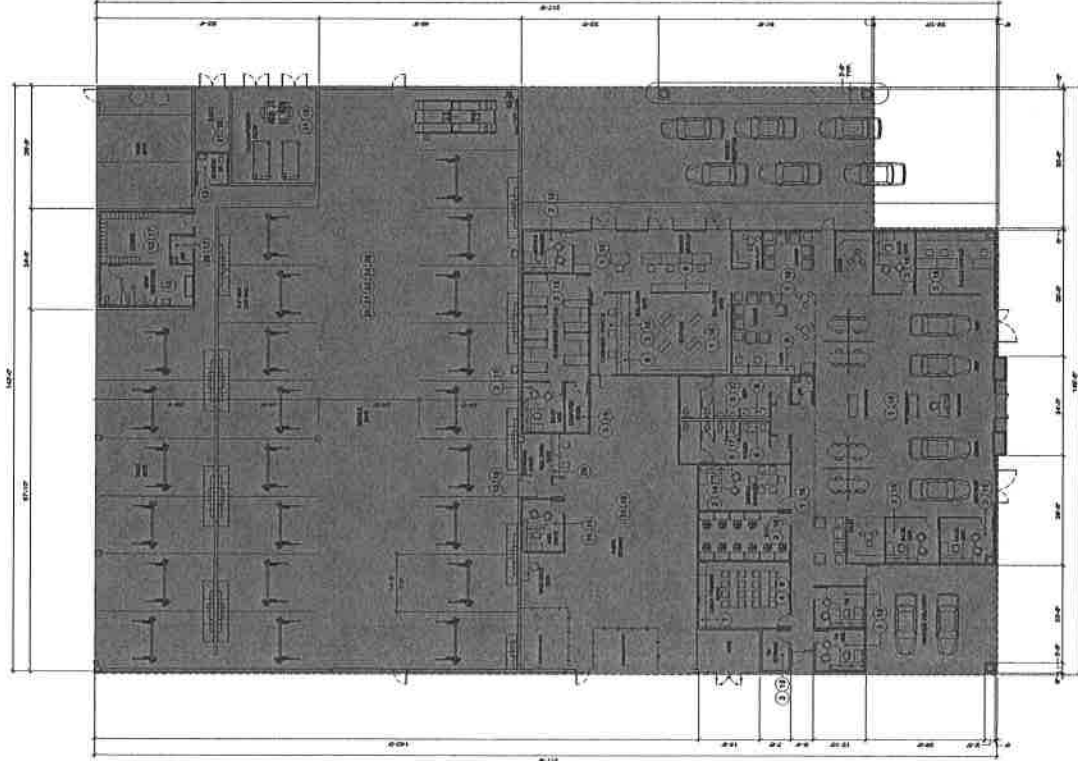


PROPOSED SITE PLAN

Exhibit C
Site Plan
PD 13-006 & CUP 13-008
(Mullahey Chrysler)

INTERIOR FINISHES

- 1) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 2) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 3) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 4) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 5) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 6) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 7) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 8) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 9) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 10) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 11) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 12) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 13) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 14) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 15) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 16) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 17) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 18) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 19) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 20) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 21) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 22) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 23) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 24) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 25) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 26) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 27) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 28) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 29) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 30) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 31) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 32) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 33) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 34) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 35) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 36) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 37) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 38) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 39) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 40) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 41) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 42) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 43) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 44) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 45) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 46) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 47) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 48) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 49) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 50) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 51) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 52) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 53) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 54) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 55) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 56) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 57) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 58) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 59) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 60) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 61) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 62) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 63) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 64) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 65) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 66) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 67) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 68) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 69) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 70) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 71) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 72) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 73) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 74) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 75) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 76) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 77) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 78) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 79) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 80) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 81) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 82) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 83) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 84) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 85) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 86) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 87) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 88) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 89) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 90) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 91) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 92) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 93) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 94) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 95) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 96) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 97) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 98) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 99) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption
- 100) Acoustic Ceiling Tiles, 2' x 2', Acoustic, White, 15% Absorption



PROPOSED FLOOR PLAN

DFA
DENNIS J. FLYNN ARCHITECTS, INC.

100 NORTH BRIDGE STREET
SUITE 200
DALLAS, TEXAS 75201
PHONE: 214.760.1234
FAX: 214.760.1235
WWW.DFA-ARCHITECTS.COM

PROJECT: PASO ROBLES
DATE: 10/15/2013
DRAWING NO.: 103-000-001

CHRYSLER JEEP DODGE RAM
PROPOSED FLOOR PLAN

PASO ROBLES
FRANCHISE
10000 W. BRIDGE STREET
DALLAS, TEXAS 75241
PHONE: 214.343.3333
WWW.PASOROBLES.COM

Exhibit D
Floor Plan
PD 13-006 & CUP 13-008
(Mullahey Chrysler)



DENNIS J. FLYNN
ARCHITECTS, INC.

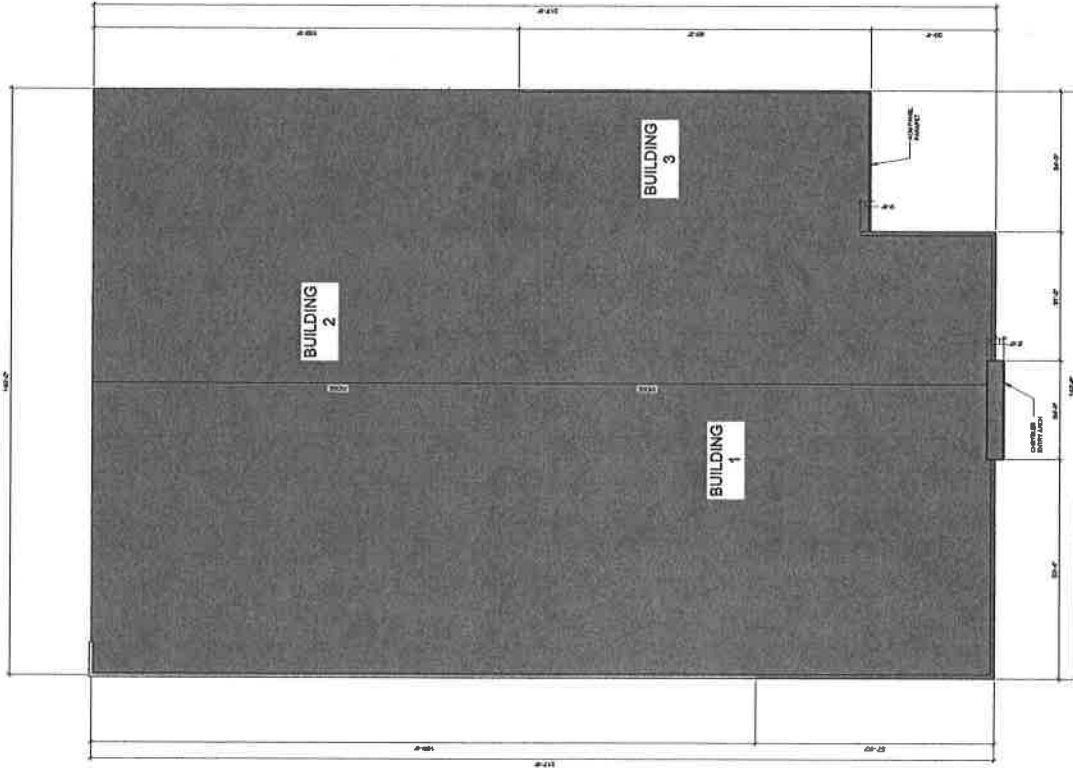
380 SOUTH GUILD STREET
PASADENA, CALIFORNIA 92305
TEL: 626-792-1100
FAX: 626-792-1101
WWW.DJFLYNNARCHITECTS.COM

CHRYSLER JEEP DODGE RAM
PROPOSED ROOF PLAN

PASO ROBLES
PASADENA, CA 92386

DATE: 08/11/10
DRAWN BY: J. FLYNN
CHECKED BY: J. FLYNN
SCALE: AS SHOWN
PROJECT NO: PD 13-006 & CUP 13-008
SHEET NO: 01 OF 02

A2.02



ROOF PLAN

SCALE: 1/8" = 1'-0"

Exhibit E
Roof Plan
PD 13-006 & CUP 13-008
(Mullahey Chrysler)



DENNIS J. FLYNN ARCHITECTS, INC.

OWNER	CHRYSLER JEEP DODGE RAM	
ARCHITECT	DENNIS J. FLYNN ARCHITECTS, INC.	
DATE	08/15/13	
PROJECT	CHRYSLER JEEP DODGE RAM	
LOCATION	PASO ROBLES, CA	
SCALE	AS SHOWN	
DESCRIPTION	ARCHITECTURAL ELEVATIONS	
REVISIONS		
NO.	DATE	DESCRIPTION
1		ISSUED FOR PERMIT
2		REVISED PER COMMENTS
3		REVISED PER COMMENTS
4		REVISED PER COMMENTS
5		REVISED PER COMMENTS
6		REVISED PER COMMENTS
7		REVISED PER COMMENTS
8		REVISED PER COMMENTS
9		REVISED PER COMMENTS
10		REVISED PER COMMENTS
11		REVISED PER COMMENTS
12		REVISED PER COMMENTS
13		REVISED PER COMMENTS
14		REVISED PER COMMENTS
15		REVISED PER COMMENTS
16		REVISED PER COMMENTS
17		REVISED PER COMMENTS
18		REVISED PER COMMENTS
19		REVISED PER COMMENTS
20		REVISED PER COMMENTS
21		REVISED PER COMMENTS
22		REVISED PER COMMENTS
23		REVISED PER COMMENTS
24		REVISED PER COMMENTS
25		REVISED PER COMMENTS
26		REVISED PER COMMENTS
27		REVISED PER COMMENTS
28		REVISED PER COMMENTS
29		REVISED PER COMMENTS
30		REVISED PER COMMENTS
31		REVISED PER COMMENTS
32		REVISED PER COMMENTS
33		REVISED PER COMMENTS
34		REVISED PER COMMENTS
35		REVISED PER COMMENTS
36		REVISED PER COMMENTS
37		REVISED PER COMMENTS
38		REVISED PER COMMENTS
39		REVISED PER COMMENTS
40		REVISED PER COMMENTS
41		REVISED PER COMMENTS
42		REVISED PER COMMENTS
43		REVISED PER COMMENTS
44		REVISED PER COMMENTS
45		REVISED PER COMMENTS
46		REVISED PER COMMENTS
47		REVISED PER COMMENTS
48		REVISED PER COMMENTS
49		REVISED PER COMMENTS
50		REVISED PER COMMENTS
51		REVISED PER COMMENTS
52		REVISED PER COMMENTS
53		REVISED PER COMMENTS
54		REVISED PER COMMENTS
55		REVISED PER COMMENTS
56		REVISED PER COMMENTS
57		REVISED PER COMMENTS
58		REVISED PER COMMENTS
59		REVISED PER COMMENTS
60		REVISED PER COMMENTS
61		REVISED PER COMMENTS
62		REVISED PER COMMENTS
63		REVISED PER COMMENTS
64		REVISED PER COMMENTS
65		REVISED PER COMMENTS
66		REVISED PER COMMENTS
67		REVISED PER COMMENTS
68		REVISED PER COMMENTS
69		REVISED PER COMMENTS
70		REVISED PER COMMENTS
71		REVISED PER COMMENTS
72		REVISED PER COMMENTS
73		REVISED PER COMMENTS
74		REVISED PER COMMENTS
75		REVISED PER COMMENTS
76		REVISED PER COMMENTS
77		REVISED PER COMMENTS
78		REVISED PER COMMENTS
79		REVISED PER COMMENTS
80		REVISED PER COMMENTS
81		REVISED PER COMMENTS
82		REVISED PER COMMENTS
83		REVISED PER COMMENTS
84		REVISED PER COMMENTS
85		REVISED PER COMMENTS
86		REVISED PER COMMENTS
87		REVISED PER COMMENTS
88		REVISED PER COMMENTS
89		REVISED PER COMMENTS
90		REVISED PER COMMENTS
91		REVISED PER COMMENTS
92		REVISED PER COMMENTS
93		REVISED PER COMMENTS
94		REVISED PER COMMENTS
95		REVISED PER COMMENTS
96		REVISED PER COMMENTS
97		REVISED PER COMMENTS
98		REVISED PER COMMENTS
99		REVISED PER COMMENTS
100		REVISED PER COMMENTS

CHRYSLER JEEP DODGE RAM PROPOSED ELEVATIONS

PASO ROBLES
 10000 N. GOLDEN HILL RD.
 PASO ROBLES, CA 93271

PREPARED BY: DENNIS J. FLYNN ARCHITECTS, INC.
 10000 N. GOLDEN HILL RD., PASO ROBLES, CA 93271
 PHONE: (805) 785-1111
 FAX: (805) 785-1112
 WWW: WWW.DJFLYNNARCHITECTS.COM
 DATE: 08/15/13
 PROJECT: CHRYSLER JEEP DODGE RAM
 DRAWING: ARCHITECTURAL ELEVATIONS

A3.01

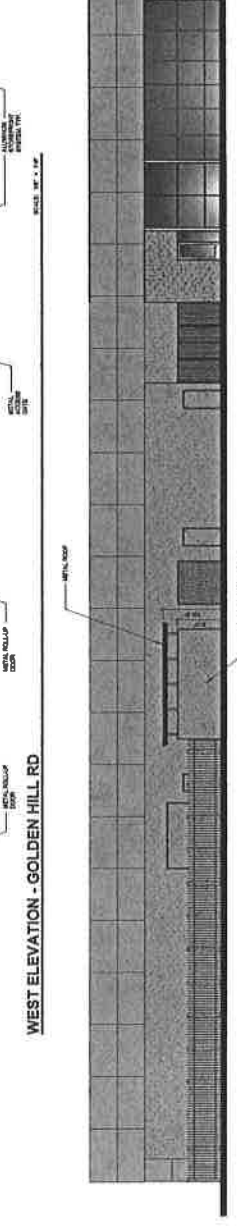
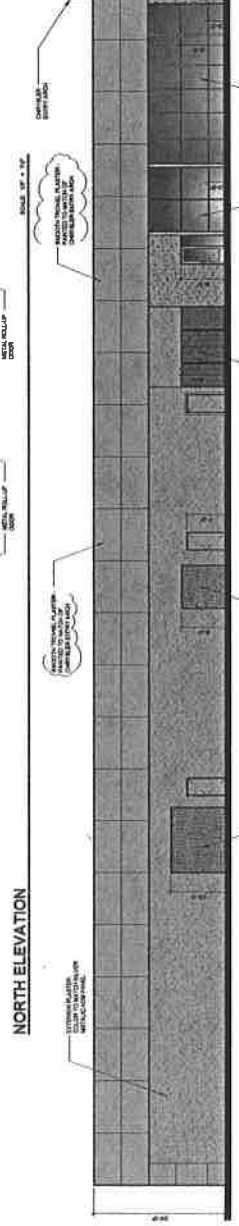
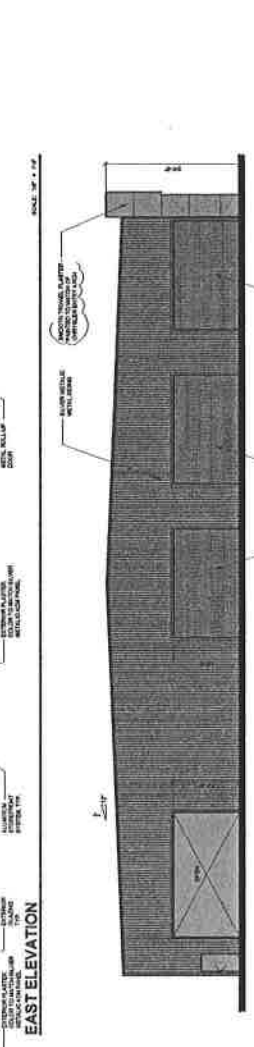
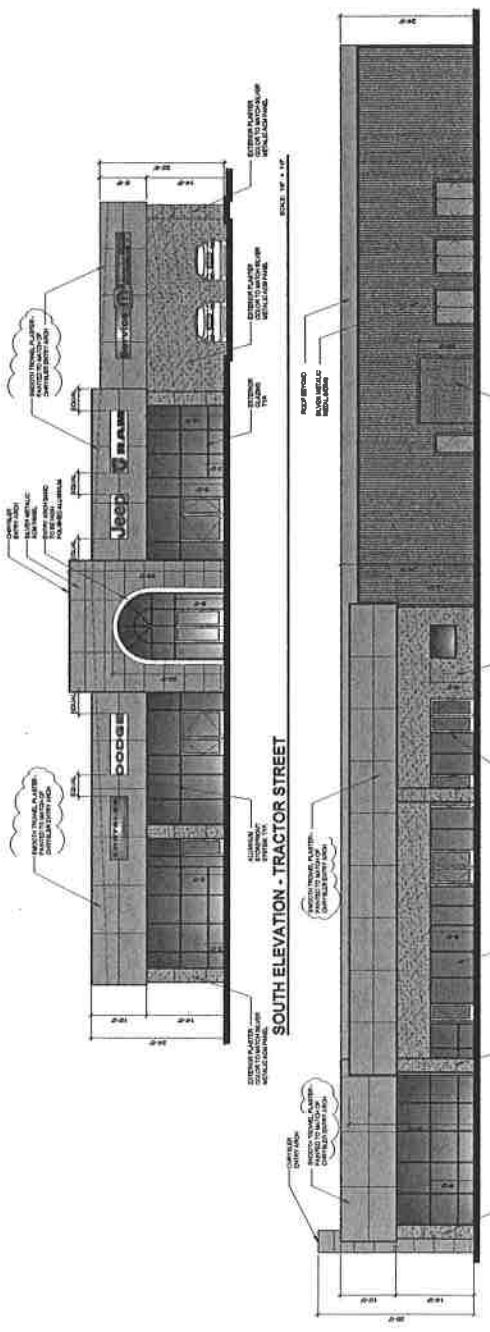
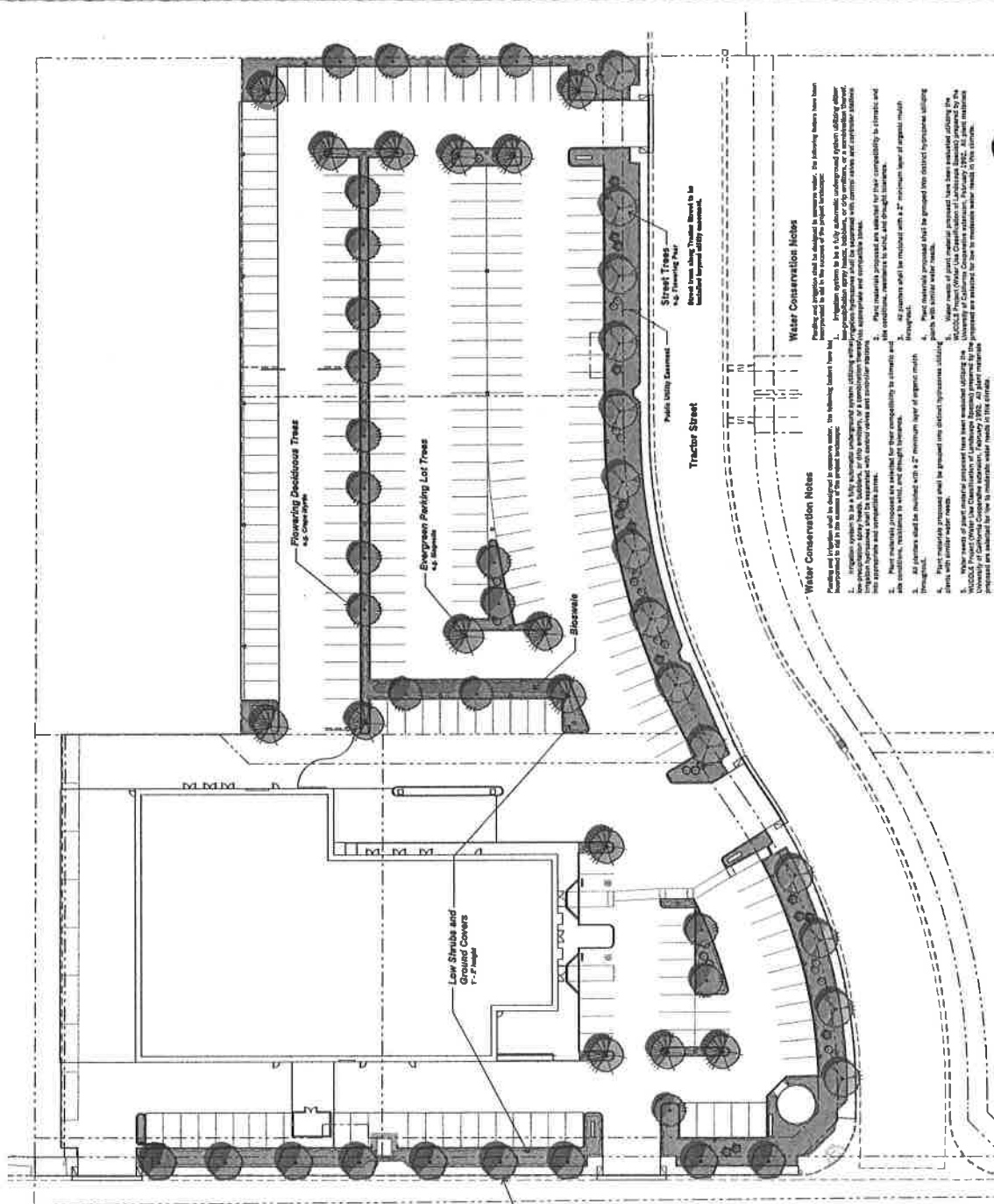


Exhibit F
Architectural Elevations
PD 13-006 & CUP 13-008
(Mullahey Chrysler)

Preliminary Plant List

- STREET TREES**
 - PLANT SPECIES: *Platanus*
 - PLANT SIZE: 12" DBH
- FLOWERING DECIDUOUS TREES**
 - PLANT SPECIES: *Malus, Prunella*
 - PLANT SIZE: 12" DBH
- EVERGREEN TREES**
 - PLANT SPECIES: *Juniperus, Taxus*
 - PLANT SIZE: 12" DBH
- LOW SHRUBS AND GROUND COVERS**
 - PLANT SPECIES: *Phlox, Sedum*
 - PLANT SIZE: 1'-2' Height
- STREET TREES**
 - PLANT SPECIES: *Platanus*
 - PLANT SIZE: 12" DBH



North
Scale: 1" = 20'-0"
firma

Chrysler Jeep Dodge Ram Preliminary Landscape Plan

Preliminary Landscape Plan

Paso Robles, CA

Exhibit G
Preliminary Landscape Plan
PD 13-006 & CUP 13-008
(Mullahey Chrysler)

Low Impact Development (LID) for Stormwater Management

1. LID Goals:
 - a. Reduce runoff volume and peak flow
 - b. Improve water quality
 - c. Enhance aesthetics
 - d. Increase grading and ground disturbance
 - e. Minimize soil disturbance
 - f. Direct runoff to pervious areas
2. LID Planning Principles:
 - a. Reduce Stormwater Runoff
 - b. Reduce Stormwater Velocity
 - c. Reduce Stormwater Volume

Best Management Practices (BMPs) incorporated into the final design include: Street Trees and Street Trees, Foundation Planting, Strategic Planting, and Permeable Pavement.

References:

1. Stormwater Best Management Practices Manual, 2nd Edition, 2000
2. Low Impact Development Handbook, Stormwater Management Strategies, 2000
3. City of Los Angeles Low Impact Development Handbook, 2000
4. City of Los Angeles, January 2009

Water Conservation Notes

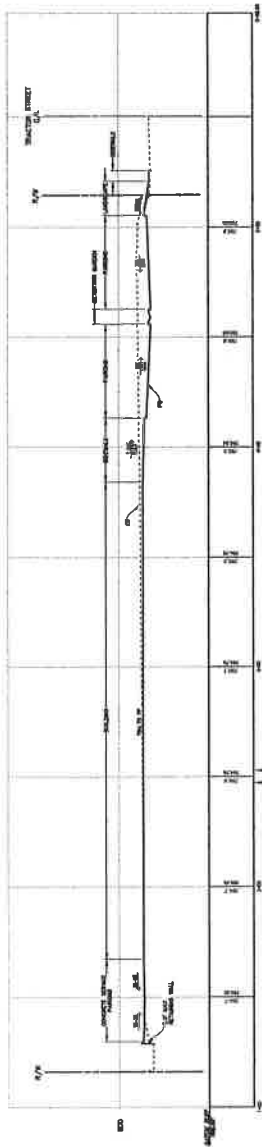
Planting and irrigation shall be designed to conserve water. The following factors have been incorporated to aid in the success of this project landscape:

1. Irrigation systems shall be designed to conserve water.
2. Plant species shall be selected for their ability to tolerate drought conditions.
3. Plant species shall be selected for their ability to tolerate drought conditions.
4. Plant species shall be selected for their ability to tolerate drought conditions.
5. Plant species shall be selected for their ability to tolerate drought conditions.
6. Plant species shall be selected for their ability to tolerate drought conditions.

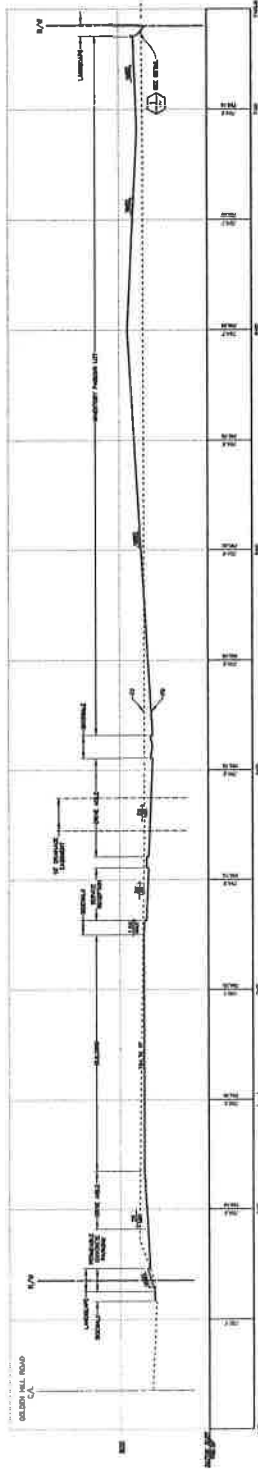
Water Conservation Notes

Planting and irrigation shall be designed to conserve water. The following factors have been incorporated to aid in the success of this project landscape:

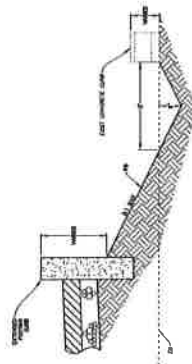
1. Irrigation systems shall be designed to conserve water.
2. Plant species shall be selected for their ability to tolerate drought conditions.
3. Plant species shall be selected for their ability to tolerate drought conditions.
4. Plant species shall be selected for their ability to tolerate drought conditions.
5. Plant species shall be selected for their ability to tolerate drought conditions.
6. Plant species shall be selected for their ability to tolerate drought conditions.



SITE SECTION 1-20



SITE SECTION 1-25



LANDSCAPE SWALE

CHRYSLER JEEP DODGE RAM
PRELIMINARY SITE CROSS
SECTIONS



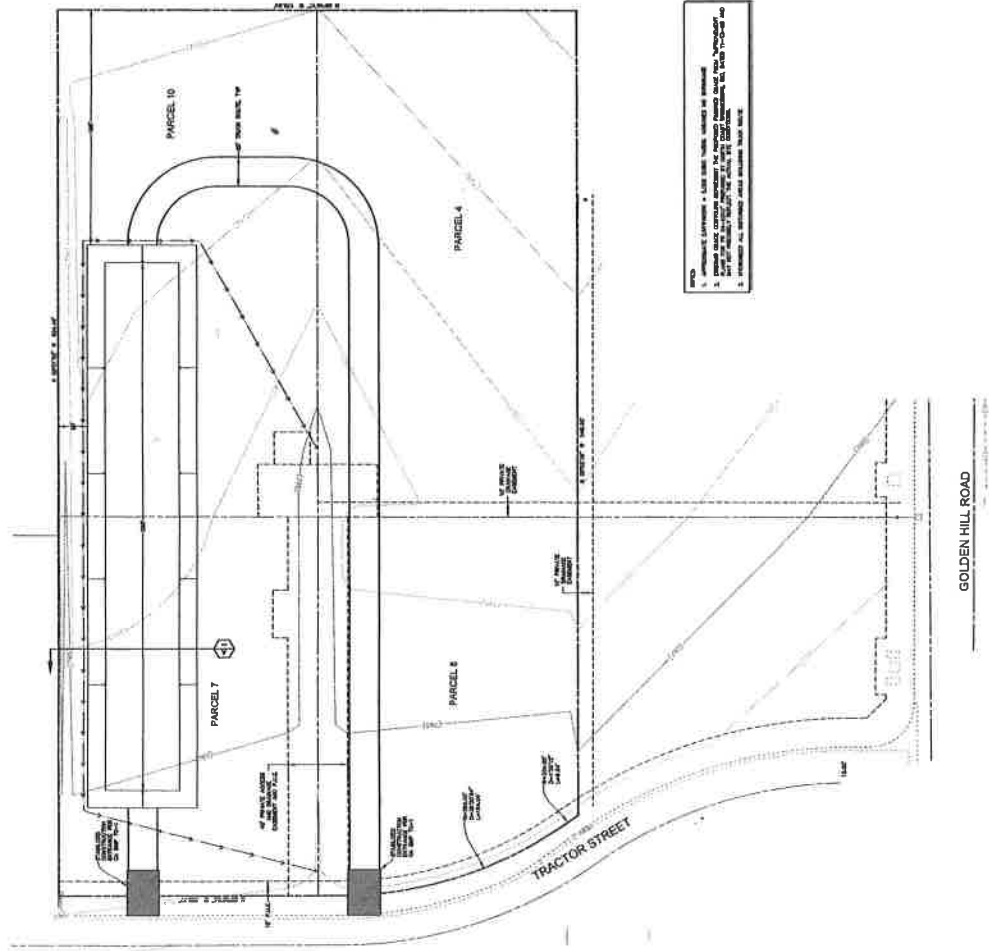
DATE: 10/13/13

Exhibit I
 Site Cross Sections
 PD 13-006 & CUP 13-008
 (Mullahey Chrysler)

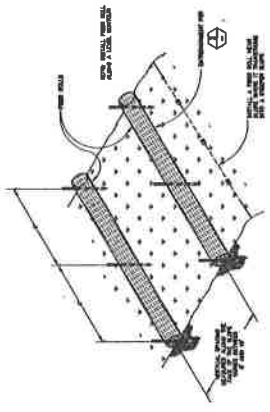
10/13/13 10:00 AM (C:\PROJECTS\13-006\13-006.dwg) PLOT DATE: 10/13/13 10:00 AM

GRADING AND DRAINAGE / UTILITY LEGEND

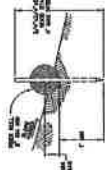
	EXISTING SURFACE		PROPOSED SURFACE
	EXISTING DRAINAGE		PROPOSED DRAINAGE
	EXISTING UTILITY		PROPOSED UTILITY
	EXISTING STRUCTURE		PROPOSED STRUCTURE
	EXISTING FENCE		PROPOSED FENCE
	EXISTING WALL		PROPOSED WALL
	EXISTING ROAD		PROPOSED ROAD
	EXISTING DRIVEWAY		PROPOSED DRIVEWAY
	EXISTING SIDEWALK		PROPOSED SIDEWALK
	EXISTING CURB		PROPOSED CURB
	EXISTING GUTTER		PROPOSED GUTTER
	EXISTING DITCH		PROPOSED DITCH
	EXISTING CULVERT		PROPOSED CULVERT
	EXISTING BRIDGE		PROPOSED BRIDGE
	EXISTING STRUCTURE		PROPOSED STRUCTURE
	EXISTING FENCE		PROPOSED FENCE
	EXISTING WALL		PROPOSED WALL
	EXISTING ROAD		PROPOSED ROAD
	EXISTING DRIVEWAY		PROPOSED DRIVEWAY
	EXISTING SIDEWALK		PROPOSED SIDEWALK
	EXISTING CURB		PROPOSED CURB
	EXISTING GUTTER		PROPOSED GUTTER
	EXISTING DITCH		PROPOSED DITCH
	EXISTING CULVERT		PROPOSED CULVERT
	EXISTING BRIDGE		PROPOSED BRIDGE



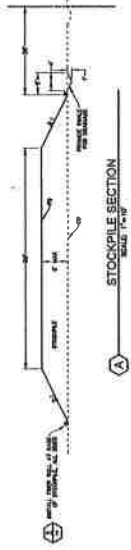
NOTES:
 1. PROPOSED GRADING - SEE THE PLAN SHEETS FOR DETAILS.
 2. SEE THE EXISTING SURFACE FOR THE EXISTING DRAINAGE AND UTILITY.
 3. SEE THE EXISTING SURFACE FOR THE EXISTING STRUCTURES AND WALLS.
 4. SEE THE EXISTING SURFACE FOR THE EXISTING FENCES AND WALLS.



2 TYPICAL FIBER ROLL INSTALLATION



3 TYPICAL FIBER ROLL ENTRENCHMENT



A STOCKPILE SECTION



**CHRYSLER JEEP DODGE RAM
 OFFSITE STOCKPILE AND
 EROSION CONTROL PLAN**



Exhibit J
 Stock Pile - Pre Grading Plan
 PD 13-006 & CUP 13-008
 (Mullahey Chrysler)



SIGN DETAIL

**9 SERIES 10' x 15'
BRAND SIGN at 40' OAH**

Square Footage

- 158.58 ft²

Manufacturing Details

- Painted aluminum exterior construction with chrome accents on cladding.
- Clear acrylic logos pushed thru cabinet face with 1st surface decoration vinyl / color digital print.

Electrical Requirements

- Fluorescent internal illumination (12) F120 / T12 / Coolwhite / High Output Lamps
- (3) Transco Ballast TRA-14-1040EP at 2.6 amps each
- LED external halo illumination (248) Agilight White Tuffrayz
- (3) Advance Xitanium Power Supply LED-120A-0012V-50F at 0.63 amps each
- Total Load: 9.69 amps at 120 V
- (1) 20 amp Circuit Required

Colors

- **Chrysler Badge:** Black ■ Process Black
Blue ■ PMS 287
Silver
- **Dodge Badge:** Red ■ PMS 485
White
Silver
- **Jeep Badge:** Green ■ PMS 371
White
Silver
- **Ram Badge:** Black ■ Process Black
Silver

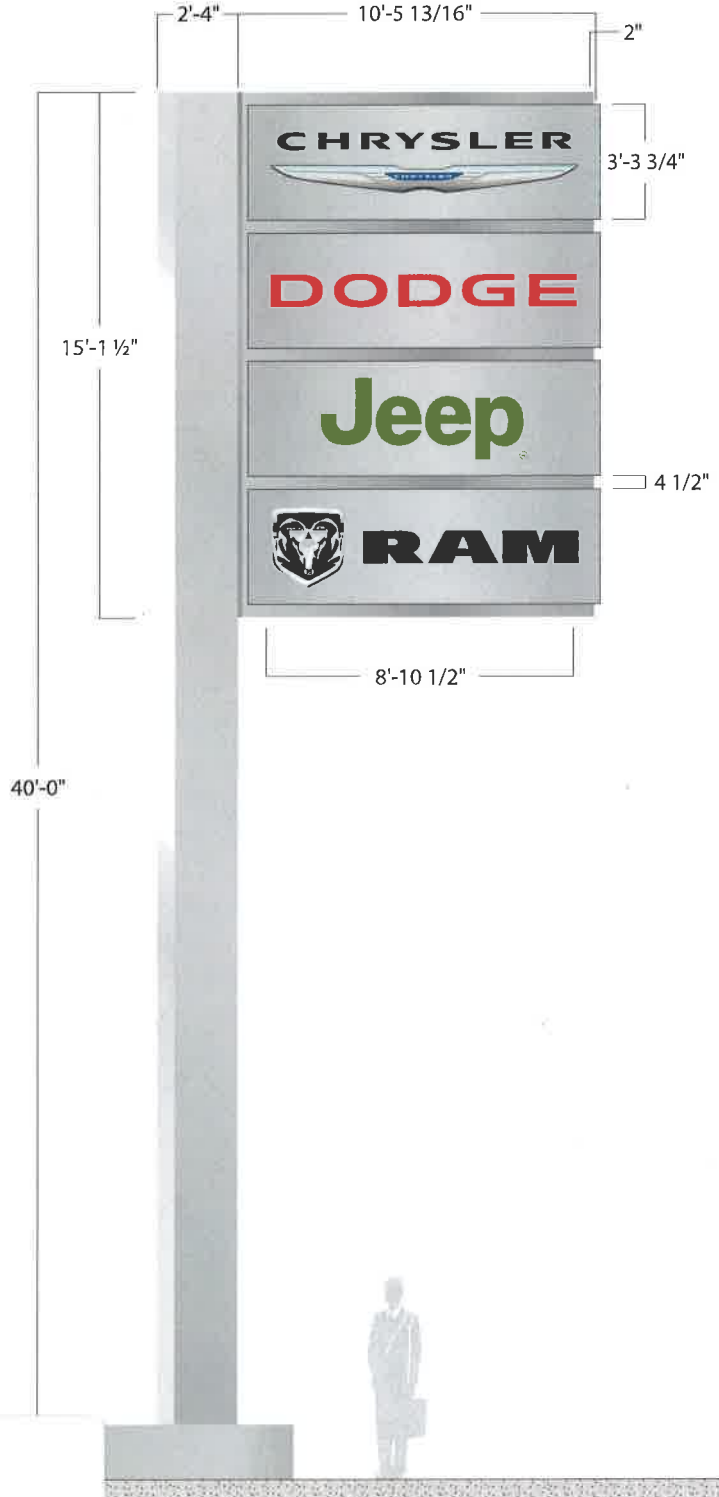


Exhibit K
Highway Oriented Sign
PD 13-006 & CUP 13-008
(Mullahey Chrysler)



CITY OF EL PASO DE ROBLES

"The Pass of the Oaks"

AFFIDAVIT OF MAIL NOTICES PLANNING COMMISSION/CITY COUNCIL PROJECT NOTICING

I, Kristen Buxkemper, employee of the City of El Paso de Robles, California, do hereby certify that the mail notices have been processed as required for Planned Development 13-006 & Conditional Use Permit 13-008 (Mullahey), on this 9th day of January 2014.

City of El Paso de Robles
Community Development Department
Planning Division

Signed: 
Kristen L. Buxkemper