

TO: JAMES L. APP, CITY MANAGER

FROM: RON WHISENAND, COMMUNITY DEVELOPMENT DIRECTOR

SUBJECT: GENERAL PLAN AMENDMENT 09-001, REZONE 09-001, PD 08-001, CUP 08-001, LOT LINE ADJUSTMENT 07-0293 (PASO ROBLES MOTORCOACH RESORT – PASO 160, LLC)

DATE: MARCH 17, 2009

Needs: For the City Council to consider applications filed by North Coast Engineering, on behalf of Paso 160 LLC, to change the General Plan Land Use and Zoning designations of the 160 acre site to accommodate the development of a 332 space Recreational Vehicle (RV) Resort.

- Facts:**
1. The 160 acre site is located at the northern end of Golden Hill Road, on the east side of the road (see Attachment 1, Vicinity Map).
 2. The site currently has a General Plan Land Use designation of Agriculture (AG). The Zoning designation is Residential Agriculture (RA).
 3. The site is located within the Airport Overlay area as indicated on the General Plan Land Use map, with portions of the site located within Airport Safety Zones 4 & 5.
 4. The 160 acre site is currently comprised of three parcels. The applicants are proposing to reconfigure the three parcels with a Lot Line Adjustment (PRAL 07-093), where Parcel 1 would be 58.8 acres, Parcel 2 would be 68.6 acres, and Parcel 3 would be 21.4 acres. (See Attachment 2)
 5. General Plan Amendment 09-001 and Rezone 09-001 propose the following changes:
 - **Parcel 1:** change the zoning designation from Residential Agriculture (RA) to Agriculture (AG), and retain the current AG land use designation;
 - **Parcel 2:** amend the land use designation from Agriculture (AG) to Parks & Open Space (POS) and change the zoning designation from Residential Agriculture (RA) to Parks & Open Space (POS);
 - **Parcel 3:** change the zoning designation from Residential Agriculture (RA) to Agriculture with a Planned Development Overlay (AG-PD), and retain the current AG land use designation.
 6. If the Council chooses to change the General Plan and Zoning, then Planned Development 08-001 & Conditional Use Permit 08-001 proposes the following:
 - **Parcel 1:** this parcel would be dedicated to the City for public use as Open Space;

- **Parcel 2:** establish the 332-space Paso Robles Motorcoach Resort;
 - **Parcel 3:** limit future uses to agricultural related uses such as winery, wine tasting and vineyards.
7. Planned Development 08-001 along with CUP 08-001 would establish the architectural, landscape and site development standards to demonstrate how the RV resort is proposed to be developed on Parcel 2.
 8. Pursuant to the Statutes and Guidelines of the California Environmental Quality Act (CEQA) and the City's Procedures for Implementing CEQA, an Initial Study and Mitigated Negative Declaration was prepared and circulated for public review and comment. Based on the information and analysis contained in the Initial Study (and comments and responses thereto), a determination has been made that potentially significant environmental impacts can be mitigated to a less than significant level, and that with implementation of mitigation measures that the PR Motorcoach project may qualify for issuance of a Mitigated Negative Declaration (MND).
 9. The Planning Commission considered this General Plan Amendment, Rezone and RV Resort at their meeting on February 24, 2009. The Commission made a recommendation to the Council to approve the change in the General Plan, Rezone and the RV Resort project on a 4 to 1 vote (1 Commissioner abstained and 1 was absent).

Analysis

And

Conclusion: When reviewing the Paso Robles Motorcoach project, the Council should first consider the impacts associated with the change in the land use and zoning designations. In other words, does the Council feel agricultural land use is no longer applicable to this area of town. Impacts and concerns over the change in land use that were brought up at the Planning Commission hearing on February 24, 2009, include traffic, water supply, drainage/flooding, natural resources, agriculture and land use compatibility. The main policy question before the Council that should be considered is whether the change in land use and zoning designations would create different or otherwise unacceptable impacts from land uses that would be permitted if the property were left in agriculture.

A discussion of potential impacts to services and resources related to the proposed amendments as well as discussion of the proposed development project is provided below.

Land Use:

The surrounding land use designations are Parks and Open Space to the east, Business Park to the south, and Public Facilities and Business Park to the northeast. Rural Residential development is located immediately west of the site, and Agriculture is directly to the north, both of which are in the County. Existing uses adjacent to the project site include commercial/light industrial, low-density single-family homes, open space, grazing, and vineyards.

Examples of permitted uses in Parks and Open Space zoning include agricultural uses and facilities, wholesale nurseries, wine-tasting rooms, convention centers, and public parks. Conditionally permitted uses in the POS zone include, but are not limited to, equestrian facilities, wineries, golf courses, ball fields, recreational vehicle parks, hotels, and motels. Conditionally permitted uses are uses that may be compatible and consistent with the specific zoning district but for which potential impacts that may result from specific uses can be addressed through implementation of conditions of approval applied to development.

The City's Agricultural (AG) zone permits (and conditionally permits) a broad range of (generally) agricultural land uses with the intent of the AG zone to support agriculture, and agricultural related uses. The General Plan purpose and intent statements of the AG and POS land use designations are provided below.

Agricultural Land Use Designation

Purpose: To allow and protect the operation of agricultural uses; to provide open space; to provide a viable land use in areas impacted by airport operations.

Parks and Open Space Land Use Designation

Purpose: To provide a category for public and private properties to be used only for open space and recreation.

Agricultural zoning allows nearly the same types of uses as Parks and Open Space (POS) with a few exceptions. The AG zone does not permit recreational vehicle parks, however RV parks are permitted in the POS zone. The relative differences in impacts between typical AG or POS land uses are minor. Since these zones are somewhat similar in nature and permitted uses, it is through the discretionary project review process that specific impacts related to particular projects can be identified.

RV Resort Project:

The RV Resort project should only be considered if the Council first determines that it is appropriate to change the General Plan and Zoning. Required City approval will include a Conditional Use Permit and Planned Development.

The property has 3 parcels, which are proposed to be modified slightly with a Lot Line Adjustment to conform to the project design. The development of the RV resort is proposed on Parcel 2. The RV park is proposed to include 332 spaces and a few community buildings. The project is intended to be designed in five phases (see Phasing Plan in PD Resolution, Attachment 9, Exhibit E), therefore, not all of the development will occur at once, and improvements such as the extension of Golden Hill Road would occur with later phases in accordance with the Phasing Plan and as conditioned and determined needed by the City Engineer.

The RV spaces are proposed along curving roads with the closest site to Golden Hill Road being set back approximately 250 feet. The nearest RV spaces to residences to the west would be about 450-500 feet. The project proposes minimal grading and integrates the

existing oak trees. The individual RV spaces are designed to only have enough paving (through pervious paving or brick pavers) for a coach and tow-car. Trails and walkable roads connect the spaces to a number of recreational and community and gathering areas. Site lighting is proposed to use downcast fixtures on buildings, and bollard standards for driveways and pathway lighting to keep lighting to minimum.

The project has been re-designed to screen views from neighbors with a combination of . existing oak trees, a decorative masonry screen wall on top of a berm, and the planting landscaping between the wall and Golden Hill Road. A line of site analysis was prepared of potential views of the RV coaches from Golden Hill Road.(See Attachment 9, Landscape Plans)

To buffer the RV spaces from adjacent uses, the applicants are proposing to re-designate Parcel 3 as Agriculture. The applicant anticipates future use of this parcel for vineyard planting, wine tasting, boutique hotel, a restaurant and other ancillary support uses for this area.

The main project and compatibility issues include:

Traffic A traffic study was prepared for the Paso Robles Motorcoach Resort, dated January 11, 2008 (The Traffic Study is available in the Community Development Department as well as on the City's website, under this Council Item). It incorporates data and analyses included in the study for the Regency Center dated August, 2007, since this study includes existing and cumulative traffic projections for the area in including traffic related to Highway 46 East. The RV park traffic study includes this information plus project specific traffic generation. Included in the background of both of those studies are state highway growth projections, produced by Caltrans in early 2007, that indicate that all of the intersections along the Highway 46 East corridor will deteriorate below Level of Service (LOS) D.

Development of this property will impact the intersection of Highway 46E and Golden Hill Road. It is anticipated that the Regency Center will complete work on improvements to that intersection within the next six months (prior to occupancy of the center), and that these improvements will bring the level of service (LOS) of the intersection into conformance with the General Plan threshold requirement of LOS D. With the intersection improvements complete the motorcoach project will not exceed the intersection beyond LOS D.

Development of this property will be subject to conditions to extend Golden Hill Road (identified as a key parallel route in the 2003 General Plan), and to pay AB 1600 transportation impact fees. The impact fees will represent the project's share towards the road extension across the Huer Huero Creek to connect Golden Hill Road to Dry Creek Road, or if a more appropriate parallel connection is identified, the fees will be based on and used for improvement(s) yet to be determined.

Water Use The proposed General Plan Amendment and project will not negatively impact the City's water resources. If the Council approves the GPA for this property, there will be

a requirement to provide municipal water for the future uses of the property. A water connection fee will cover the project share of Nacimiento water.

A brief water use analysis between use of the site for a vineyard and an RV park was provided by the applicant. It indicates that the proposed RV park with POS zoning would use less water than if the site were used for agricultural uses such as vineyards. This analysis was based on information from the local vineyard industry and a locally operated RV park.

Drainage Flooding and drainage has long been an issue for the Circle B neighborhood. The project can be designed to mitigate resort generated drainage. The City is obligated by the Regional Water Quality Control Board to require that all projects implement Low-Impact Development (LID) storm water drainage practices to mitigate impacts to water quality from storm water run-off, and to reduce the rate and volume of storm water run-off to the maximum extent practical. Drainage from the development project is largely controlled naturally since a majority of the site slopes and drains toward the Huer Huero Creek. On-site bio-swales are planned on the west side of the property to catch water that would otherwise leave the site. This water will be put into a retention pond and be designed to allow percolation back down through the soil.

Natural Resources

- **Oak Trees**

A & T Arborists prepared an Arborist Report for the project site, which includes an inventory and survey for 336 oak trees that are located within the development area of the project (Parcel 2 and the Golden Hill Road right of way). There are over 600 oak trees located on the total site (Parcels 1, 2 & 3). A total of 33 oak trees are proposed to be removed to accommodate this project and the extension of Golden Hill Road. Five of the trees are already dead, and most of the trees proposed to be removed are in poor to very poor health. The development will be required to mitigate the tree removals and replant new oak trees in compliance with the City's Oak Tree Ordinance. The Arborist report is attached to the Initial Study.

- **Biology**

A Biology Report was prepared in December 2006 and updated in January 2008. The report identified that the project site has six designated habitat types including annual grassland, blue oak woodland, riparian, ephemeral drainage, vernal pool, and seasonal wetland. That there are more than 108 animal species, including 11 special status species, have the potential to occur on the project site, including documented active Golden Eagle nesting onsite (2 adults and 1 juvenile). The Golden eagle is a fully protected species under the California Endangered Species Act (CESA), and they are federally protected.

The property is within a strategic section of the San Joaquin Kit Fox (SJKF) migration corridor. Based on consultation with the U.S. Fish and Wildlife Service and California

Department of Fish and Game, the applicant modified the site development plan to reduce the area of impact to the San Joaquin Kit Fox habitat and migration corridor. Additionally, Althouse and Meade provided a supplemental letter (See Exhibit E-2) providing additional on and off-site mitigation measures in addition to the standard kit fox mitigations.

The applicant has also consulted with the agencies regarding an existing Golden Eagle nest and vernal pool habitats. Additional site construction and operational mitigations (i.e. avoidance) were submitted to protect them from potential impacts. These mitigation measures are provided in the Mitigated Negative Declaration resolution, see Attachment 6.

- **Agriculture:**

Based on an analysis of agricultural soils contained in the General Plan EIR, the underlying soil on this property is not prime, of statewide importance, or unique farmland. Historical and existing agricultural uses include cattle grazing on the project site, and there are vineyards north of the property.

While not prime agricultural soils, the project would result in elimination of approximately 70 acres of agriculturally designated land that could support viable agricultural uses. The 2003 General Plan includes specific goals and policies directing protection of agricultural resources. If the Council supports conversion of this property to POS there are a couple options available to address loss of agricultural resources and to regulate appropriate land uses:

- 1. Permanent Agricultural Conservation Easement**

An agricultural conservation easement could be established on parcels 1 and 3 since they are proposed to be re-designated and rezoned to Agriculture, with Parcel 1 dedicated to the City. Conservation easements are typically managed by third party organizations, such as the Land Conservancy of San Luis Obispo County (Land Conservancy). They are voluntary agreements negotiated with willing property owners. Specific uses and areas to be precluded from development are determined through negotiation between the owners, the City and the Land Conservancy. The following excerpt is from the Land Conservancy's website.

A Conservation Easement is a legal document that is recorded on the deed of the conservation property that protects the resources on that property. Each conservation easement is tailored specifically to meet conservation and landowner needs. There are many benefits to Conservation Easements including the maintenance of agricultural viability and several types of tax advantages for landowners. Tax benefits include charitable deductions on income tax and reduced estate values for estate planning. In some cases property taxes may be lowered, but this is not always the case and is never guaranteed.

- 2. Zoning Overlay**

As noted above, the applicant is interested in limiting the types and location of future land uses for Parcel 3. An alternative means to do this would be to establish

a Planned Development Overlay (PD Overlay) district on the property. Specific land uses could be permitted with a PD Overlay to enable future development and preclude undesirable land uses. However, the City's experience with this technique in other circumstances, such as the Kiessig property, is that agriculturally zoned land can be changed over time.

Policy

Reference: General Plan Land Use Element; General Plan Update EIR certified in 2003; Zoning Code and California Environmental Quality Act (CEQA), Airport Land Use Plan, Economic Strategy.

Fiscal

Impact: None

Options: After consideration of all public testimony, that the City Council may choose the following options, and in chronological order:

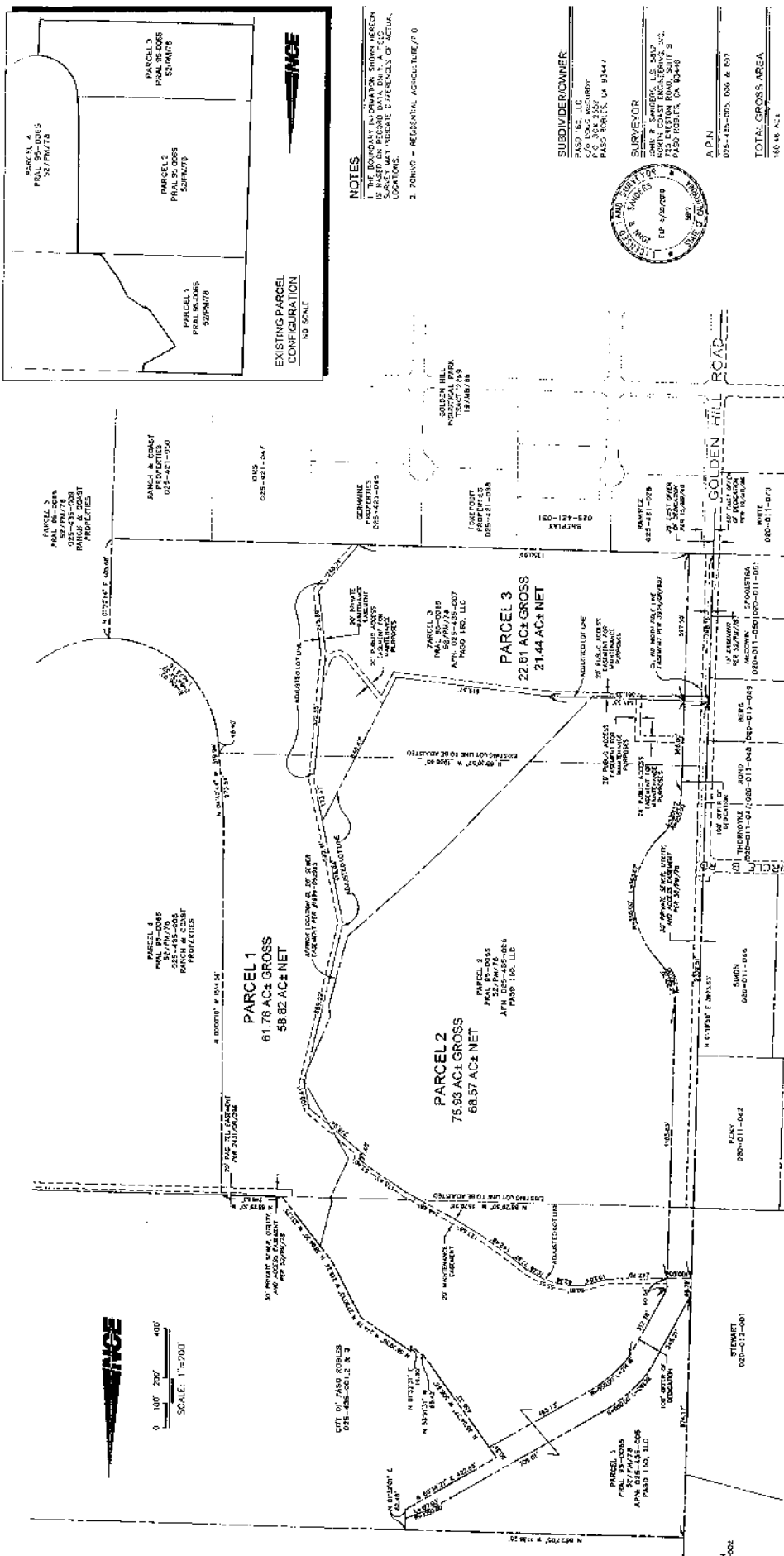
- a.
 1. Adopt a Resolution approving a Mitigated Negative Declaration for the project;
 2. Adopt a Resolution adopting GPA 09- 001;
 3. Adopt an Ordinance to rezone the project site for Rezone 09-001;
 4. Adopt a Resolution adopting PD 08- 001, subject to site specific and standard conditions of approval;
 5. Adopt a Resolution adopting Conditional Use Permit 08-001, subject to site specific and standard conditions of approval;
 6. Adopt a Resolution adopting Lot Line Adjustment 07-0293, subject to site specific and standard conditions of approval; or
 7. Adopt a Resolution allowing the removal of 36 oak trees, subject to the planting of replacement trees as required by the Oak Tree Ordinance; or
- b. Amend, modify or reject the foregoing option.

Prepared by: Darren Nash, Associate Planner

Susan DeCarli, Planning Mgr.

Attachments:

1. Vicinity Map
2. Lot Line Adjustment and Proposed Land Use and Zoning Map
3. City Engineer Memo
4. Letter from Victoria Berg, received by e-mail on March 5, 2009
5. Applicant's Project Description
6. Resolution approving a Mitigated Negative Declaration
7. Resolution approving a GPA
8. Ordinance adopting Rezone for 1st reading
9. Resolution approving PD
10. Resolution approving CUP
11. Resolution approving LLA
12. Resolution approving oak tree removals
13. Newspaper and Mail Notice Affidavits



EXISTING PARCEL CONFIGURATION
NO SPLIT

PARCEL 1
PRAL 30.0085
52.9M/78

PARCEL 2
PRAL 30.0085
52.9M/78

PARCEL 3
PRAL 30.0085
52.9M/78

NCE

NOTES

1. THIS PLAN IS A PRELIMINARY PLAN AND IS BASED ON RECORD DATA AND FIELD SURVEY. SURVEY MAY REVEAL DIFFERENCES IN LOCATION.

2. ZONING = RESIDENTIAL AGRICULTURE (R 9)

SUBDIVIDER/OWNER:
PASO ROBLES, LLC
270 VIA CALIFORNIA
PASO ROBLES, CA 93447

SURVEYOR:
JOHN P. SANDERS, L.S. 5817
270 VIA CALIFORNIA
PASO ROBLES, CA 93447

APN:
088-143-000, 006 & 007

TOTAL GROSS AREA:
163.48 AC±

PASO ROBLES MOTORCOACH RESORT
TENTATIVE LOT LINE ADJUSTMENT
PRAL 07-0293

IN THE CITY OF PASO ROBLES, COUNTY OF SAN LUIS OBISPO, STATE OF CALIFORNIA, BEING A LOT IN A SUBDIVISION BEING PART OF AND 3 OF PARCEL MAP PRAL 95-0065, RECORDED IN BOOK 52 OF PARCEL MAPS, AT PAGE 78 IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

NCE NORTH COAST ENGINEERING, INC.
555 E. Folsom St., Suite 111, Paso Robles, CA 93426
PHONE: (805) 238-2222
FAX: (805) 238-2223
WWW.NCE-ENGINEERING.COM

GPA 09-001 (Land Use)
Parcel 1: Remain as AG-AP
Parcel 2: Change from AG-AP to POS-AP
Parcel 3: Remain as AG-AP

Rezone 09-001 (Zoning)
Parcel 1: Change from RA-PD to AG-AP
Parcel 2: Change from RA-PD to POS-AP
Parcel 3: Change from RA-PD to AG-PD-AP

Attachment 2
LLA/Proposed Land Use & Zoning Designations
GPA 08-001, Rezone 08-001, PD 09-001
LLA 07-0293 & CUP 08-001
(PR Motorcoach)

MEMORANDUM

TO: Darren Nash
FROM: John Falkenstien
SUBJECT: PD 08-001 Paso Robles Motorcoach Resort
DATE: February 11, 2009

Grading, Drainage and Storm Water Quality

The City is obligated by the Regional Water Quality Control Board to require that this project implement low impact development best management practices to mitigate impacts to the quality of storm water run-off and to limit the increase in the rate and volume of storm water run-off to the maximum extent practical.

As the City continues to negotiate a long-term hydro-modification mitigation strategy with the Regional Board, standards for control of storm water run-off will change. The mitigation measures provided on the preliminary grading and drainage plans are consistent with anticipated standards. More grass lined swales in lieu of curbs and gutters, and other LID practices may be required.

Traffic

The traffic study dated January 11, 2008, is based primarily on information found in the study for the Regency Center produced in August, 2007. These studies indicate that all of the intersections along the Highway 46E corridor will deteriorate below acceptable levels as defined by the current circulation element of the General Plan.

The City is in the process of developing a new City-wide traffic model for the purposes of updating the circulation element. In addition to verification of current studies, the model will test the effectiveness of a series of road connections not currently included in the Circulation Element. These include parallel routes that may help mitigate impacts of City developments on highway 46E and improved access across the highway, most likely at Union Road.

The Motorcoach traffic study references the City's on-going studies and indicates that the applicant should participate in the projects that are recommended. Participation typically occurs through impact fees. In accordance with General Plan policy, the City Council will update the traffic mitigation fees in accordance with an updated Circulation Element. The applicant should be aware that mitigation fees are collected upon occupancy in the amount in effect at that time.

Streets

Typically all development projects construct improvements to adjacent streets in accordance with the Circulation Element. In this case the Circulation Element provides for the extension of Golden Hill Road north to Dry Creek Road. A bridge over the Huer Huero is needed for this connection. The applicant has submitted a preliminary design for the northerly extension of Golden Hill Road. Due to topographic constraints and projected traffic demands the northerly extension of Golden Hill Road is designed as a two lane road with bike lanes. The road will be constructed in phases along with the resort development.

Attachment 3

City Engineer Memo

GPA 08-001, Rezone 08-001, PD 09-001 *Agenda Item #2 Page 11 of 180*

LLA 07-0293 & CUP 08-001

(PR Motorcoach)

Sewer and Water

Sanitary sewer is available to the project from the City's main trunk line traversing the site and serving the airport area.

Water is available from a 12-inch water main in Golden Hill Road. This main will be extended northerly as Golden Hill Road is developed.

Floodplain

Portions of the project are located in an A Zone as indicated on Flood Insurance Rate Maps prepared by FEMA. In an A Zone no base flood information has been determined. In accordance with the City's floodplain ordinance the applicant will be required to submit a floodplain study prepared by a civil engineer demonstrating that the placement of fill along the Huer Huero will not encroach into the floodway or substantially raise the height of flooding on adjacent properties. The study must state that the project will comply with all provisions of the City's floodplain ordinance.

Recommended Conditions

Low impact development best management practices shall be incorporated into the project grading plans in accordance with City standards at the time of permitting.

The project will be subject to traffic impact and other development impact fees in effect at the time of occupancy of the project. Traffic mitigation will include the deposit of \$1,600,000 towards construction of a bridge over the Huer Huero proportionally applied to the incremental development of the project. When transportation impact fees are updated by council action, the final fee obligation shall be equal to that defined by the new fee structure.

Golden Hill Road shall be constructed in general conformance to the preliminary plans, plans approved by the City Engineer, and in accordance with the phasing plan proposed.

A 12-inch water main shall be extended in Golden Hill Road in accordance with plans approved by the City Engineer and in accordance with the phasing plan proposed.

Prior to development of phases 5, 6, 7 and 8, the applicant shall provide a floodplain study prepared by a civil engineer demonstrating compliance with the City's floodplain ordinance.

February 22, 2009

Dear Planning Commission:

I am a resident of 3649 Golden Hill Road in Paso Robles. I live directly across from the proposed recreational vehicle project which is on your agenda February 24th, 2009. I am opposed to the agenda item regarding proposed zoning changes and an RV project for many reasons, and will list these with supporting statements from the city of Paso Robles strategic plan (2006).

TRAFFIC: The long-term effects of this proposal are in opposition with Paso Robles city's commitment to environmental responsibility, corporate responsibility, and positive outcomes from long-term investment. Short-term revenue generated from this project will exacerbate, not improve, existing problems in the years to come.

1. Traffic generated by recreational vehicles, and vehicles they may tow, will profoundly impact existing traffic problems on Golden Hill Road and the vicinity.
2. Traffic studies attached to the agenda use old data from the 2006 GHRC traffic study to generate proposed mitigations and are invalid.
3. The average daily trip increase from the project to 1224 ADT on Golden Hill is unacceptable, and exceeds Tier I air quality specifications for the APCD (Exhibit C).
4. Golden Hill Road extension to meet Dry Creek Road should not be funded by increasing the traffic problem, with developers paying to "mitigate". The extension should exist to reduce current problems, and alternate funding sources used.
5. Paso City cannot be assured that suggested traffic and air quality mitigations outlined in the APCD report, such as shuttles from the project to town, will be used by patrons of the project, even if these are available.
6. Patrons driving their towed vehicles into the city of Paso Robles will negatively impact already limited parking availability.
7. There is little government money available to fix existing roads and highways. It is unrealistic to think the developers will shoulder the expense of authentic mitigation for existing problems as their project will add to these problems.
8. The RV park traffic will impact on Highway 46. How will Caltrans support the traffic increase when they have not fixed existing problems?

Attachment 4

Victoria Berg Letter

GPA 08-001, Rezone 08-001, PD 09-001 *Agenda Item #2 Page 13 of 180*

LLA 07-0293 & CUP 08-001

(PR Motorcoach)

9. Is Caltrans in agreement with Paso city's "Alternate Route Study" and on board with funding repairs and maintenance to 46?
10. A completed "Alternate Route Study" with Caltrans approval for their involvement should be attained before the RV project is proposed.

POLLUTION: The pollution generated by the RV project is in conflict with the city's strategic plan to support compact development, corporate responsibility and environmental responsibility. It brings harm to the environment and public health. It will negatively impact a "distinctive" community neighborhood, consisting of a gated community; surrounding homes zoned RA, and neighborhoods of tract homes in the vicinity.

1. "The project may exceed the APCD Tier 1 threshold of significant air quality impacts requiring mitigation." (Exhibit C).
2. The noise, glare and air pollution generated from this project, as planned, cannot be mitigated.
3. Average daily trips (ADT) calculated in the traffic studies may not have included trips from towed vehicles attached to the RV's.
4. There are parcels and existing RV park development outside the city limits more suitable for RV parks, which would decrease pollution factors of light glare, noise and vehicle emissions in proximity to our existing residential development and the surrounding area.
5. Are the existing hotel and resort projects approved by the city and now built filled to capacity? Patrons of La Quinta, Hampton Inn, Holiday Inn Express, etc, may drive their vehicles to Paso. Where will these vehicles be parked?
6. How do emissions from RV's compare to those of other vehicles? As RV's consume much more fuel to operate, common sense would indicate the negative air-quality affects are much greater.
7. I am concerned about the impact of air quality during the development of this project and the subsequent RV traffic. I am not aware of the outcome of studies to determine naturally occurring asbestos and dust from currently approved projects on Golden Hill, and how these are being "mitigated". The RV project may have similar affects, exceeding Tier I, as described in the APCD reports attached to the Feb. 24th meeting agenda. The ongoing emissions from RV's will affect us permanently, and as my daughter and I have been diagnosed with asthma, I am opposed to living in proximity to greater air pollution.
8. The extreme light glare from existing development on Golden Hill (ex. the Business Park) has already changed the "night sky" visibility in our neighborhood and the surrounding environment. The city has not protected the existing residential development from the impacts of approved projects. Many already

approved projects have not yet been built, so the cumulative effects of light glare, noise and pollution from existing approved developments is not determined.

9. The city already approved lighting systems in the business park which were much too bright for the surrounding area. Lights or glare the proposed RV Park are inconsistent with existing neighborhoods.
10. The noise and vehicle emissions from travel on the extension of Golden Hill will be a significant impact on our residential neighborhood without adding a minimum of 1224 vehicle RV trips per day.
11. Paso Robles is already a city in San Luis Obispo County with worsening air quality in the summer months, based on newspaper reports. I am opposed to a business which exacerbates this problem, when the city's stated plan is to attract businesses that do not harm the environment or public health.

Natural Resources: The proposed zoning changes and RV Park are environmentally irresponsible. Wildlife, flora and fauna and water resources are irreversibly and negatively affected, not improved.

1. Proposed wildlife habitat mitigations are inappropriate. The scale and nature of this project do not enable peaceful coexistence, but destroy the environment to a large extent.
2. As everyone knows, despite the addition of water from the Nacimiento pipeline, no one is certain of the available supply.

Thank you for your consideration.

Sincerely,

Victoria Simon Berg

PASO ROBLES MOTORCOACH RESORT

12-16-08

Introduction

The City of Paso Robles currently has one Recreational Vehicle Resort, Wine Country RV Resort located on Airport Road. The highly popular resort, consisting of 166 spaces, serves visitors to the Paso Robles area and has been in business since 2002.

Based on the feedback the owners received from the RV community, it became apparent that there was a demand for additional RV Resorts to serve the Paso Robles area. Particularly noticeable was the need for a very high quality resort that could cater to larger RVs and more affluent travelers.

A 160 acre site at the north end of Golden Hill Road seemed to be the perfect place to create such a resort in Paso Robles. The rural ambiance of gently rolling hills, studded with oaks, connecting with the Huer Huero Creek was a fit for a resort on this property. The site provided ample areas for high quality and spacious RV sites, while respecting the existing environment.

What is a Motorcoach Resort?

Motorcoach Resorts differ from regular RV parks by catering to the upper end of the RV traveling population. Only the high-end motorcoaches (or class A coaches over 35 feet in length) will be granted entry into the resort. Individual sites are improved to a point of perfection; landscaping is meticulously manicured and clear rules maintain the order and exquisite ambiance of the resort. Sites are larger, more individualized and typically have more privacy and physical amenities. The visitors tend to be successful, wealthy, highly educated and well traveled. Some are retired, but many work with a relaxed or flexible work lifestyle.

Resorts of this type are found throughout the United States and there are a number of them in California. The successful resorts craft their amenities and environment to fit the local character. The visitors expect a "Sense of Place" as opposed to a place to park the RV for the night.

Many of these individuals travel a good part of the year and may "park hop", staying at sites in numerous resorts across the country and travel from resort to resort throughout the year. While the visitors of these outdoor resorts typically maintain permanent residency in some other location, it is their desire and privilege to tour other parts of the country in comfortable, personalized accommodations. Rather than renting hotel rooms and packing and unpacking as they travel, the traveler can load up their motorcoach once before leaving for a trip and travel for months at a time before returning home again.

Attachment 5

Applicant's Project Description
GPA 08-001, Rezone 08-001, PD 09-001
LLA 07-0293 & CUP 08-001
(PR Motorcoach)

As a result, people traveling in this lifestyle typically spend a great deal of money on a motorcoach that they can feel comfortable staying in, with plenty of space and amenities while away from home. Additionally they tend to take advantage of local golf courses, wineries, restaurants, supermarkets and other local commercial services.

Location

The proposed Resort site is located at the current end of Golden Hill Road, north of Highway 46. To the south is the Golden Hill Business Park, which has begun to fill with new buildings and businesses. To the north, across the Huer Huero Creek the properties are agricultural, with vineyards, located outside of the City limits. Currently the owner, Jan Kris Vineyards, is proposing to construct a crush facility and has applied to the County for approval. The area to the west is a large lot residential subdivision in the County. Most of the lots are 5 acres or larger. The average distance from the project property line to these homes is approximately 135 feet. The area to the east is the abandoned City wastewater plant and a parcel owned by Tom Erskine zoned Parks and Open Space. In the future, Golden Hill Road will extend to the north through the Resort property and connect to Dry Creek Road as part of the City of Paso Robles Circulation Plan. Refer to Exhibit A for a vicinity map.

Existing Land Use

Currently under the City of Paso Robles General Plan, this property is designated Agriculture. In the Zoning Code, the designation is Residential Agriculture with a Planned Development Overlay (RA PD).

The property has been historically vacant and the only agricultural use has been limited to grazing a small number of cattle. The land of this area consist of soils that are not largely suited for intensive agricultural use and there are no prime soils on the property. The prominent soil type on the property is type 200 (Sesame sandy loam), which has a classification of Type IV irrigated or non-irrigated. The remaining soils classifications fall between III and VIII, with the dominant soil type being one of type IV. There are no class I or class II soils. Due to the topography, number of oak trees, and river area, it does not generally lend itself well to agricultural uses. The southern portion of the property adjacent to the Golden Hill Business Park is the only area that would have a limited potential for agricultural and agriculturally related uses.

The property currently has 3 legal parcels. The parcels are 36.0, 79.9 and 44.6 acres for a total of 160.5 acres.

Project Overview

Paso Robles Motorcoach Resort is proposing to create a resort of approximately 332 motorcoach sites on the 160.5 acre property. Through the use of a Lot Line Adjustment, the three existing parcels will be reconfigured to conform more appropriately to the topography, land forms and proposed future uses. Only one of the three parcels, Parcel 2, is proposed for development. Nearly two thirds (65%) of the project site is to be preserved as open space and for agricultural uses.

Parcel 1- Public Open Space-Huer Huero Creek

Proposed Parcel 1, consisting of 58.8 acres, encompasses the Huer Huero Creek and is proposed to be offered to the City for public use as Open Space. This parcel is currently zoned Residential Agriculture with a PD Overlay. The request is to rezone the property to Agriculture. The “residential” component of the current zoning is clearly in conflict with the Airport Land Use plan. The centerpiece of the natural environment of the site is the Huer Huero Creek. The proposed 58.8 acres of open space with hundreds of oaks will provide a unique and highly enjoyable environment for the visitors to the resort, as well as public access to Huer Huero Creek. Public access will be available on both sides of the creek through the offer of dedication for Golden Hill Road.

This parcel provides a number of public benefits for hiking, biking, horseback riding and an essential link in what someday will be a Huer Huero Creek Plan, similar to the Salinas River Plan, currently in progress. Additionally, being adjacent to current City owned property (the abandoned wastewater plant) the value to the community is enhanced. This parcel configuration, agricultural zoning, and dedication to the City exceed the expectations of the City’s Purple Belt Policy by providing a buffer within the City limits.

Parcel 2- Motorcoach Resort

Proposed Parcel 2, consisting of 68.6 acres is subject to a General Plan Amendment to change the land use from Agriculture to Parks and Open Space. This parcel, located in the middle of the property, is the proposed site for the Motorcoach Resort. The 332 Motorcoach sites are limited to this parcel. Each site is designed to comfortably accommodate a motorcoach, an area to park a support/visitor vehicle and plenty of room for the occupants to enjoy their surroundings, barbequing, and relaxing. The sites range in size from approximately 3,000 sf to over 1/4 of an acre, with the average being in excess of 4,500 sf.

Trails and walkable roads connect the sites to a number of recreational and community areas including swimming pools, exercise facilities, common rooms, and gathering areas.

Parcel 3- Agriculture

Proposed Parcel 3, consisting of 21.4 acres, is located at the southerly end of the property adjacent to the Golden Hill Business Park. It is proposed to rezone this property from Residential Agriculture to Agriculture. It is intended that this parcel act as a buffer for the Motorcoach Resort from the business park to the south and from the neighbors to the west. A vineyard is proposed for this property to separate and enhance the entrance to the Resort. No development is planned for this parcel at this time. A restrictive covenant is proposed to be applied to this parcel to prohibit future land use changes such as rezoning to a different land use and to restrict the parcel to agriculturally zoned uses in perpetuity. Additional proposed land use restrictions on the property limit future allowed uses to less than those currently allowed in the Agriculture zone. This restriction would be recorded as constructive notice on the title for the property.

The owner is offering an access easement through this property to the Open Space parcel for City crews to access the creek for maintenance purposes.

Project Concept Development

Project Design

To address the expectations of the upper end of the RV community, it is necessary to provide a very high quality, luxurious, relaxing environment. Site amenities must be top notch.

To assist with local knowledge and technical expertise, the property owners engaged North Coast Engineering (NCE) acting as Project Manager, providing base mapping, site layout, grading and drainage design, hydrological and hydraulic analysis, and survey services including oak tree locations and plotting of easements. Dan Meade and Lynne Dee Althouse of Althouse and Meade provided biological consultation and input on resource avoidance and protection. Ron Wolff provided architectural design and Casey Patterson designed the landscaping. Artistic and aesthetic input was provided by local artist Steve Kalar. Chip Tamagni and Steve Alvarez of A & T Arborists provided professional arborist advice and analysis for the protection of the oak tree resources on the property.

Considerable time was spent on the site hand selecting individual motorcoach sites, factoring in: visibility, views, accessibility, prevailing winds, grading, drainage, oak tree impacts, solar access, and general ambiance. This approach resulted in the creation of unique, low impact, highly desirable sites. Roads were located to minimize grading, impacts to trees and known biological resources.

The combination of operational, consumer, and technical expertise provided a solid basis for creating a high quality Motorcoach Resort in the City of Paso Robles.

Project Function

The function and operation of the Resort is of utmost importance to the visitors. On arrival, the guests will be greeted by a welcome center with a guarded gate, manned 24 hours a day. Most of the arrivals and departures will be during daylight hours. Visitors will not be allowed to tow boats, ATV's or other trailers behind the Motorcoach, only tow vehicles will be allowed.

Onsite amenities include 3 pool, club house, and recreation areas as well as areas where visitors can purchase basic necessities without leaving the resort. Generous open space areas combined with ample recreational opportunities will prove to be very attractive to the visitors.

Landscape maintenance will be performed on a regular basis and trash removal from the sites will be provided daily.

Visitors will be limited to a stay of a maximum of 30 consecutive days.

PROJECT PHASING CONCEPT

The Paso Robles Motorcoach Resort will be constructed in phases, corresponding with the phasing plan. Phases will be developed in the sequence listed, with the potential of developing smaller increments in sub-phases or more than one phase occurring at the same time. Modification of the phase limits or the sequencing of phases would be approved by staff through the standard process.

Grading activities for the construction of the project will also be phased. Grading permits will be issued by the City of Paso Robles corresponding with the area of proposed disturbance. San Joaquin Kit Fox (SJKF) mitigation fees will be required for the same corresponding area of disturbance prior to issuance of the necessary grading permit. It will be the developer's option to pay SJKF mitigation fees for the entire project with the first phase of grading.

Offsite improvements shall include the construction of Golden Hill Road, between the southerly boundary line and the resort entrance, consistent with the Concept Plan included in the CUP plans. This portion of Golden Hill Road shall be completed and accepted by the City prior to issuance of occupancy for the first phase of the resort, or 48 sites. The balance of Golden Hill Road, to the southerly limit of the Huer-Huero bridge, will not be constructed with the resort, but the resort developer will pay in lieu fees to the City corresponding with the cost of constructing the road per the Concept Plan. A total cost for the construction of the remaining portion of Golden Hill Road will be determined and agreed upon by the City Engineer. The cost will be prorated by site, to be paid to the City as the resort is developed and occupancy is granted. The Huer-Huero bridge and the balance of Golden Hill Road north of the bridge are regional improvements, to be paid for through the City's AB1600 fee program. The Paso Robles Motorcoach Resort will

pay traffic impact fees toward the construction of the future improvements with payment of development impact fees.

Community Outreach and Public Input

Neighborhood Input

The project property owners felt that it was very important to communicate early in the process their ideas and intentions for their property with the neighbors.

In October of 2006, the project property owners contacted the Circle B Homeowners Association (neighboring properties in the County) to offer a meeting to explain their proposed plan. The Circle B homeowners declined to meet.

In July of 2007, the project property owners sent an invitation to all the neighbors in the area to attend an open house to review conceptual site plans for the project and discuss any issues the neighbors might have. The neighbors responded with a letter signed by 10 local property owners rejecting the offer to meet and review the project. The open house was held on July 9th. Two of the neighbors attended. They reviewed the project, shared their comments and concerns. Some of their suggestions were incorporated into the plan and the plans were revised. A week later NCE met with one additional neighbor.

In early December of 2007, the neighbors were again invited to an open house to review a revised version of the project. The open house was attended by 5 of the neighbors. The revised site plan and development concept was generally well received.

An additional meeting with the neighboring property owners is scheduled to take place prior to the Planning Commission hearing.

Native American Input

The project was referred, as required by State law, to the Native American tribes familiar with the cultural heritage of Native Americans of this area. Site visits in 2007 by the local representative of the Northern Chumash resulted in a determination that the only recommendation would be to avoid impacts to the eagle nest identified in the Althouse and Meade biological review.

Public Input

There has been considerable opportunity for the public to provide input on the proposed Resort through a number of public hearings, both at the Planning Commission and City Council. The project was noticed by the City consistent with public noticing

requirements. Additionally, numerous articles appeared in local newspapers, keeping the public further informed.

The first public hearing was held at the Planning Commission on August 28, 2007. Several members of the public spoke, some in support of the GPA and Rezone, and some against. The Planning Commission voted 4-2-1 to recommend to the City Council that the GPA/Rezone be denied.

The second public hearing was the City Council hearing of September 18, 2007. Twenty members of the public spoke. The City Council could not agree on a decision and the GPA/Rezone was denied by lack of action by the Council.

The original application to the City was for a General Plan Amendment and Rezone. While the GPA and Rezone was being processed by the City the site specific design was being prepared by the project applicants and their consultants. At the time of the initial application it was the policy of the City to require the GPA and Rezone to be processed and approved prior to the review and approval of the associated Development Plan. It became clear through the Planning Commission and City Council review process that this policy would not be appropriate in this case. The Council clearly expressed the desire to review the proposed resort details as part of the GPA/Rezone process.

The property owners have prepared an application to the City including all of the components for project approval.

Local Business Input

The property owners have also met with the Paso Robles Chamber of Commerce, Main Street, local wineries and businesses and have had very strong support for the Resort. They have received numerous letters of support.

Consistency with City of Paso Robles General Plan, Goals and Objectives

The proposed Paso Robles Motorcoach Resort is consistent with and furthers many of the goals and objectives of the City of Paso Robles.

The Paso Robles Economic Strategy

The proposed project furthers many of the goals of the Paso Robles Economic Strategy. As a high quality destination resort, this project would attract numerous visitors. Additionally, it takes an underutilized piece of property and maximizes the value of the property for the community. The City General Plan requires new development to be

financially neutral. This project goes beyond that requirement by contributing a significant sum of money to the community in the form of Transient Occupancy Tax (TOT). Based on an average occupancy rate of 60%, it is estimated that the completed Motorcoach Resort would generate approximately \$600,000 a year in TOT.

The economic impact of this proposed project would be highly positive for the City of Paso Robles, not only from the collection of the taxes, but the spin-off effects of the visitors to the community. Visitors will be spending money in restaurants, wineries, stores, and for fuel, while not having impacts on schools and very little other impacts on city facilities and services. According to the California Travel Parks Association, visitors to these types of resorts spend an average of \$200 a day for these services. It is estimated that this resort would generate over \$16,000,000 a year in spinoff sales and services and generate over \$1,100,000 a year in sales tax.

The project would contribute in excess of \$1,000,000 to the City's AB1600 Development Impact Fees Program.

The completed project is expected to employ 20 people in full-time positions and 10 people in part-time positions. A full time staff will operate the resort and will be assisted by various part time support personnel. Additional enhancements to job creation for the community as a whole will occur due to the increased demand for services from the visitors to the Paso Robles Motorcoach Resort.

A less measurable but significant benefit would be the potential for intellectual and business development resulting from the visitors experiencing the area who may be encouraged to bring business to Paso Robles as well as engage with existing businesses.

Paso Robles Circulation Element

The proposed project site performs an important role in providing the opportunity to further the goals of the Circulation Element of the General Plan. Golden Hill Road, as identified in the Circulation Element, is an important, if not vital, link for future traffic circulation. Golden Hill Road would be constructed through this property to connect with a bridge over the Huer Huero Creek to Dry Creek Road, another vital link for traffic circulation. See attached Exhibit B.

The Huer Huero Creek bridge is included in the City's AB1600 Development Impact Fee Program for a total of \$14,000,000. This demonstrates a commitment to complete this portion of the City's Circulation Plan. The importance of this link has been recently reinforced by new traffic studies performed by CalTrans and traffic studies prepared for the Lowes Regional Commercial project, approved at the intersection of Golden Hill Road and Highway 46.

Local Traffic Circulation Impacts

The literal implementation of the current Paso Robles Circulation Element requirements for the construction of Golden Hill Road would result in significant impacts to the residents of the Circle B neighborhood. See Exhibit C.

When the Circle B area Tract Map (Tract 1576) was approved by the County the conditions of approval required an Offer of Dedication of a 25' wide strip of their property. An additional 15' Offer of Dedication from the Motorcoach Resort parcel provided the minimum 40' of access that the County required for the approval of their project. See Exhibit D.

The City Circulation Element currently requires an Offer of Dedication of 100' and road improvements for two traffic lanes in each direction with a landscaped median. Taking into account the current alignment of Golden Hill Road constructed by the Golden Hill Business Park, the future improvement to Golden Hill Road would replace the existing private road in the Circle B neighborhood. The existing 25' Offer of Dedication on the Circle B side of the property line would be used as well as 75' of the Motorcoach property. The Circle B residents would be fronting on an arterial road. A number of oaks would require removal due to the design characteristics of the City Standard Arterial.

The Motorcoach Resort is proposing an alternative alignment and road design to reduce these impacts. In consultation with the City Engineer a low impact, topographically sensitive design approach is being suggested. The alternative would be a reduced street width constructed completely on the project property. The offer of dedication would remain at a 100' minimum and would also be totally within the project property. Meandering travel lanes would significantly reduce oak tree impacts. The existing private road serving the Circle B residents would remain essentially intact. A new gate would be constructed by the project to provide safe access to the Circle B neighborhood off of the new Golden Hill Road. The existing Circle B Road gate would be used for exit only. See Exhibit E.

Airport Land Use Plan

The vast majority of the property is located in Airport Land Use Zone 5. A very small portion, located at the southeast corner of the property in the Huer Huero Creek, is located in Airport Land Use Zone 3. The proposed use of an RV resort is classified as transient lodging in the Airport Land Use Plan, and is an allowed use. It is not uncommon for an RV park to be located close to an airport. The people that frequent these types of facilities are accustomed to airport associated noise, and being transient, are not stakeholders in the community, and would therefore have very little concern with the operations of the airport. On June 20, 2007, at a public hearing, the Airport Land Use Commission found the rezone to be in conformance with the Airport Land Use Plan.

Oak Tree Impacts

There are a large number of oak trees on the property. They are sparse in the southerly area of the property increasing in density to the north, particularly on the slopes adjacent to the Huer Huero Creek. The arborist describes these woodland areas as an “aging forest”, in need of thinning of diseased trees, pruning of the older, heavier limbs and planting of young trees to prolong the longevity of the oak woodland character of the project.

Early in the design process for the resort, North Coast Engineering surveyed all the trees potentially impacted by the development. The trees were field surveyed, measured, cataloged and drafted into the base map for the site plan. A & T Arborists rated every tree based on City Oak Tree Ordinance requirements.

As a result of careful design, it was possible to design the Resort without removing any healthy oak trees. The only trees proposed for removal for the Resort are rated 2 or less on a scale of 0-10 with 0 being dead and 10 being a specimen tree with perfect shape, structure and foliage. A total of 5 dead trees and 19 trees rated 1 and 2 are proposed for removal for the Resort. Only one tree with a rating of 3 is proposed for removal.

The design for Golden Hill Road within the public right of way will require the removal of 9 oak trees. Four of these trees would be considered healthy with a rating of 3 or better. The five others are rated 2 or lower. The road design specifically avoided oak tree impacts as much as possible, while maintaining the design standards required by the City.

Area Drainage

Property owners in the Circle B area west of the proposed project site historically have had concerns and problems regarding storm water and flooding impacts to their properties. The Circle B area, developed in the County, is relatively flat. No comprehensive drainage design was done with the development of the County properties in this area. The proposed resort site makes up a relatively small portion of the total watershed area. Nevertheless, existing residents have expressed concerns about the existing conditions. Recognizing this, the approach to the drainage design is to respect those concerns. As proposed, the project design would meet or exceed the City Standards for the City of Paso Robles and be required to comply with the requirements of the Regional Water Quality Control Board. The focus of the drainage design would be to utilize low impact development design to reduce impacts downstream. Additionally, improvements to the existing termination of Golden Hill Road could improve existing localized flooding issues.

Low Impact Development

The character of the project lends itself to the utilization of numerous low impact development techniques. Larger sites, narrow roads and ample open space facilitate on-site storm water recharge. With the minimization of permeable surfaces and inclusion of

infiltration areas on sites and in open space, initial and regular rainfall events would be largely absorbed into the local aquifer.

It is proposed that the majority of the sites employ individual onsite biofiltration and infiltration. Additionally, area-wide biofiltration and storm water detention areas are proposed. By capturing storm water as it hits the ground the issues of storm water quality are easily handled, as opposed to more traditional techniques of collecting storm water in pipes and trying to treat it downstream.

Wetland and Habitat Restoration

Early in the site review process specific emphasis was placed on the relation of the Huer Huero Creek to the proposed project. In a series of site visits Althouse and Meade proposed a creek restoration plan in conjunction with proposed fill areas adjacent to the creek. It was felt that by re-contouring the overflow area adjacent to the project that the result would provide for better maintained flows, habitat improvement and opportunities to remove exotic plant species and replace them with native species. Due to the relation of the normal high water line, it is unlikely that a permit is necessary from the Army Corps of Engineers. A streambed alteration permit would be required from the Department of Fish and Game.

To confirm the validity of the restoration plan concept a Hydrological analysis of Huer Huero Creek was prepared by NCE.

Project Utilities

Public sewer and water is available to the project site. Currently there is a 12" water line in Golden Hill Road and a sewer line runs through the property. Private sewer lift stations would have to be constructed to serve a number of the phases of the project. The existing overhead utilities on the west side of the project would be placed underground.

RESOLUTION NO:

A RESOLUTION OF THE CITY COUNCIL
OF THE CITY OF EL PASO DE ROBLES
APPROVING A MITIGATED NEGATIVE DECLARATION FOR
GENERAL PLAN AMENDMENT 09-001, REZONE 09-001, PD 08-001, CUP 08-001
AND LOT LINE ADJUSTMENT 07-0293
NORTH END OF GOLDEN HILL ROAD, APN 025-435-005, 006 and 007
APPLICANT – PASO 160, LLC – MCCURDY
(PASO ROBLES MOTORCOACH RESORT)

WHEREAS, the Paso Robles Motorcoach Project consists of General Plan Amendment 09-001, Rezone 09-001, Planned Development 08-001, Conditional Use Permit 08-001 and Lot Line Adjustment 07- 093; and

WHEREAS, the project has been filed by North Coast Engineering on behalf of Paso 160, LLC to construct a 332 space RV resort on the 160 acre site located at the northern terminus of Golden Hill Road, North of Highway 46 East; and

WHEREAS, the 160 acre site is currently comprised of three parcels, the applicants are proposing to reconfigure the three parcels via Lot Line Adjustment 07-093, where Parcel 1 would be 58.8 acres, Parcel 2 would be 68.6 acres and Parcel 3 would be 21.4 acres; and

WHEREAS, General Plan Amendment 09-001 and Rezone 09-001 propose the following changes:

- **Parcels 1:** change the zoning designation from Residential Agriculture (RA) to Agriculture (AG) and retain the current AG land use designation;
- **Parcel 2:** amend the land use designation from Agriculture (AG) to Parks & Open Space (POS) and change the zoning designation from Residential Agriculture (RA) to Parks & Open Space (POS);
- **Parcels 3:** change the zoning designation from Residential Agriculture (RA) to Agriculture with a Planned Development Overlay (AG-PD) and retain the current AG land use designation;

WHEREAS, Planned Development 08-001 & Conditional Use Permit 08-001 proposes the following:

- **Parcel 1:** this parcel would be dedicated to the City for public use as Open Space;
- **Parcel 2:** establish the 332-space Paso Robles Motorcoach Resort;
- **Parcel 3:** use PD Overlay district to limit future uses to agricultural related uses such as winery, wine tasting and vineyards;

WHEREAS, pursuant to the Statutes and Guidelines of the California Environmental Quality Act (CEQA), and the City's Procedures for Implementing CEQA, an Initial Study was prepared and circulated for public review and comment; and

WHEREAS, based on the information and analysis contained in the Initial Study, a determination has been made that the proposed Project qualifies for adoption of a Mitigated Negative Declaration; and

WHEREAS, an Initial Study was prepared for this project (Attached as Exhibit A) which concludes and proposes that a Mitigated Negative Declaration be approved; and

WHEREAS, Public Notice of the proposed Mitigated Negative Declaration was given as required by Section 21092 of the Public Resources Code; and

WHEREAS, the Planning Commission held a duly noticed public hearing on February 24, 2009 on this project to accept public testimony on the proposed Mitigated Negative Declaration; and

WHEREAS, the Planning Commission on a 4-1 vote (1 abstention, 1 absent) recommended that the City Council approve the proposed Mitigated Negative Declaration; and

WHEREAS, based on the information contained in the Initial Study prepared for this project and testimony received as a result of the public notice, the City Council finds no substantial evidence that there would be a significant impact on the environment based on the Mitigation Agreement and Mitigation Measures incorporated with this project; and

NOW, THEREFORE, BE IT RESOLVED, by the City Council of the City of El Paso de Robles, based on its independent judgment, approves a Mitigated Negative Declaration for General Plan Amendment 09-001, Rezone 09-001, Planned Development 08-001, Conditional Use Permit 08-001 and Lot Line Adjustment 07-093 in accordance with the California Environmental Quality Act, subject to the mitigation measures outlined in Exhibit B Mitigation Summary.

PASSED AND ADOPTED by the City Council of the City of Paso Robles this 17th day of March, 2009 by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

Duane Picanco, Mayor

ATTEST:

Cathy David, Deputy City Clerk

**CITY OF PASO ROBLES – PLANNING DIVISION
INITIAL STUDY**

1. GENERAL PROJECT INFORMATION

PROJECT TITLE: Paso Robles Motorcoach Resort (GPA 09-001, RZ 09-001, PD 08-001, CUP 08-001, & PRAL 07-0293)

LEAD AGENCY: City of Paso Robles - 1000 Spring Street, Paso Robles, CA 93446

Contact: Darren Nash, Associate Planner
Telephone: (805) 237-3970

PROJECT LOCATION: Golden Hill Road north of Highway 46 (APN 025-435-005, 006 and 007)

PROJECT PROPONENT: Applicant: Paso 160, LLC- Doug McCurdy
P.O. Box 4964, Paso Robles, CA 93447
Representative: Larry Werner-North Coast Engineering

**LEAD AGENCY CONTACT/
INITIAL STUDY PREPARED BY:** Darren Nash, Associate Planner

Telephone: (805) 237-3970
Facsimile: (805) 237-3904
E-Mail: dnash@prcity.com

GENERAL PLAN DESIGNATION: Agriculture (AG)/Airport Overlay Zone 4 and Zone 3C

ZONING: Residential Agriculture Planned Development (RA PD)

Exhibit A
Initial Study
GPA 09-001, RZ 09-001, PD 08-001
CUP 08-001 & LLA 07-0293
(PR Motorcoach)

2. PROJECT DESCRIPTION

Paso Robles Motorcoach Resort is proposing to create a resort of approximately 332 motorcoach sites on the 160.5 acre property. Through the use of a Lot Line Adjustment, the three existing parcels will be reconfigured to conform more appropriately to the topography, land forms and proposed future uses. Only one of the three parcels, Parcel 2, is proposed for development. Nearly two thirds (65%) of the project site is to be preserved as open space and for agricultural uses.

Parcel 1- Public Open Space-Huer Huero Creek

Proposed Parcel 1, consisting of 58.8 acres, encompasses the Huer Huero Creek and is proposed to be offered to the City for public use as Open Space. This parcel is currently zoned Residential Agriculture with a PD Overlay. The request is to rezone the property to Agriculture. The "residential" component of the current zoning is clearly in conflict with the Airport Land Use plan. The centerpiece of the natural environment of the site is the Huer Huero Creek. The proposed 58.8 acres of open space with hundreds of oaks will provide a unique and highly enjoyable environment for the visitors to the resort, as well as public access to Huer Huero Creek. Public access will be available on both sides of the creek through the offer of dedication for Golden Hill Road.

This parcel provides a number of potential public benefits for habitat protection, hiking, biking, horseback riding and an essential link in what someday could be a Huer Huero Creek Plan, similar to the Salinas River Plan, currently in progress. Additionally, being adjacent to current City owned property (the abandoned

wastewater plant) the value to the community is enhanced. This parcel configuration, agricultural zoning, and dedication to the City fulfills the expectations of the City's Purple Belt Policy by providing a buffer within the City limits.

Parcel 2- Motorcoach Resort

Proposed Parcel 2, consisting of 68.6 acres is subject to a General Plan Amendment to change the land use from Agriculture to Parks and Open Space. This parcel, located in the middle of the property, is the proposed site for the Motorcoach Resort. The 332 Motorcoach sites are limited to this parcel. Each site is designed to comfortably accommodate a motorcoach, an area to park a support/visitor vehicle and plenty of room for the occupants to enjoy their surroundings, barbequing, and relaxing. The sites range in size from approximately 3,000 sf to over 1/4 of an acre, with the average being in excess of 4,500 sf.

Trails and walkable roads connect the sites to a number of recreational and community areas including swimming pools, exercise facilities, common rooms, and gathering areas.

Parcel 3- Agriculture

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The owner is offering an access easement through this property to the Open Space parcel for City crews to access the creek for maintenance purposes.

Project Phasing Plan

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AB1600 fee program. The Paso Robles Motorcoach Resort will pay traffic impact fees toward the construction of the future improvements with payment of development impact fees.

Environmental Analysis

The site is not currently in active agricultural production. The property does not have prime soils. Historical use of the site has been limited to grazing a small amount of cattle on the property. Remnants of past agricultural use include a 12x5 foot concrete pad, watering trough, circular concrete pad for a water tank and a water pump. The site is otherwise undeveloped with site topography consisting of rolling hills densely vegetated with a mature blue oak forest. Valley oaks are also present near Huerhuero Creek. The total number of oak trees located on the project site is approaching 600. 336 of those trees are located within the general development area of the project and were surveyed into the plans. Of the 336 trees, 9 trees are proposed to be removed as a result of the construction of the extension of Golden Hill Road. 24 trees (5 are considered dead) are proposed to be removed for the development of the RV resort. Surrounding land uses include light industrial uses to the south, Huerhuero Creek and agriculture to the north and east, and rural residential development to the west.

The applicant has provided supplemental assessments as requested by the City for biology, oak trees, cultural resources, and traffic circulation. The site contains blue oak woodland, riparian habitat associated with Huerhuero Creek, and vernal pools that provide potential habitat for the federally listed endangered fairy shrimp. Nesting birds and sensitive plants also occur on the site. Bats, badgers, and western spadefoot toad may also occur on the site. The site is within the strategic section of the San Joaquin Kit Fox migration corridor. Golden eagles use the grasslands on the property for hunting and nest in oak trees on the property.

This initial study evaluates the potential environmental impacts of the proposed General Plan Amendment, Rezone and development plan. The development plan proposes a project that includes a 332-space luxury recreational vehicle vacation resort with health spa, tennis courts, swimming pool, jacuzzi, showers, and laundry facilities.

The site is within the Airport Overlay District and is subject to consistency with the Airport Land Use Plan (ALUP). The project site is required to include mitigation measures for consistency with the ALUP.

This project also includes the extension of Golden Hill Road to the north, this extension is consistent with the Circulation Element of the General Plan which calls for Golden Hill Road to extend and connect with Dry Creek Road. The construction of the road would be phased with the phasing of the project. Ultimately, a bridge will need to be constructed over the Huer Huero Creek to allow for the connection with Dry Creek Road.

3. OTHER AGENCIES WHOSE APPROVAL MAY BE REQUIRED (For example, issuance of permits, financing approval, or participation agreement):

California Department of Fish and Game, US Fish and Wildlife Service, US Army Corps of Engineers, Regional Water Quality Control Board, and Caltrans.

4. EARLIER ENVIRONMENTAL ANALYSIS AND RELATED ENVIRONMENTAL DOCUMENTATION:

This Initial Study incorporates by reference the City of El Paso de Robles General Plan Environmental Impact Report (EIR) (SCH#2003011123).

5. CONTEXT OF ENVIRONMENTAL ANALYSIS FOR THE PROJECT:

This Initial Study relies on expert opinion supported by the facts, technical studies, and technical appendices of the City of El Paso de Robles General Plan EIR. These documents are incorporated herein by reference. They provide substantial evidence to document the basis upon which the City has arrived at its environmental determination regarding various resources.

6. PURPOSES OF AN INITIAL STUDY

The purposes of an Initial Study for a Development Project Application are:

- A. To provide the City with sufficient information and analysis to use as the basis for deciding whether to prepare an Environmental Impact Report, a Mitigated Negative Declaration, or a Negative Declaration for a site specific development project proposal;
- B. To enable the Applicant of a site specific development project proposal or the City as the lead agency to modify a project, mitigating adverse impacts before an Environmental Impact Report is required to be prepared, thereby enabling the proposed Project to qualify for issuance of a Negative Declaration or a Mitigated Negative Declaration;
- C. To facilitate environmental assessment early in the design of a project;
- D. To eliminate unnecessary EIRs;
- E. To explain the reasons for determining that potentially significant effects would not be significant;
- F. To determine if a previously prepared EIR could be used for the project;
- G. To assist in the preparation of an Environmental Impact Report if one is required; and
- H. To provide documentation of the factual basis for the finding of no significant effect as set forth in a Negative Declaration or a Mitigated Negative Declaration prepared for the a project.

7. EXPLANATION OF ANSWERS FOUND ON THE ENVIRONMENTAL CHECKLIST FORM

A. Scope of Environmental Review

This Initial Study evaluates potential impacts identified in the following checklist.

B. Evaluation of Environmental Impacts

- 1. A brief explanation is required for all answers to the questions presented on the following Environmental Checklist Form, except where the answer is that the proposed project will have “No Impact.” The “No Impact” answers are to be adequately supported by the information sources cited in the parentheses following each question or as otherwise explained in the introductory remarks. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to the project. A “No Impact” answer should be explained where it is based on project-specific factors and/or general standards. The basis for the “No Impact” answers on the following Environmental Checklist Form is explained in further detail in this Initial Study in Section 9 (Earlier Environmental Analysis and Related Environmental Documentation) and Section 10 (Context of Environmental Analysis for the Project).

2. All answers on the following Environmental Checklist Form must take into account the whole action involved with the project, including implementation. Answers should address off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. “Potentially Significant Impact” is appropriate, if an effect is significant or potentially significant, or if the lead agency lacks information to make a finding of insignificance. If there are one or more “Potentially Significant Impact” entries when the determination is made, preparation of an Environmental Impact Report is warranted.
4. “Potentially Significant Impact Unless Mitigated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level. Mitigation Measures from Section 9 (Earlier Environmental Analysis and Related Environmental Documentation) may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). See Section 4 (Earlier Environmental Analysis and Related Environmental Documentation) and Section 11 (Earlier Analysis and Background Materials) of this Initial Study.
6. References to the information sources for potential impacts (e.g., general plans, zoning ordinances) have been incorporated into the Environmental Checklist Form. See Section 11 (Earlier Analysis and Related Environmental Documentation). Other sources used or individuals contacted are cited where appropriate.
7. The following Environmental Checklist Form generally is the same as the one contained in Title 14, California Code of Regulations, with some modifications to reflect the City’s needs and requirements.
8. Standard Conditions of Approval: The City imposes standard conditions of approval on Projects. These conditions are considered to be components of and/or modifications to the Project and some reduce or minimize environmental impacts to a level of insignificance. Because they are considered part of the Project, they have not been identified as mitigation measures. For the readers’ information, the standard conditions identified in this Initial Study are available for review at the Community Development Department.
9. Certification Statement: The statements made in this Initial Study and those made in the documents referenced herein present the data and information that are required to satisfy the provisions of the California Environmental Quality Act (CEQA) – Statutes and Guidelines, as well as the City’s Procedures for Implementing CEQA. Further, the facts, statements, information, and analysis presented are true and correct in accordance with standard business practices of qualified professionals with expertise in the development review process, including building, planning, and engineering.

8. ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The proposed project may potentially affect the environmental factors checked below, and may involve at least one impact that is a "Potentially Significant Impact" or is "Potentially Significant Unless Mitigated," if so indicated on the following Environmental Checklist Form (Pages 8 to.15)

- | | | |
|---|--|--|
| <input type="checkbox"/> Land Use & Planning | <input checked="" type="checkbox"/> Transportation/Circulation | <input type="checkbox"/> Public Services |
| <input type="checkbox"/> Population & Housing | <input checked="" type="checkbox"/> Biological Resources | <input type="checkbox"/> Utilities & Service Systems |
| <input type="checkbox"/> Geological Problems | <input type="checkbox"/> Energy & Mineral Resources | <input type="checkbox"/> Aesthetics |
| <input type="checkbox"/> Water | <input checked="" type="checkbox"/> Hazards | <input type="checkbox"/> Cultural Resources |
| <input checked="" type="checkbox"/> Air Quality | <input type="checkbox"/> Noise | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Mandatory Findings of Significance | | |

9. ENVIRONMENTAL DETERMINATION: On the basis of this initial evaluation: I find that:

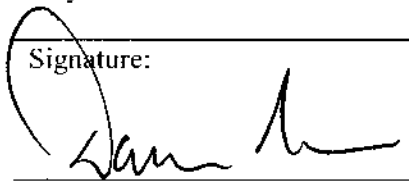
The proposed project could not have a significant effect on the environment; and, therefore, a **NEGATIVE DECLARATION** will be prepared.

Although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on an attached sheet have been added to the project. Therefore, a **MITIGATED NEGATIVE DECLARATION** will be prepared.

The proposed project may have a significant effect on the environment; and, therefore an **ENVIRONMENTAL IMPACT REPORT** is required.

The proposed project may have a significant effect(s) on the environment, but one or more effects (1) have been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) have been addressed by mitigation measures based on the earlier analysis as described on attached sheets, if the effect is a "potentially significant impact" or is "potentially significant unless mitigated."

Therefore, an **ENVIRONMENTAL IMPACT REPORT** is required, but it will analyze only the effect or effects that remain to be addressed.

Signature: 

Date: January 29, 2009

Darren Nash, Associate Planner

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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I. LAND USE AND PLANNING. Would the Proposal:

a) Conflict with general plan designation or zoning?

(Sources: 1 & 8)

Discussion: The entire 160 acre property currently has a General Plan Land Use designation of Agriculture (AG) and a zoning designation of Residential Agriculture Planned Development (RA-PD). The property currently consists of three separate parcels.

The proposed project via a lot line adjustment would re-orient the parcels (Parcels 1, 2 & 3) in a manner that would better follow the existing topography and overall provide for the proposed project. Also necessary is the request to change the land use and zoning designation for the parcels as follows:

Parcel 1, 36 acres: retain the existing AG land use designation and change the zoning to Agriculture (AG) to be consistent with the land use. Parcel 1, which is the portion of the site that contains the Heur Huero Creek would be dedicated to the City as open space.

Parcel 2, 79.9 acres: would change the land use and zoning designation to Parks and Open Space (POS) to allow for the development of the 332-space luxury recreational vehicle vacation resort with health spa, tennis courts, swimming pool, jacuzzi, showers, and laundry facilities.

Parcel 3, 44.6 acres: retain the existing AG land use designation and change the zoning to Agriculture (AG). In conjunction with the rezone, by utilizing the Planned Development Overlay provided in the Zoning Code, future uses of Parcel 3 would be limited to agricultural related uses.

The City's General Plan and Land Use Map include the subject property in the Airport Overlay Designation (Zone 3A and Zone 4), which requires review of proposed General Plan or Zoning Amendments by the Airport Land Use Commission (ALUC) for a determination of consistency with the Airport Land Use Plan (ALUP). The City submitted the project description to the ALUC for consideration at their August 20, 2008 meeting, and the ALUC made the requisite finding of consistency.

With the conditions of approval along with any required environmental mitigation measures, conflict with the general plan or zoning designations with the development of the RV park will be less than significant.

b) Conflict with applicable environmental plans or policies adopted by agencies with jurisdiction over the project?

(Sources: 1 & 3)

Discussion: As noted above, the project site includes an Airport Overlay (AP) and is therefore subject to special review by the Airport Land Use Commission for consistency with the Airport Land Use Plan (ALUP). The ALUC found the proposed GPA/RZ to be consistent with the ALUP on August 20, 2008.

The project applicants along with City Staff have had multiple meetings and correspondence with several agencies including the California Department of Fish and Game, US Fish and Wildlife Service, US Army Corps of Engineers and Regional Water Quality Control Board.

In order to address specific concerns from Department of Fish and Game and US Fish and Wildlife Service related to biological impacts, the project has been redesigned. It is anticipated that the most recent redesign will address the concerns from these agencies.

10 Environmental Checklist Form

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
ISSUES (and Supporting Information Sources):				

Protection of biological resources including the Huerhuero Creek, vernal pools, wetlands, sensitive plants, and sensitive animals are discussed in Section VII - Biological Resources. The proposed project does not conflict with applicable environmental plans and policies.

- c) Be incompatible with existing land uses in the vicinity?
 (Sources: 1 & 3)

Discussion: The surrounding land use designations are Parks and Open Space to the southeast; Business Park to the south; Residential Agriculture Planned Development to the east and Public Facilities and Business Park to the north. Rural Residential development is located immediately west of the site in the jurisdiction of San Luis Obispo County. Existing uses adjacent to the project site include commercial/light industrial, low-density single-family homes, and open space/grazing.

Examples of permitted uses in Parks/Open Space zoning include agricultural uses and facilities, wholesale nurseries, wine-tasting rooms, convention centers, and public parks. Conditionally permitted uses in the POS zone include, but are not limited to, equestrian facilities, wineries, golf courses, ball fields, recreational vehicle parks, hotels, and motels. Conditionally permitted uses are uses that may be compatible and consistent with the specific zoning district but for which potential impacts that may result from specific uses can be addressed through implementation of conditions of approval applied to development. Commercial recreation and recreational vehicle parks are conditionally permitted uses in the proposed Parks and Open Space zone, would not be incompatible with surrounding land uses since it would allow for relatively low density uses and activities that would not affect the function of AG uses or industrial uses. Potential impacts to the low density residential area to the west can be addressed through site development design features such as setbacks, screening, light and noise control and other measures.

Additionally, to ensure compatibility of the proposed Paso Robles Motorcoach project, the applicants are proposing to re-designate Parcel 3 as Agriculture, anticipating vineyard/wine tasting type uses which would provide for a significant buffer area.

The potential compatibility issues from the proposed project, such as light, noise, and traffic, have been addressed through project specific mitigation measures and/or conditions of approval. Therefore, it is determined that designation and zoning of Parks and Open Space for the proposed General Plan amendment and rezoning, and Agriculture for Parcel 3, along with the development of the 332 space RV Resort, will result in less than significant impacts to existing uses in the project vicinity.

- d) Affect agricultural resources or operations (e.g., impacts to soils or farmlands, or impacts from incompatible uses)?

Discussion: The General Plan EIR included an evaluation of the City's agricultural resources, which indicates the underlying soil on this property is not prime, of statewide importance, or unique farmland. Historical and existing agricultural uses include cattle grazing on the project site, and there are vineyards north of the property. Use of the site for cattle grazing has been limited and not reliant on soils. Potential use of the site for this proposed project would not result in significant impacts to AG resources or operations.

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Unless Mitigation Incorporated	Potentially Significant Less Than Significant Impact	No Impact
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Additionally, since the property is not considered to have prime soils and is in the vicinity of an existing residential neighborhood and industrial uses, agricultural uses such as farming may not be the best use of the site in terms of neighborhood compatibility.

Therefore, the proposed amendments and development of the RV Resort project will result in less than significant impacts to agricultural resources or operations.

- e) Disrupt or divide the physical arrangement of an established community (including a low-income or minority community)? (Sources: 1 & 3)

Discussion: The project site is currently designated for agricultural production and located adjacent to other similar land use designations. The general plan amendment/rezone will not disrupt or divide the established community. Additionally, the establishment of the RV Resort project would disrupt or divide the established community.

II. POPULATION AND HOUSING. Would the proposal:

- a) Cumulatively exceed official regional or local population projections? (Sources: 1 & 3)

Discussion: The proposed project does not include a residential component nor is it large enough to result in creating a significant number of new jobs that could affect cumulative population projections. The proposed GPA/RZ & RV Resort project will not cumulatively exceed local or regional population projections; therefore, the project will not result in significant impacts.

- b) Induce substantial growth in an area either directly or indirectly (e.g., through projects in an undeveloped area or extension of major infrastructure)? (Sources: 1 & 3)

Discussion: The GPA/rezone would change the land use category from Agriculture to Parks and Open Space, in an area adjacent to light industrial and residential uses. Developed areas northeast of the site within the City's sphere of influence include business park and airport uses. The development of the RV resort project will require extension of City services to the project site including water lines, sewer service, and the extension of Golden Hill Road north to Dry Creek Road. The City's Circulation Element of the General Plan identified and projected the extension of Golden Hill Road and evaluated it as part of the General Plan EIR.

Concurrent with the proposed project, there will be discussions and actions by the Planning Commission and ultimately by the City Council to determine the timing of Golden Hill Road and whether it will be extended entirely with this project or not.

However the extension of services that may occur as a result of the proposed GPA/rezone and the development of the RV resort project are not anticipated to induce substantial growth.

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
c) Displace existing housing, especially affordable housing? (Sources: 1, 3, & 5)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: There is no housing currently existing on the project site, thus the project will not displace any existing housing.

III. GEOLOGIC PROBLEMS. Would the proposal result in or expose people to potential impacts involving:

a) Fault rupture? (Sources: 1, 2, & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: The potential for and mitigation of impacts that may result from fault rupture in the project area are identified and addressed in the General Plan EIR, pg. 4.5-8. There are two known fault zones on either side of this valley. The Rinconada Fault system runs on the west side of the valley. The San Andreas Fault is on the east side of the valley and runs through the community of Parkfield east of Paso Robles. The City of Paso Robles recognizes these geologic influences in the application of the California Building Codes to all new development within the City. Review of available information and examinations indicate that neither of these faults is active with respect to ground rupture in Paso Robles. Soils reports and structural engineering in accordance with local seismic influences may be necessary in conjunction with future development proposals. Based on standard conditions of approval, the potential for fault rupture and exposure of persons or property to seismic hazards is not considered significant.

b) Seismic ground shaking? (Sources: 1, 2, & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: The City is located within an active earthquake area that could experience seismic ground shaking from the Rinconada and San Andreas Faults. The General Plan EIR identified impacts resulting from ground shaking as less than significant and provided mitigation measures that will be incorporated into the design of future development projects including adequate structural design and not constructing over active or potentially active faults. Future structures will be constructed to current California Building codes.

c) Seismic ground failure, including liquefaction? (Sources: 1, 2 & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: Per the General Plan EIR, the project site is located in an area with soil conditions that have a low potential for liquefaction or other type of ground failure due to seismic events. No special considerations other than what would be required by ordinance or code are necessary.

d) Seiche, tsunami, or volcanic hazard? (Sources: 1, 2, & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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e) Landslides or Mudflows? (Sources: 1, 2, & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: d. and e. The project site is not located near bodies of water or volcanic hazards, nor is the site located in an area subject to landslides. A portion of Huerhuero Creek is located on the project site, and future development would be set back and constructed to allow for flows from large storm events. The project has been

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Mitigation Incorporated	Less Than Significant Impact	Potentially Significant No Impact
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designed in accordance with the Preliminary Biological Assessment (Althouse & Meade, December 2006, updated January 2008) prepared for the project, which identifies a 100-foot buffer from the edge of riparian resources (refer to Section VII Biological Resources). The proposed GPA/RZ along with the project is not anticipated to expose people to potential impacts from landslides or mudflows.

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|-------------------------------------|
| f) Erosion, changes in topography or unstable soil conditions from excavation, grading, or fill? (Sources: 1, 2, 3, & 4) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| g) Subsidence of the land? (Sources: 1, 2, & 3) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| h) Expansive soils? (Sources: 4) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| i) Unique geologic or physical features? (Sources: 1 & 3) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion: f-h. Per the General Plan EIR, the soil condition is not erosive or otherwise unstable. Expansive soils characteristics would be identified in association with future development proposals. No unique geologic or physical features are present that would be disturbed. As such, no significant impacts are anticipated. Standard erosion control measures, Low Impact Design requirements and building code requirements would be adequate to reduce potential environmental impacts of development to a less than significant level.

IV. WATER. Would the proposal result in:

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a) Changes in absorption rates, drainage patterns, or the rate and amount of surface runoff? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b) Exposure of people or property to water related hazards such as flooding? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c) Discharge into surface waters or other alteration of surface water quality (e.g., temperature, dissolved oxygen or turbidity)? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Discussion: a – c:

Paso Robles Motorcoach Resort is proposing to create a resort of approximately 332 motorcoach spaces on the 160.5 acres. Through the use of a Lot Line Adjustment, the three existing parcels will be reconfigured to conform more appropriately to the topography, land forms and proposed future uses. Only one of the three parcels, Parcel 2, is proposed for development. Nearly two thirds (65%) of the project site is to be preserved as open space and for agricultural uses.

Proposed Parcel 2, consisting of 68.6 acres is subject to a General Plan Amendment to change the land use from Agriculture to Parks and Open Space. Each space is designed to comfortably accommodate a motorcoach, an area to park a support/visitor vehicle and allow for the occupants to remaining site area. The spaces range in size from approximately 3,000 sf to over 1/4 of an acre, with the average being in excess of

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Mitigation Incorporated	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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4,500 sf.

Trails and walkable roads connect the spaces to a number of recreational and community areas including swimming pools, exercise facilities, common rooms, and gathering areas.

The character of the project lends itself to the utilization of numerous low impact development techniques. Larger sites, narrow roads and ample open space facilitate on-site storm water retention and ground water recharge. Impermeable area is limited to the roads, RV parking spaces and the community center pads. With the minimization of permeable surfaces and inclusion of infiltration areas on sites rainfall would be largely absorbed into the local aquifer or follow historic flow patterns to the Huer Huero Creek.

A majority of the sites employ individual onsite biofiltration and infiltration systems. Additionally, area-wide biofiltration and storm water detention areas are proposed. By capturing storm water on site, storm water quality will be addressed through natural filtration systems, opposed to more traditional techniques of collecting storm water in pipes and trying to treat it downstream.

Property owners in the Circle B neighborhood west of the proposed project site historically have had concerns and problems regarding storm water and flooding impacts to their properties. The Circle B area, developed in the County, is relatively flat. No comprehensive drainage system was incorporated with the development of these County properties. The proposed resort site is comprised of a relatively small portion of the total watershed area. Nevertheless, existing residents have expressed concerns about the existing conditions. Recognizing this, the approach to the site drainage design is to address those concerns. As proposed, the project design would meet or exceed the City Standards for the City of Paso Robles and be required to comply with the requirements of the Regional Water Quality Control Board. The focus of the drainage design would be to utilize low impact development design to reduce impacts downstream. Additionally, improvements to the existing termination of Golden Hill Road could improve existing localized flooding issues.

With the significant amount of permeable surfaces, along with the utilization of LID techniques proposed with the project, impacts to storm water in relation to surface water absorption rate, drainage pattern, and water quality will be less than significant.

- d) Changes in the amount of surface water in any water body? (Sources: 1, 3, & 7)

Discussion: d: Since the project is being designed to absorb storm water into on-site biofiltration areas that will allow the water to percolate into the local aquifer and not into a water body, changes in the amount of surface water in any water body will be less than significant.

- e) Changes in currents, or the course or direction of water movement? (Sources: 1, 3, & 7)

Discussion: e: As discussed above, the project will have minimal pervious surfaces and will be designed to incorporate LID techniques. There will not be changes in currents, or the course or direction of water movement.

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| f) Change in the quantity of ground waters, either through direct additions or withdrawals, or through interception of an aquifer by cuts or excavations or through substantial loss of groundwater recharge capability?
(Sources: 1,3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: f: The proposed project would be connected to City water to provide potable water to each RV site, as well as water for the club houses, accessory buildings, and landscaping. RV parks are typically not high water users. The project landscaping is proposed to be drought tolerant. The area of the site where a majority of the grading is proposed is significantly higher in relation to the water table, and therefore impacts to the aquifer by cuts or excavation would be less than significant.

The site is designated in the general plan as Agriculture, with anticipation that the site would be used for Ag uses permitted in the zoning ordinance. A water usage analysis was prepared by North Coast Engineering that compared the water usage of agricultural activities such as if a vineyard was located on the site which is a typical agricultural use in the area. The water use projections for the RV Park are based on actual water use from an existing RV Park near the subject site. The calculation for the vineyard is based on an assumption of 80 acres of planted vineyards. The analysis indicates that the 332 space RV park would use approximately 11,096,808 gallons of water per year and the vineyard would use approximately 47,790,640 gallons per year. The analysis concludes that the vineyard would use four times the amount of water the RV resort would use. The analysis used for these calculations is attached as Exhibit G.

Therefore Impacts from the project versus what the property may otherwise likely be used for are less than what was anticipated with the General Plan and therefore would result in less than significant impacts on water resources. Additionally, the quantity of ground waters, either through direct additions or withdrawals, or through interception of an aquifer by cuts or excavations or through substantial loss of groundwater recharge capability will be less than significant.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| g) Altered direction or rate of flow of groundwater?
(Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: g: This project will not alter the direction or rate of flow of ground water.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| h) Impacts to groundwater quality? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion:h: See discussion above for a-c regarding LID techniques.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| i) Substantial reduction in the amount of groundwater otherwise available for public water supplies?
(Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: a – i: The change in zoning from Rural Agriculture to Parks and Open Space would allow for the RV Resort with a Conditional Use Permit. Generally, taking in consideration the water usage for the RV park and

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	Potentially Significant No Impact
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the fact that approximately 65-percent of the site will be preserved for open space, impacts to the availability of groundwater would be less than significant. Additionally, taking into consideration an alternative AG use such as vineyards, this project will not have significant demand on ground water.

V. AIR QUALITY. Would the proposal:

- | | | | | |
|---|--------------------------|-------------------------------------|--------------------------|-------------------------------------|
| a) Violate any air quality standard or contribute to an existing or projected air quality violation? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b) Expose sensitive receptors to pollutants? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| c) Alter air movement, moisture, or temperature? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Create objectionable odors? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion a – d:

The resort project has been reviewed by the San Luis Obispo Air Pollution Control District. See the attached letter (Attachment C) from the APCD indicating the necessary mitigation measures for the construction and operation phases of the project to reduce emissions from this project to a less than significant level. The mitigation measures are described as follows:

APCD-1 Prior to any grading on the site, the project proponent shall ensure that a geologic evaluation is conducted to determine if Naturally Occurring Asbestos (NOA) is present within the area that will be disturbed. If NOA is not present, an exemption form must be filed with the District. If NOA is found at the site the applicant must comply with all requirements outlined in the Asbestos (Air Toxics Control Measure) ACTM.

APCD-2 The project shall be conditioned to comply with all applicable District regulations pertaining to the control of fugitive dust (PM-10) as contained in section 6.5 of the Air Quality Handbook. All site grading and demolition plans noted shall list the following regulations:

- a. Reduce the amount of the disturbed area where possible.
- b. Use of water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site. Increased watering frequency would be required whenever wind speeds exceed 15 mph. Reclaimed (nonpotable) water should be used whenever possible.
- c. All dirt stockpile areas should be sprayed daily as needed.
- d. Permanent dust control measures identified in the approved project revegetation and landscape plans should be implemented as soon as possible following completion of any soil disturbing activities.
- e. Exposed ground areas that are to be reworked at dates greater than one month after initial grading should be sown with a fast-germinating native grass seed and watered until vegetation is established.
- f. All disturbed soil areas not subject to revegetation should be stabilized using approved chemical soil binders, jute netting, or other methods approved in advance by the APCD.

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Mitigation Incorporated	Less Than Significant Impact	No Impact

- g. All roadways, driveways, sidewalks, etc. to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used.
- h. Vehicle speed for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site.
- i. All trucks hauling dirt, sand, soil, or other loose materials are to be covered or should maintain at least two feet of freeboard (minimum vertical distance between top of load and top of trailer) in accordance with CVC Section 23114.
- j. Install wheel washers where vehicles enter and exit unpaved roads onto streets, or wash off trucks and equipment leaving the site.
- k. Sweep streets at the end of each day if visible soil material is carried onto adjacent paved roads. Water sweepers with reclaimed water should be used where feasible

APCD-3 Construction Permit Requirements:

If portable equipment, 50 horsepower or greater, are used during construction, a California statewide portable equipment registration (issued by the California Air Resources Board) or an APCD permit. The following list is provided as a guide to equipment and operations that may have permitting requirements, but should not be viewed as exclusive. For a more detailed listing, refer to page A-5 in the Districts CEQA Handbook.

- Power screens, conveyors, diesel engines, and/or crushers;
- Portable generators and equipment with engines that are 50hp or greater;
- IC Engines;
- Concrete batch plants;
- Rock and pavement crushing;
- Tub grinders; and
- Trommel screens.

APCD-4 Develop a comprehensive Construction Activity Management Plan designed to minimize the amount of large construction equipment operating during any given time period. The plan should be submitted to the District for review and approval prior to the start of construction. The plans should include but not be limited to the following elements:

- Schedule construction truck trips during non-peak hours to reduce peak hour emissions;
- Limit the length of the construction work-day period, if necessary; and,
- Phase construction activities, if appropriate.

APCD-5 Standard NOx Control Measures for Construction Equipment

The standard construction equipment mitigation measures for reducing nitrogen oxide (NOx) emissions are listed below and in section 6.3.1 of the Air Quality Handbook. **These measures are applicable to all projects where construction equipment will be used:**

- Maintain all construction equipment in proper tune according to manufacturer's specifications.
- Fuel all off-road and portable diesel powered equipment with ARB certified motor vehicle diesel fuel (non-taxed version suitable for use off-road).

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	Potentially Significant	Potentially Significant	Potentially Significant	Potentially Significant
	Unless Mitigation Incorporated	Unless Mitigation Incorporated	Unless Mitigation Incorporated	Unless Mitigation Incorporated
ISSUES (and Supporting Information Sources):	Less Than Significant Impact	Less Than Significant Impact	Less Than Significant Impact	No Impact

- Maximize to the extent feasible, the use of on-road heavy-duty equipment and trucks that meet the ARB’s 1998 or newer certification standard for on-road heavy-duty diesel engines.
- All on and off-road diesel equipment shall not be allowed to idle for more than 5 minutes. Signs shall be posted in the designated queuing areas and or job sites to remind drivers and operators of the 5 minute idling limit.

APCD 6 OPERATIONAL PHASE MITIGATION

Greenhouse Gas Impacts and Mitigation

While California successfully passed Assembly Bill 32, California's Global Solutions Act of 2006, little guidance was provided to lead agencies regarding how to address greenhouse gas (GHG) impacts in the CEQA process. In the 2007 California legislative session, Senate Bill 97 was passed and required that the California Office of Planning and Research, by July 1, 2009, prepare and develop guidelines for the feasible mitigation of GHG emissions or the effects of GHG emissions as required by CEQA, including, but not limited to, effects associated with transportation or energy consumption. As guidelines are not currently available, the APCD suggests that projects subject to CEQA should quantify project related GHG emissions and identify feasible mitigation.

The APCD staff considered the operational impact of this proposed development by running the URBEMIS2007 computer model, a tool for estimating vehicle travel, fuel use and the resulting emissions related to this project’s land uses. This indicated that operational phase impacts of the greenhouse gas known as carbon dioxide (CO2) will be approximately 7,277 pounds per day in the summer and 6,906 pounds per day in the winter. **While statewide/global thresholds have not yet been defined for GHG impacts, SLO County APCD recommends the implementation of feasible mitigation measures that minimize project related GHG impacts.** Examples of potential measures for this development include:

- Developments within Urban Reserve Lines with walking or bicycling access to nearby commercial and transit services thus reducing automobile dependence;
- Install on-site solar power infrastructure to offset grid-based power consumption;
- Provide low-speed neighborhood electric vehicles (NEVs) and charging stations for internal use by resort patrons;
- Include pedestrian amenities that provide improved connectivity to existing amenities;
- Securing shuttle services;
- Green building techniques such as:
 - Installing outdoor electrical outlets to encourage the use of electric appliances and tools;
 - Planting of native, drought resistant landscaping;
 - Use of locally or nearby produced building materials; and,
 - Use of renewable or reclaimed building materials.

Other measures suitable for GHG as well as ozone precursor mitigation are listed below in this comment letter.

Operational Permit Requirements

Based on the information provided, we are unsure of the types of equipment that may be present at the site. Operational sources may require APCD permits. The following list is provided as a guide to equipment and operations that may have permitting requirements, but should not be

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	Potentially Significant Impact	Potentially Significant Mitigation Incorporated	Potentially Significant Impact	Less Than Significant Impact	No Impact
ISSUES (and Supporting Information Sources):					

viewed as exclusive. For a more detailed listing, refer to page A-5 in the District's CEQA Handbook.

- Portable generators and equipment with engines that 50 hp or greater;
- Electric generation plants or the use of standby generator; and
- Cogeneration facilities.

VI. TRANSPORTATION/CIRCULATION.

Would the proposal result in:

- a) Increased vehicle trips or traffic congestion?
- (Sources: 1, 3, & 7)

Discussion: A Traffic Study was prepared by Associated Traffic Engineers (ATE) in January 2008 (Attachment D) to study the traffic and circulation affects of the proposed RV Resort project at the north end of Golden Hill Road.

The City Engineer reviewed the traffic study and provided the following determinations and conclusions:

The traffic study dated January 11, 2008, is based primarily on information found in the study for the Regency Center produced in August, 2007. These studies indicate that all of the intersections along the Highway 46E corridor will deteriorate below acceptable levels as defined by the current circulation element of the General Plan.

The City is in the process of developing a new City-wide traffic model for the purposes of updating the circulation element. In addition to verification of current studies, the model will test the effectiveness of a series of road connections not currently included in the Circulation Element. These include parallel routes that may help mitigate impacts of City developments on highway 46E and improved access across the highway, most likely at Union Road.

The Motorcoach traffic study references the City's on-going studies and indicates that the applicant should participate in the projects that are recommended. Participation typically occurs through impact fees. In accordance with General Plan policy, the City Council will update the traffic mitigation fees in accordance with an updated Circulation Element. The applicant should be aware that mitigation fees are collected upon occupancy in the amount in effect at that time.

Larry Newland, Transportation Planning Branch Manager, South for Caltrans District 5, reviewed the traffic study for the project and submitted a letter (attachment D-2) concluding that the best way to mitigate this project, and other projects in this area of the City was for the City to collect appropriate traffic impact fees to fund improvements that will be needed. The specific mitigation requiring the applicant to pay traffic impact and other development impact fees will be applied to this project. See mitigation measure T-1, below.

Typically all development projects construct improvements to adjacent streets in accordance with the Circulation Element. In this case the Circulation Element provides for the extension of Golden Hill Road north to Dry Creek Road. A bridge over the Huer Huero is needed for this connection. The applicant has submitted a preliminary design for the northerly extension of Golden Hill Road. Due to topographic constraints and projected traffic demands the northerly extension of Golden Hill Road is designed as a two lane road with bike lanes. The road will be constructed in phases along with the resort development.

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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Potentially Significant Less Than Impact	No Impact
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As a result of the implementation of the mitigation measures, the increase in traffic trips and congestion created by this project will be less than significant. The mitigation measures are included as follows:

T-1. The project will be subject to traffic impact and other development impact fees in effect at the time of occupancy of the project.

T-2. Golden Hill Road shall be constructed in general conformance to the preliminary plans, plans approved by the City Engineer, and in accordance with the phasing plan proposed.

- b) Hazards to safety from design features (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? (Sources: 1, 3, & 7)

Discussion: The project has been designed to have an entrance road off of Golden Hill Road. RVs will enter and leave the project from the one main entrance/exit. The entrance/exit has been designed to intersect Golden Hill road in a relatively flat area that will have plenty of site distance from traffic traveling on Golden Hill Road. Additionally it is not anticipated that there will be any conflicts with the Circle B residents entering and exiting Circle B Road. Therefore, this project will not result in hazards to safety from design features or incompatible uses.

- c) Inadequate emergency access or inadequate access to nearby uses? (Sources: 1, 3, & 7)

Discussion: As mentioned above the main entry and exit for the site is the main driveway located at the south end of the project. There is a secondary emergency access proposed to be located about midway along the project frontage on Golden Hill Road. The plan has been reviewed and accepted by the Emergency Services Department and therefore this project will not result in inadequate emergency access or inadequate access to nearby uses.

- d) Insufficient parking capacity on-site or off-site? (Sources: 1, 3, 7, & 8)

Discussion: The parking necessary to meet the Zoning Code requirements has been designed into the project, therefore there will not be an impact.

- e) Hazards or barriers for pedestrians or bicyclists? (Source: 7)

Discussion: The improvements required for Golden Hill Road will include the construction of a bike path as well as sidewalk. The project will not create a hazard or barriers for pedestrians or bicyclists.

- f) Conflicts with adopted policies supporting alternative transportation (e.g., bus turnouts, bicycle racks)? (Sources: 1 & 8)

The project will provide a private shuttle service for tenants of the RV park, but it will not necessitate the need to install a public bus stop. The project is not located on a current bus route.

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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	Potentially Significant No Impact
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g) Rail, waterborne or air traffic impacts?

Discussion: The project site is not adjacent to rail or waterway corridors and will have no impact. The project site is within the Paso Robles Airport Area though impacts to air traffic are not anticipated as a result of the project (Refer to Section IX Hazards for more discussion of potential airport impacts.)

VII. BIOLOGICAL RESOURCES.

Would the proposal result in impacts to:

- a) Endangered, threatened or rare species or their habitats (including but not limited to: plants, fish, insects, animals, and birds)?
- b) Locally designated species (e.g., heritage trees)?
- c) Locally designated natural communities (e.g., oak forest, coastal habitat, etc.)?
- d) Wetland habitat (e.g., marsh, riparian and vernal pool)?
- e) Wildlife dispersal or migration corridors?

Discussion a - e: The project site has six designated habitat types including annual grassland, blue oak woodland, riparian, ephemeral drainage, vernal pool, and seasonal wetland. The California Natural Diversity Database (CNDDB) identified several sensitive species with the potential to occur within the project area. A Biology Report was prepared December 2006, which was updated January 2008, attached as Exhibit E (Althouse & Meade). Additionally, a floristic survey was conducted from May through July 2006 (Althouse & Meade) that identified 162 species of plants on the property including 110 native species, 52 introduced species, with one special-status plant (shinning navarretia [Navarretia nigelliformis ssp. radians]) mapped on the site. Other special-status botanical species with the potential to occur (but not identified) include: Salinas Valley goldfields (Lasthenia leptalea), round-leaved erodium (Erodium macrophyllum), Douglas' spineflower (Chorizanthe douglasii) Obispo Indian paintbrush (Castilleja densiflora obispoensis), and dwarf calycadenia (Calycadenia villosa).

More than 108 animal species, including 11 special status species, have the potential to occur on the project site. Site surveys in 2006 observed Golden eagle (Aquila chrysaetos) and documented active nesting onsite (2 adults and 1 juvenile). (Althouse & Meade). The Golden eagle is a fully protected species under the California Endangered Species Act (CESA), and they are federally protected.

Other zoological species with the potential to occur include pallid bat (Antrozous pallidus), burrowing owl (Athene cunicularia), vernal pool fairy shrimp (Branchinecta lynchi), white-tailed kite (Elaenus leucurus), loggerhead shrike (Lanius ludovicianus), California linderiella (Linderiella occidentalis), western spadefoot toad (Spea hammondii), American badger (Taxidea taxus), and San Joaquin Kit Fox(SJKF) (Vulpes macrotis mutica).

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ISSUES (and Supporting Information Sources):				

The property is within a strategic section of the SJKF migration corridor, therefore specific, unique project design and mitigation measures are incorporated into this project as recommended through consultation with the U.S. Fish and Wildlife Service (USFWS) and the California Department of Fish and Game (CDFG). The applicant modified the site development plan to reduce the area of impact to the San Joaquin Kit Fox habitat and migration corridor by reducing the proposed area to be developed and by increasing the width of the migration corridor by an additional 250 feet. Additionally, Althouse and Meade provided a supplemental letter (See Exhibit E-2) providing additional mitigation measures that would require some on and off site improvements in addition to the standard kit fox mitigation measures.

There are no wetlands on the property or designated natural communities. Oak woodland and grassland are the prominent vegetation types on the property. Vernal pools, a sensitive natural community of local concern, are present on the property, however, after conducting vernal pool surveys it has been determined that they do not contain the federally protected Fairy Shrimp species.

The applicant has also consulted with the agencies regarding Golden Eagle nesting birds and associated regulations. Additional site construction and operational mitigations were submitted to protect them from potential impacts. The project biologist submitted detailed onsite mitigation strategies to avoid and reduce potential impacts to these species. These mitigation measures are provided below. The project is designed to avoid all the vernal pool locations and areas with floral species shining nararretia.

A & T Arborists prepared an Arborist Report for the project site, which includes an inventory and survey of all trees (blue oaks and valley oaks) on the property. The inventory documented approximately 336 oak trees are located within the development area of the project (approximately 600 are located on the total site). A total of 33 oak trees are proposed to be removed to accommodate this project and the extension of Golden Hill Road. Five of the trees are already dead, and most of the trees proposed to be removed are in poor to very poor health. The development will be required to mitigate the tree removals and replant new oak trees in compliance with the City's Oak Tree Ordinance. The Arborist report is provided in attachment C, and the oak tree mitigation measures are included below.

The proposed project will have impacts on biological resources, however, with the implementation of the following mitigation measures, biological impacts will be less than significant:

- BR-1. Avoidance and protection of vernal pools on the property.** Vernal pools shall be avoided and protected where possible. If listed fairy shrimp species are found in vernal pools on the property, the vernal pools shall be avoided and a 50-foot setback distance shall be observed for all activities. If rare species are not found and vernal pools cannot be avoided, a vernal pool mitigation plan shall be prepared by a qualified biologist that specifies creation of vernal pool habitat in kind at a one to one ratio within open space areas on the property.
- BR-2. Interpretive signs shall be developed in cooperation with the project biologist** to inform guests at the Resort of the sensitive biological resources located on and near the property. Signs shall be placed on at least two sides of all vernal pools or vernal pool complexes that remain within the project open space areas. The signs shall provide general information about vernal pools in the Paso Robles region, including potential rare species that could be present.
- BR-3.** Tree canopies and trunks within 50 feet of proposed disturbance zones should be mapped and numbered by a certified arborist or qualified biologist and a licensed land surveyor. Data for each tree should include

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	Unless Mitigation Incorporated	Unless Mitigation Incorporated	Unless Mitigation Incorporated	Unless Mitigation Incorporated
	Less Than Significant Impact	Less Than Significant Impact	Less Than Significant Impact	Less Than Significant Impact
ISSUES (and Supporting Information Sources):				No Impact

date, species, number of stems, diameter at breast height (dbh) of each stem, critical root zone (CRZ) diameter, canopy diameter, tree height, health, habitat notes, and nests observed.

- BR-4.** An oak tree protection plan shall be prepared and approved by the City of Paso Robles.
- BR-5.** Impacts to the oak canopy or critical root zone (CRZ) should be avoided where practicable. Impacts include pruning, any ground disturbance within the dripline or CRZ of the tree (whichever distance is greater), and trunk damage.
- BR-6.** Impacts to oak trees shall be assessed by a licensed arborist. Mitigations for impacted trees shall comply with the City of Paso Robles tree ordinance.
- BR-7.** Replacement oaks for removed trees must be equivalent to 25% of the diameter of the removed tree(s). For example, the replacement requirement for removal of two trees of 15 inches dbh (30 total diameter inches), would be 7.5 inches (30" removed x 0.25 replacement factor). This requirement could be satisfied by planting five 1.5 inch trees, or three 2.5 inch trees, or any other combination totaling 7.5 inches. A minimum of two 24 inch box, 1.5 inch trees shall be required for each oak tree removed.
- BR-8.** Replacement trees should be seasonally maintained (browse protection, weed reduction and irrigation, as needed) and monitored annually for at least 7 years. Replacement trees shall be of local origin, and of the same species as was impacted or removed.
- BR-9.** **Within one week of ground disturbance or tree removal/trimming activities**, if work occurs between March 15 and August 15, nesting bird surveys shall be conducted. To avoid impacts to nesting birds, grading and construction activities that affect trees and grasslands shall not be conducted during the breeding season from March 15 to August 15. If construction activities must be conducted during this period, nesting bird surveys shall take place within one week of habitat disturbance. If surveys do not locate nesting birds, construction activities may be conducted. If nesting birds are located, no construction activities shall occur within 100 feet of nests until chicks are fledged. Construction activities shall observe a 300-foot buffer for occupied raptor nests. A 500-foot buffer shall be observed from occupied nests of all special status species (refer to BR-12 and BR-13). A pre-construction survey report shall be submitted to the lead agency immediately upon completion of the survey. The report shall detail appropriate fencing or flagging of the buffer zone and make recommendations on additional monitoring requirements.
- BR-10.** To prevent disturbance to nesting eagles, if construction is planned between January 30th and August 15th, a pre-construction survey should be conducted to determine if eagles are present. If eagles are not present after March 15th, work could commence. If eagles are present on the nest, work within 500 feet of the occupied nest should be delayed until after either adult eagles have left the nest, or eagle chicks have fledged and are no longer dependant on the nest as determined by a qualified biologist. At the commencement of work, a qualified biologist should monitor the eagles. If commencement of construction disturbs the eagles, the qualified monitor would be authorized to stop construction activity within range of the nest that causes disturbance to the eagles. Work within that area could commence once the eagle chicks have fledged and are no longer dependant on the nest.
- BR-11.** **If the project design cannot avoid shining navarretia on the property**, a mitigation and monitoring plan shall be developed by the project biologist to replace lost navarretia habitat at a 1:1 ratio on-site. The mitigation plan will provide details on appropriate mitigation sites, seed collection and distribution methods, and maintenance and monitoring requirements.

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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Mitigation Incorporated	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- BR-12. Interpretive signs shall be developed in cooperation with the project biologist** to inform guests at the Resort of the sensitive biological resources located on and near the property. Signage shall be placed on all sides of the rare plant occurrence, and shall have specific information about the plant and its ecology, including photographs.
- BR-13. All occupied nests shall be mapped using GPS or survey equipment.** The mapped locations shall be placed on a copy of the grading plans with a 500-foot buffer indicated. Work shall not be allowed within the 500 foot buffer while the nest is in use by eagles. The buffer zone shall be delineated on the ground with orange construction fencing where it overlaps work areas.
- BR-14. Occupied nests of special status bird species that are within 500 feet of project work areas shall be monitored bi-monthly** through the nesting season to document nest success and check for project compliance with buffer zones. Once nests are deemed inactive and/or chicks have fledged and are no longer dependant on the nest, work can commence.
- BR-15. Interpretive signs shall be developed in cooperation with the project biologist** to inform guests at the Resort of the sensitive biological resources located on and near the property. If the golden eagle nest continues to be occupied seasonally at the time the Resort opens to the public, signs shall be placed on the hilltop to exclude entry within approximately 300 feet of the eagle nest.
- BR-16. Prior to removal of any trees over 20 inches dbh,** a survey shall be conducted by a qualified biologist to determine if any of the trees proposed for removal or trimming harbor sensitive bat species or maternal bat colonies. Maternal bat colonies may not be disturbed.
- BR-17. The following supplemental measures for kit fox protection** are from the December 17, 2008 letter from Dan Meade of Althouse and Meade, Inc. The following measures when employed on the site, would reduce potential impacts to the San Joaquin Kit Fox. The consideration of these additional measures and the substantial widening of the proposed open space corridor, adjustment of the offsite mitigation requirements may be appropriate. The reduction of the mitigation ratio for kit fox payments from four to one to three to one can be made with review and approval by the Department of Fish and Game.
1. Kit fox friendly fencing shall be incorporated into all fences on the property. For chain link, wildlife, no-climb, or other wire fences with openings, at ground level less than eight inches square, kit fox passages shall be made in the fences every 100 yards. Passages shall be created by cutting wire and placing spreader bars to form a smooth 8-inch wide by 12-inch high, or as specified by the Endangered Species Recovery Program. In solid walls, an 8-inch diameter concrete pipe shall be placed at ground level in the wall every 100 yards.
 2. Four SJKF escape dens and a chambered den shall be constructed as per guidelines provided in the Endangered Species Recovery Program. The precise location of each den shall be designated in the field by a qualified kit fox biologist.
 3. BR-31. All pets on the property shall be kept on a leash at all times. Owners shall be required to clean up after their pets. Resort maintenance personnel shall conduct daily clean up on the property to remove pet waste.
 4. BR 32. Lighting shall be shielded to prevent direct lighting of the riparian corridor. All lighting shall be directed down and shall be low intensity.
 5. BR 33. Use of poisons including rodenticides on the property should be restricted. This is necessary

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	Less Than Significant Impact	Less Than Significant Impact	Less Than Significant Impact	Less Than Significant Impact
ISSUES (and Supporting Information Sources):				No Impact

to prevent primary or secondary poisoning of kit foxes and the depletion of prey populations on which they depend. All uses of such compounds should observe labels and other restrictions, mandated by the U.S. Agriculture, and other State and Federal legislation, as well as additional project-related restrictions deemed necessary by the Service. If rodent poison must be utilized, zinc phosphate should be used because of proven lower risk to kit fox. (U.S. Fish and Wildlife Service, 1999).

6. Quiet hours shall be observed after 10 pm every night to reduce disturbance. Generators will **not** be permitted to run at the resort at any time.
7. Speed limits. To avoid accidental injury to animals on the property a speed limit of 10 miles per hour shall be enforced on the property for all vehicles. Speed limits shall be posted at the entrance gate and throughout roadways on the property.
8. To enhance habitat for use by kit fox vegetation management shall be conducted on neighborhood properties, including the City sewer facility on the north bank of the Heur Heuro Creek adjacent to the Paso Robles Motorcoach Resort property. Work shall consist of removal of overgrown vegetation and removal of barrier fence when appropriate.
9. Neighborhood fencing improvements shall be conducted where fencing is a barrier to kit fox movement on properties adjacent to the Paso Robles Motorcoach property. Improvements will consist of either replacement of fences with kit fox friendly fencing, or creation of kit fox passages in existing fences every 100 yards where feasible.

BR-18. Prior to issuance of grading and/or construction permits, the applicant shall submit evidence to the County of San Luis Obispo, Department of Planning and Building, Environmental and Resource Management Division (County) (see contact information below) that states that one or a combination of the following three San Joaquin kit fox mitigation measures has been implemented:

- a. Provide for the protection in perpetuity, through acquisition of fee or a conservation easement of **219.3 (73.1 disturbed area x3)** acres of suitable habitat in the kit fox corridor area (e.g. within the San Luis Obispo County kit fox habitat area, northwest of Highway 58), either on-site or off-site, and provide for a non-wasting endowment to provide for management and monitoring of the property in perpetuity. Lands to be conserved shall be subject to the review and approval of the California Department of Fish and Game (Department) and the County.

This mitigation alternative (a.) requires that all aspects of this program must be in place before County permit issuance or initiation of any ground disturbing activities.

- b. Deposit funds into an approved in-lieu fee program, which would provide for the protection in perpetuity of suitable habitat in the kit fox corridor area within San Luis Obispo County, and provide for a non-wasting endowment for management and monitoring of the property in perpetuity.

Mitigation alternative (b) above, can be completed by providing funds to The Nature Conservancy (TNC) pursuant to the Voluntary Fee-Based Compensatory Mitigation Program (Program). The Program was established in agreement between the Department and TNC to preserve San Joaquin kit fox habitat, and to provide a voluntary mitigation alternative to project proponents who must mitigate the impacts of projects in accordance with the California Environmental Quality Act (CEQA). The fee, payable to "The Nature Conservancy", would total **\$548,250**. This fee is calculated based on the current cost-per-unit of \$2500 per acre of mitigation, which is scheduled to be adjusted to address the increasing cost of property in San Luis Obispo County; your actual cost may increase depending on the

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		Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
ISSUES (and Supporting Information Sources):				

timing of payment. This fee must be paid after the Department provides written notification about your mitigation options but prior to County permit issuance and initiation of any ground disturbing activities.

- c. Purchase 219.3 credits in a Department-approved conservation bank, which would provide for the protection in perpetuity of suitable habitat within the kit fox corridor area and provide for a non-wasting endowment for management and monitoring of the property in perpetuity.

Mitigation alternative (c) above, can be completed by purchasing credits from the Palo Prieto Conservation Bank (see contact information below). The Palo Prieto Conservation Bank was established to preserve San Joaquin kit fox habitat, and to provide a voluntary mitigation alternative to project proponents who must mitigate the impacts of projects in accordance with the California Environmental Quality Act (CEQA). The cost for purchasing credits is payable to the owners of The Palo Prieto Conservation Bank, and would total **\$548,250**. This fee is calculated based on the current cost-per-credit of \$2500 per acre of mitigation. The fee is established by the conservation bank owner and may change at any time. Your actual cost may increase depending on the timing of payment. Purchase of credits must be completed prior to County permit issuance and initiation of any ground disturbing activities.

BR-19. Prior to issuance of grading and/or construction permits, the applicant shall provide evidence that they have retained a qualified biologist acceptable to the City. The retained biologist shall perform the following monitoring activities:

- i. **Prior to issuance of grading and/or construction permits and within 30 days prior to initiation of site disturbance and/or construction**, the biologist shall conduct a pre-activity (i.e. pre-construction) survey for known or potential kit fox dens and submit a letter to the City reporting the date the survey was conducted, the survey protocol, survey results, and what measures were necessary (and completed), as applicable, to address any kit fox activity within the project limits.
- ii. **The qualified biologist shall conduct weekly site visits during site-disturbance activities** (i.e. grading, disking, excavation, stock piling of dirt or gravel, etc.) that proceed longer than 14 days, for the purpose of monitoring compliance with required Mitigation Measures BR-19 through BR-26. Site disturbance activities lasting up to 14 days do not require weekly monitoring by the biologist unless observations of kit fox or their dens are made on-site or the qualified biologist recommends monitoring for some other reason (see BR-19iii). When weekly monitoring is required, the biologist shall submit weekly monitoring reports to the City.
- iii. **Prior to or during project activities**, if any observations are made of San Joaquin Kit fox, or any known or potential San Joaquin kit fox dens are discovered within the project limits, the qualified biologist shall re-assess the probability of incidental take (e.g. harm or death) to kit fox. At the time a den is discovered, the qualified biologist shall contact USFWS and the CDFG for guidance on possible additional kit fox protection measures to implement and whether or not a Federal and/or State incidental take permit is needed. If a potential den is encountered during construction, work shall stop until such time the USFWS determines it is appropriate to resume work.

If incidental take of kit fox during project activities is possible, **before project activities commence**, the applicant must consult with the USFWS. The results of this consultation may require the applicant to obtain a Federal and/or State permit for incidental take during project activities. The applicant should be aware that the presence of kit foxes or known or potential kit fox dens at the project site could result in further delays of project activities.

- iv. **In addition**, the qualified biologist shall implement the following measures:

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ISSUES (and Supporting Information Sources):

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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1. **Within 30 days prior to initiation of site disturbance and/or construction**, fenced exclusion zones shall be established around all known and potential kit fox dens. Exclusion zone fencing shall consist of either large flagged stakes connected by rope or cord, or survey laths or wooden stakes prominently flagged with survey ribbon. Each exclusion zone shall be roughly circular in configuration with a radius of the following distance measured outward from the den or burrow entrances:
 - Potential kit fox den: 50 feet
 - Known or active kit fox den: 100 feet
 - Kit fox pupping den: 150 feet
2. All foot and vehicle traffic, as well as all construction activities, including storage of supplies and equipment, shall remain outside of exclusion zones. Exclusion zones shall be maintained until all project-related disturbances have been terminated, and then shall be removed.
3. If kit foxes or known or potential kit fox dens are found on site, daily monitoring by a qualified biologist shall be required during ground disturbing activities.

Monitoring: Required prior to issuance of a grading and/or construction permit. Compliance will be verified by the City Planning Division.

BR-20. Prior to issuance of grading and/or construction permits, the applicant shall clearly delineate the following as a note on the project plans: *“Speed signs of 25 mph (or lower) shall be posted for all construction traffic to minimize the probability of road mortality of the San Joaquin kit fox”*. Speed limit signs shall be installed on the project site **within 30 days prior to initiation of site disturbance and/or construction**.

BR-21. During the site disturbance and/or construction phase, grading and construction activities after dusk shall be prohibited unless coordinated through the City, during which additional kit fox mitigation measures may be required.

BR-22. Prior to issuance of grading and/or construction permit and within 30 days prior to initiation of site disturbance and/or construction, all personnel associated with the project shall attend a worker education training program, conducted by a qualified biologist, to avoid or reduce impacts on sensitive biological resources (i.e. San Joaquin kit fox). At a minimum, as the program relates to the kit fox, the training shall include the kit fox’s life history, all mitigation measures specified by the City, as well as any related biological report(s) prepared for the project. The applicant shall notify the City shortly prior to this meeting. A kit fox fact sheet shall also be developed prior to the training program, and distributed at the training program to all contractors, employers and other personnel involved with the construction of the project.

BR-23. During the site-disturbance and/or construction phase, to prevent entrapment of the San Joaquin kit fox, all excavations, steep-walled holes and trenches in excess of two feet in depth shall be covered at the close of each working day by plywood or similar materials, or provided with one or more escape ramps constructed of earth fill or wooden planks. Trenches shall also be inspected for entrapped kit fox each morning prior to onset of field activities and immediately prior to covering with plywood at the end of each working day. Before such holes or trenches are filled, they shall be thoroughly inspected for entrapped kit fox. Any kit fox so discovered shall be allowed to escape before field activities resume, or removed from the trench or hole by a qualified biologist and allowed to escape unimpeded.

10 Environmental Checklist Form

	Potentially Significant	Potentially Significant	Potentially Significant	Potentially Significant
	Unless Mitigation Incorporated	Less Than Significant Impact		
ISSUES (and Supporting Information Sources):				No Impact

BR-24. During the site-disturbance and/or construction phase, any pipes, culverts, or similar structures with a diameter of four inches or greater, stored overnight at the project site shall be thoroughly inspected for trapped San Joaquin kit foxes before the subject pipe is subsequently buried, capped, or otherwise used or moved in any way. If during the construction phase a kit fox is discovered inside a pipe, that section of pipe will not be moved. If necessary, the pipe may be moved only once to remove it from the path of activity, until the kit fox has escaped.

BR-25. During the site-disturbance and/or construction phase, all food-related trash items such as wrappers, cans, bottles, and food scraps shall be disposed of only in closed containers. These containers shall be regularly removed from the site. Food items may attract San Joaquin kit foxes onto the project site, consequently exposing such animals to increased risk of injury or mortality. No deliberate feeding of wildlife shall be allowed.

BR-26. Prior to, during and after the site-disturbance and/or construction phase, use of pesticides or herbicides shall be in compliance with all local, State and Federal regulations. This is necessary to minimize the probability of primary or secondary poisoning of endangered species utilizing adjacent habitats, and the depletion of prey upon which San Joaquin kit foxes depend.

BR-27. During the site-disturbance and/or construction phase, any contractor or employee that inadvertently kills or injures a San Joaquin kit fox or who finds any such animal either dead, injured, or entrapped shall be required to report the incident immediately to the applicant and City. In the event that any observations are made of injured or dead kit fox, the applicant shall immediately notify the USFWS and CDFG by telephone. In addition, formal notification shall be provided in writing within three working days of the finding of any such animal(s). Notification shall include the date, time, location and circumstances of the incident. Any threatened or endangered species found dead or injured shall be turned over immediately to CDFG for care, analysis, or disposition.

BR-28. Prior to final inspection, or occupancy, whichever comes first, should any long internal or perimeter fencing be proposed or installed, the applicant shall do the following to provide for kit fox passage:

- i. If a wire strand/pole design is used, the lowest strand shall be no closer to the ground than 12 inches.
- ii. If a more solid wire mesh fence is used, 8" x 12" openings near the ground shall be provided every 100 yards
- iii. Upon fence installation, the applicant shall notify the City to verify proper installation. Any fencing constructed after issuance of a final permit shall follow the above guidelines

Monitoring (San Joaquin Kit Fox Measures BR-18 to BR-26): Compliance will be verified by the City of Paso Robles, Planning Division in consultation with the California Department of Fish and Game. As applicable, each of these measures shall be included on construction plans.

BR-29. A pre-construction survey shall be conducted within thirty days of beginning work on the project to identify if badgers are using the site. The results of the survey shall be sent to the project manager, CDFG, and the City of Paso Robles.

If the pre-construction survey finds potential badger dens, they shall be inspected to determine whether they

10 Environmental Checklist Form

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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ISSUES (and Supporting Information Sources):

are occupied. The survey shall cover the entire property, and shall examine both old and new dens. If potential badger dens are too long to completely inspect from the entrance, a fiber optic scope shall be used to examine the den to the end. Inactive dens may be excavated by hand with a shovel to prevent re-use of dens during construction. If badgers are found in dens on the property between February and July, nursing young may be present. To avoid disturbance and the possibility of direct take of adults and nursing young, and to prevent badgers from becoming trapped in burrows during construction activity, no grading shall occur within 100 feet of active badger dens between February and July. Between July 1 and February 1 all potential badger dens shall be inspected to determine if badgers are present. During the winter badgers do not truly hibernate, but are inactive and asleep in their dens for several days at a time. Because they can be torpid during the winter, they are vulnerable to disturbances that may collapse their dens before they rouse and emerge. Therefore, surveys shall be conducted for badger dens throughout the year. If badger dens are found on the property during the pre-construction survey, the CDFG wildlife biologist for the area shall be contacted to review current allowable management practices.

VIII. ENERGY AND MINERAL RESOURCES. Would the proposal:

- a) Conflict with adopted energy conservation plans?
 (Sources: 1)

Discussion: The proposed project will not conflict with adopted energy conservation plans. The structures constructed on the site will be required to comply with California Energy Code.

- b) Use non-renewable resources in a wasteful and inefficient manner? (Sources: 1)

Discussion: The project will not use or promote the use of non-renewable resource in a wasteful and inefficient manner.

- c) Result in the loss of availability of a known mineral resource that would be of future value to the region and the residents of the State? (Sources: 1, 7)

Discussion: The project is not located in an area of known mineral resources that would be of future value to the region and the residents of the State.

IX. HAZARDS. Would the proposal involve:

- a) A risk of accidental explosion or release of hazardous substances (including, but not limited to: oil, pesticides, chemicals or radiation)?

Discussion: The proposed project does not include the use, transport, or storage of hazardous materials and will not result in a risk of accidental explosion or release of hazardous substances.

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- b) Possible interference with an emergency response plan or emergency evacuation plan? (Sources: I & 7)

Discussion: The proposed project will not interfere with an emergency response plan or emergency evacuation plan since it is not a designated emergency response location to be used for staging or other uses in an emergency.

- c) The creation of any health hazard or potential hazards?

Discussion: All projects in the Airport SubArea/Overlay, including policy and regulation modifications, must be consistent with the ALUP (Refer to ALUP Section 4.5). The majority of the site is located in Safety Zone 4 for outer approach and departure zones with a small portion of the site in Zone 3 for turning and sideline zones. Incorporation of the mitigation measures identified below will ensure compliance with the ALUP and reduce potentially significant effects of airport-related hazards to a less than significant level.

Hazard Mitigation Measures

H-1 – Airport and Aircraft Safety: *Development of any new land use on the project site shall not create an undue public safety risk from overflight of aircraft. The eastern portion of project site is in Airport Safety Zone 3 for turning and sideline zones and the western portion is Safety Zone 4 for outer approach and departure zones. All development plan, proposed use, or subdivision on the project site is subject to the nonresidential land use densities and open space requirements as provided in Chapter 4 of the Paso Robles ALUP which are excerpted below (Table 5, ALUP, 2007).*

Handley Property Airport Safety Areas	Maximum Land Use Density (persons/acre)	Maximum Single Acre Land Use Density (persons/acre)	Maximum Percent Open Space (% gross area)
Safety Zone 3	60	120	25 ²
Safety Zone 4	40	120	20 ²

¹ No structures, congregations of equipment or vehicles, or public venues shall be located within 250 feet of any extended runway centerline and within 6000 feet of the corresponding runway end.

²When feasible, development should be planned in a manner that maintains maximum open space within 50 feet of any extended runway centerline.

H-2 - Airspace Protection: *No object or structure may be erected, and no plant allowed to grow, to penetrate any “imaginary surface” as defined in Federal Aviation Regulations Part 77. Any proposed feature approaching these surfaces will be referred to the airport manager for review and recommendation. Building within the height limits of this specific plan will not approach the FAA imaginary surfaces.*

H-3 - Operations Interference: *No use shall be established which produces visually significant quantities of smoke.*

H-4 - Bird Attractants: *No use shall be established and no activity conducted which attracts birds to the extent of creating a significant hazard of bird strikes. Examples are outdoor storage or disposal of food or grain, or large, artificial water features. This provision is not intended to prevent enhancement or protection of existing wetlands, the mitigation of impacts to wetlands or construction of required detention basins.*

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Mitigation Incorporated	Unless Mitigation Incorporated	Less Than Significant Impact	Potentially Significant No Impact
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H-5 Avigation Easements: At the time of subdivision development, avigation easements shall be recorded for each affected parcel in a form approved by the County of San Luis Obispo Airport Land Use Commission.

H-6 Real Estate Disclosure: All owners, potential purchasers, occupants (whether as owners or renters), and potential occupants (whether as owners or renters) shall receive full and accurate disclosure concerning the noise, safety, or overflight impacts associated with airport operations prior to entering any contractual obligation to purchase, lease, rent, or otherwise occupy any property or properties within the airport area. The format of the disclosure shall be approved by the County of San Luis Obispo Airport Land Use Commission.

- d) Increased fire hazard in areas with flammable brush, grass, or trees?

Discussion: The project site is not located in an area with the potential for increased fire hazards. The site will be required to be in compliance with City and County brush and grass clearance requirements.

X. NOISE. Would the proposal result in:

- a) Increases in existing noise levels? (Sources: 1, 7, & 8)

Discussion: The project will not likely result in a significant increase in operational noise levels. The applicant will implement night time curfew hours for noise of 10pm. Construction activities may result in short-term construction noise; however, construction noise will be limited to specific daytime hours per City regulations.

- b) Exposure of people to severe noise levels? (Source: 3)

Discussion: The proposed project is not anticipated to expose people to severe noise levels. The RV Park is considered transient in nature and therefore not subject to special regulations regarding airport noise.

XI. PUBLIC SERVICES. Would the proposal have an effect upon, or result in a need for new or altered government services in any of the following areas:

- a) Fire protection? (Sources: 1, 3, 6, & 7)
- b) Police Protection? (Sources: 1, 3, & 7)
- c) Schools? (Sources: 1, 3, & 7)
- d) Maintenance of public facilities, including roads? (Sources: 1, 3, & 7)
- e) Other governmental services? (Sources: 1,3, & 7)

Discussion: a.-e. The proposed project includes a General Plan Amendment (GPA) and zoning change from
Initial Study-Page 29 **Agenda Item #2 Page 57 of 180**

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Agriculture to Park and Open Space (POS), along with the proposal to develop a 332 space RV Resort. It is not anticipated that this project will impact governmental services to a significant level.

XII. UTILITIES AND SERVICE SYSTEMS. Would the proposal result in a need for new systems or supplies, or substantial alterations to the following utilities:

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a) Power or natural gas? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b) Communication systems? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c) Local or regional water treatment or distribution facilities? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| d) Sewer or septic tanks? (Sources: 1, 3, 7, & 8) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| e) Storm water drainage? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| f) Solid waste disposal? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| g) Local or regional water supplies? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Discussion: a.-g. The proposed project includes a General Plan Amendment (GPA) and zoning change from Agriculture to Park and Open Space (POS), along with the proposal to develop a 332 space RV Resort. The project will require extending utility services to the site including sewer and water service and others such as gas, power and cable TV. The project will be required to mitigate impacts in the form of facilities or development impact fees.

XIII. AESTHETICS. Would the proposal:

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|-------------------------------------|
| a) Affect a scenic vista or scenic highway? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Have a demonstrable negative aesthetic effect? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Discussion for a-b: Golden Hill Road is not considered scenic highway and this site would not be considered a scenic vista. However, the Circle B residences who live on large single family residential parcels to the east have voiced concerns with this project, mainly aesthetic visual concerns.

The designers of the project have taken particular care to design the project in an manner that places the RV spaces in areas of the site that are less visible from a public street and Circle B residences to the west. Additionally the project has been designed to have a decorative screen wall along with dense landscaping to further help screen the RVs from views of the Circle B properties.

Since the site is not located on a scenic highway of vista, and since the project has been designed to screen the views from the Circle B residential properties, the project would not have a demonstrable negative aesthetic

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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effect.

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| c) Create light or glare? (Sources: 1, 3, 7, & 8) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: Elevated light levels may be experienced on site as a result from this development on the project site, but all light fixtures will be shielded and downcast as required per city regulations.

XIV. CULTURAL RESOURCES. Would the proposal:

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|-------------------------------------|
| a) Disturb paleontological resources? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Disturb archaeological resources? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c) Affect historical resources? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Have the potential to cause a physical change which would affect unique ethnic cultural values? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e) Restrict existing religious or sacred uses within the potential impact area? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion: a. through e. No known paleontological resources are located in the vicinity. There are no known religious or sacred uses on or near the project site. The project is not proposed in a location where it could affect unique ethnic cultural values. The project site is located in the vicinity of known prehistoric and historic resources. A Phase I archaeological reconnaissance and a historical records search of the project site (Parker & Associates, May 14, 2006, attached as Exhibit F) did not identify the presence of significant prehistoric or intact historic resources. Evidence of historical agricultural uses included a foundation, wells, and irrigation materials. Since the project site is located along the Huer Huero Creek, potentially hidden or buried resources may be present; it is recommended that an archaeological monitor be present during initial grubbing/grading activities on the site. If buried remains or otherwise hidden resources are discovered during grading and excavation activities, additional standard mitigation measures would apply. Those mitigation are as follows:

Cultural Resources Mitigation Measures

CR-1: Prior to issuance of development permits, the applicant shall retain a qualified historic archaeologist to monitor initial grubbing and grading on the site and to develop a recovery program if necessary. The monitor shall have the authority to stop work in the event potentially significant cultural resources are discovered.

CR-2: In the event archaeological resources are unearthed or discovered during any construction activities, the following standards apply:

- a. Construction activities shall cease, and the Community Development Director shall be notified so that the extent and location of discovered materials may be recorded by a qualified archaeologist, and disposition of artifacts may be accomplished in accordance with state and federal law.
- b. In the event archaeological resources are found to include human remains, or in any other case where human remains are discovered during construction, the County Coroner is to be notified in addition to the Community Development Director so that proper disposition may be accomplished.

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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XV. RECREATION. Would the proposal:

- a) Increase the demand for neighborhood or regional parks or other recreational facilities? (Sources: 1, 3, & 7)

Discussion: The project is non-residential and will not affect the demand for parks and recreational facilities. The rezone would allow for potential future recreational facilities.

- b) Affect existing recreational opportunities? (Sources 1, 3, & 7)

Discussion: The proposed project would change the zoning from Agriculture to Parks and Open Space. The City's Parks and Recreation element states that lands within the floodplain Huerhuero Creek are potential sites for development of public park and passive recreational uses. The proposed GPA/rezone would allow for the development potential of future recreational projects and would not affect existing recreational opportunities.

XVI. MANDATORY FINDINGS OF SIGNIFICANCE.

- a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? (Sources: 1 & 3)

Discussion: With the proposed conditions of approval along with required mitigation measures, the proposed project will not in itself degrade the quality of the environment or impact habitat or populations of listed plant animal species. Significant existing natural resources have been identified on the project site and mitigation measures are recommended to minimize potential impacts from the project to less than significant levels.

- b) Does the project have the potential to achieve short-term, to the disadvantage of long-term environmental goals? (Sources: 1 & 3)

Discussion: The project will not likely have a potential to achieve short-term, to the disadvantage of long-term environmental goals.

- c) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future **Agenda Item #2 Page 60 of 180**)

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):

Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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projects.) (Sources: 1 & 3)

Discussion: The project will not result in significant cumulative impacts.

- d) Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly? (Sources: 1 & 3)
- | | | | |
|--------------------------|--------------------------|--------------------------|-------------------------------------|
| <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The project will not result in substantial adverse environmental impacts on human beings, either directly or indirectly.

11. EARLIER ANALYSIS AND BACKGROUND MATERIALS

Earlier analyses may be used where, pursuant to tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or negative declaration. Section 15063 (c)(3)(D). The earlier documents that have been used in this Initial Study are listed below.

Reference Number	Document Title	Available for Review At
1	City of Paso Robles General Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
2	Seismic Safety Element for City of Paso Robles	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
3	Final Environmental Impact Report City of Paso Robles General Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
4	Soil Survey of San Luis Obispo County, California Paso Robles Area	USDA-NRCS, 65 Main Street-Suite 108 Templeton, CA 93465
5	Uniform Building Code	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
6	City of Paso Robles Standard Conditions of Approval For New Development	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
7	City of Paso Robles Zoning Code	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
8	City of Paso Robles, Water Master Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
9	City of Paso Robles, Sewer Master Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
10	Federal Emergency Management Agency Flood Insurance Rate Map	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446

12. Attachments:

Exhibit A-1 – Vicinity Map

Exhibit A-2 – Site Plan

Exhibit A-3 – Creek Front Modification Map

Exhibit B – Mitigation Summary Table

The following Exhibits (C thru H) are available upon request in the Community Development Department as well as on the City website at www.prcity.com

Exhibit C – APCD Letter

Exhibit D-1 – Traffic and Circulation Study

Exhibit D-2 Letter from Larry Newland, Caltrans District 5

Exhibit E-1 – Preliminary Biological Assessment

Exhibit E-2 – Althouse & Meade Dec. 17, 2008 letter

Exhibit E-3 – Dry Season Sampling

Exhibit E-4 – Althouse & Meade Sept. 27, 2007 letter

Exhibit E-5 – Kit Fox Habitat Evaluation Form

Exhibit F - Arborist Report

Exhibit G - Cultural Resource Investigation

Exhibit H – Water Usage Analysis

Exhibit A

Vicinity Map



Mundee General Plan Amendment and Rezone
Golden Hill Rd, north of Highway 46
City of Paso Robles



SITE STATISTICS - OVERVIEW

RECORD OWNER
 PASO ROBLES LLC
 PASO ROBLES, CA 94047

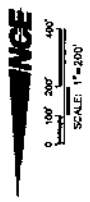
APPLICANT
 DOWD HANCOCK & MENUNDEE
 P.O. BOX 220
 PASO ROBLES, CA 94047

ENGINEER
 NORTH COAST ENGINEERING INC
 275 CREATION RD, STE B
 PASO ROBLES, CA 94047
 CHRISTOPHER RYAN
 P.E. #481
 EIP 60309

EXISTING PARCEL	USE	AREA (SQ. FT.)	AREA (AC)
EXISTING AREA (NO)	36.3	79.9	1.8
EXISTING USE	VACANT	AGRICULTURE	VACANT
EXISTING ZONING	RA-PD	RA-PD	RA-PD
PROPOSED ZONING	36.3	79.9	1.8
PROPOSED USE	RESORT	RESORT	RESORT
PROPOSED ZONING	AC	POS	AC
MOTORVEHICLE	33	33	0.8
MOTORVEHICLE	33	33	0.8
OPEN SPACE AND RECREATION	36.3	79.9	1.8
AGRICULTURE (AD)			21.4

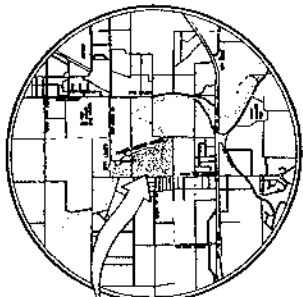
LEGEND

- GOLDEN HILL ROAD RIGHT-OF-WAY
- OFFER OF PROCEEDING
- RESORT OPEN SPACE AREA
- RECREATION AREAS
- AGRICULTURAL ZONING DESIGNATED TO THE CITY FOR OPEN SPACE USE
- RESTRICTIVE USE CATEGORY



SHEET INDEX

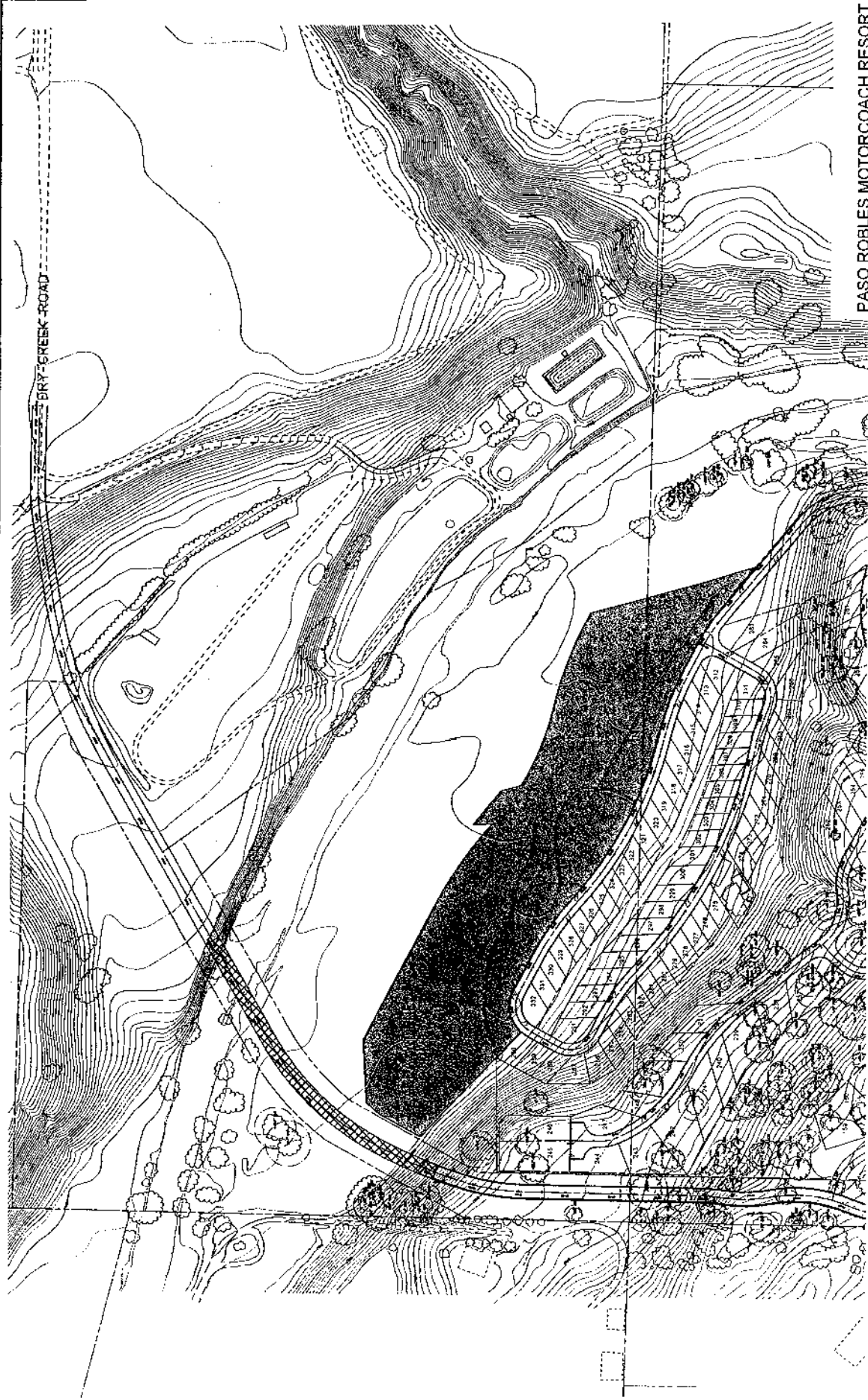
1. COVER SHEET
2. TENTATIVE LOT, BE ADJUSTMENT
3. SITE PLAN INDEX
4. PHASING PLAN
5. PRELIMINARY GRADING & DRAINAGE PLAN - NORTH
6. PRELIMINARY GRADING & DRAINAGE PLAN - MIDSECTION
7. PRELIMINARY GRADING & DRAINAGE PLAN - SOUTH
- 8.
- 9.
- 10.
- 11.
- 12.
- 13.
- 14.
- 15.
- 16.
- 17.



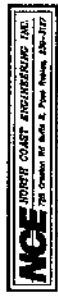
VICINITY MAP
 NO SCALE

**PASO ROBLES MOTORCOACH RESORT
 SITE LAYOUT PLAN**





PASO ROBLES MOTORCOACH RESORT
CREEK FRONT MODIFICATION



0 100' 200' 400'
GOLDEN HILL ROAD

SCALE: 1" = 200'



NORTH COAST ENGINEERING INC. copyright © 2008

Exhibit A-3
Creek Front Modification Map
GPA 09-001, RZ 09-001, PD 08-001
CUP 08-001, & PRAL 07-0293
(PR Motorcoach)

Air Quality Mitigation Measures:

APCD-1 Prior to any grading on the site, the project proponent shall ensure that a geologic evaluation is conducted to determine if Naturally Occurring Asbestos (NOA) is present within the area that will be disturbed. If NOA is not present, an exemption form must be filed with the District. If NOA is found at the site the applicant must comply with all requirements outlined in the Asbestos (Air Toxics Control Measure) ACTM.

APCD-2 The project shall be conditioned to comply with all applicable District regulations pertaining to the control of fugitive dust (PM-10) as contained in section 6.5 of the Air Quality Handbook. All site grading and demolition plans noted shall list the following regulations:

- a. Reduce the amount of the disturbed area where possible.
- b. Use of water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site. Increased watering frequency would be required whenever wind speeds exceed 15 mph. Reclaimed (nonpotable) water should be used whenever possible.
- c. All dirt stockpile areas should be sprayed daily as needed.
- d. Permanent dust control measures identified in the approved project revegetation and landscape plans should be implemented as soon as possible following completion of any soil disturbing activities.
- e. Exposed ground areas that are to be reworked at dates greater than one month after initial grading should be sown with a fast-germinating native grass seed and watered until vegetation is established.
- f. All disturbed soil areas not subject to revegetation should be stabilized using approved chemical soil binders, jute netting, or other methods approved in advance by the APCD.
- g. All roadways, driveways, sidewalks, etc. to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used.
- h. Vehicle speed for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site.
- i. All trucks hauling dirt, sand, soil, or other loose materials are to be covered or should maintain at least two feet of freeboard (minimum vertical distance between top of load and top of trailer) in accordance with CVC Section 23114.
- j. Install wheel washers where vehicles enter and exit unpaved roads onto streets, or wash off trucks and equipment leaving the site.
- k. Sweep streets at the end of each day if visible soil material is carried onto adjacent paved roads. Water sweepers with reclaimed water should be used where feasible.

APCD-3 Construction Permit Requirements:

If portable equipment, 50 horsepower or greater, are used during construction, a California statewide portable equipment registration (issued

by the California Air Resources Board) or an APCD permit. The following list is provided as a guide to equipment and operations that may have permitting requirements, but should not be viewed as exclusive. For a more detailed listing, refer to page A-5 in the Districts CEQA Handbook.

- Power screens, conveyors, diesel engines, and/or crushers;
- Portable generators and equipment with engines that are 50hp or greater;
- IC Engines;
- Concrete batch plants;
- Rock and pavement crushing;
- Tub grinders; and
- Trommel screens.

APCD-4 Develop a comprehensive Construction Activity Management Plan
designed to minimize the amount of large construction equipment operating during any given time period. The plan should be submitted to the District for review and approval prior to the start of construction. The plans should include but not be limited to the following elements:

- Schedule construction truck trips during non-peak hours to reduce peak hour emissions;
- Limit the length of the construction work-day period, if necessary; and,
- Phase construction activities, if appropriate.

APCD-5 Standard NOx Control Measures for Construction Equipment
The standard construction equipment mitigation measures for reducing nitrogen oxide (NOx) emissions are listed below and in section 6.3.1 of the Air Quality Handbook. These measures are applicable to all projects where construction equipment will be used:

- Maintain all construction equipment in proper tune according to manufacturer's specifications.
- Fuel all off-road and portable diesel powered equipment with ARB certified motor vehicle diesel fuel (non-taxed version suitable for use off-road).
- Maximize to the extent feasible, the use of on-road heavy-duty equipment and trucks that meet the ARB's 1998 or newer certification standard for on-road heavy-duty diesel engines.
- All on and off-road diesel equipment shall not be allowed to idle for more than 5 minutes. Signs shall be posted in the designated queuing areas and or job sites to remind drivers and operators of the 5 minute idling limit.

APCD 6 OPERATIONAL PHASE MITIGATION

Greenhouse Gas Impacts and Mitigation

While California successfully passed Assembly Bill 32, California's Global Solutions Act of 2006, little guidance was provided to lead agencies regarding how to address greenhouse gas (GHG) impacts in the CEQA

process. In the 2007 California legislative session, Senate Bill 97 was passed and required that the California Office of Planning and Research, by July 1, 2009, prepare and develop guidelines for the feasible mitigation of GHG emissions or the effects of GHG emissions as required by CEQA, including, but not limited to, effects associated with transportation or energy consumption. As guidelines are not currently available, the APCD suggests that projects subject to CEQA should quantify project related GHG emissions and identify feasible mitigation.

The APCD staff considered the operational impact of this proposed development by running the URBEMIS2007 computer model, a tool for estimating vehicle travel, fuel use and the resulting emissions related to this project's land uses. This indicated that operational phase impacts of the greenhouse gas known as carbon dioxide (CO₂) will be approximately 7,277 pounds per day in the summer and 6,906 pounds per day in the winter.

While statewide/global thresholds have not yet been defined for GHG impacts, SLO County APCD recommends the implementation of feasible mitigation measures that minimize project related GHG impacts.

Examples of potential measures for this development include:

- Developments within Urban Reserve Lines with walking or bicycling access to nearby commercial and transit services thus reducing automobile dependence;
- Install on-site solar power infrastructure to offset grid-based power consumption;
- Provide low-speed neighborhood electric vehicles (NEVs) and charging stations for internal use by resort patrons;
- Include pedestrian amenities that provide improved connectivity to existing amenities;
- Securing shuttle services;
- Green building techniques such as:
 - Installing outdoor electrical outlets to encourage the use of electric appliances and tools;
 - Planting of native, drought resistant landscaping;
 - Use of locally or nearby produced building materials; and,
 - Use of renewable or reclaimed building materials.

Other measures suitable for GHG as well as ozone precursor mitigation are listed below in this comment letter.

Operational Permit Requirements

Based on the information provided, we are unsure of the types of equipment that may be present at the site. Operational sources may require APCD permits. The following list is provided as a guide to equipment and operations that may have permitting requirements, but should not be viewed as exclusive. For a more detailed listing, refer to page A-5 in the District's CEQA Handbook.

- Portable generators and equipment with engines that 50 hp or greater;
- Electric generation plants or the use of standby generator; and
- Cogeneration facilities.

Traffic Impact Mitigation Measures:

- T-1. The project will be subject to traffic impact and other development impact fees in effect at the time of occupancy of the project.
- T-2. Golden Hill Road shall be constructed in general conformance to the preliminary road improvement plans approved by the City Engineer, and in accordance with the phasing plan proposed.

Biological Resources Mitigation Measures

- BR-1. Avoidance and protection of vernal pools on the property.** Vernal pools shall be avoided and protected where possible. If listed fairy shrimp species are found in vernal pools on the property, the vernal pools shall be avoided and a 50-foot setback distance shall be observed for all activities. If rare species are not found and vernal pools cannot be avoided, a vernal pool mitigation plan shall be prepared by a qualified biologist that specifies creation of vernal pool habitat in kind at a one to one ratio within open space areas on the property.
- BR-2. Interpretive signs shall be developed in cooperation with the project biologist** to inform guests at the Resort of the sensitive biological resources located on and near the property. Signs shall be placed on at least two sides of all vernal pools or vernal pool complexes that remain within the project open space areas. The signs shall provide general information about vernal pools in the Paso Robles region, including potential rare species that could be present.
- BR-3.** Tree canopies and trunks within 50 feet of proposed disturbance zones should be mapped and numbered by a certified arborist or qualified biologist and a licensed land surveyor. Data for each tree should include date, species, number of stems, diameter at breast height (dbh) of each stem, critical root zone (CRZ) diameter, canopy diameter, tree height, health, habitat notes, and nests observed.
- BR-4.** An oak tree protection plan shall be prepared and approved by the City of Paso Robles.
- BR-5.** Impacts to the oak canopy or critical root zone (CRZ) should be avoided where practicable. Impacts include pruning, any ground disturbance within the dripline or CRZ of the tree (whichever distance is greater), and trunk damage.
- BR-6.** Impacts to oak trees shall be assessed by a licensed arborist. Mitigations for impacted trees shall comply with the City of Paso Robles tree ordinance.
- BR-7.** Replacement oaks for removed trees must be equivalent to 25% of the diameter of the removed tree(s). For example, the replacement requirement for removal of two trees of 15 inches dbh (30 total diameter inches), would be 7.5 inches (30" removed x 0.25 replacement factor). This requirement could be satisfied by planting five 1.5 inch trees, or three 2.5 inch trees, or any other combination totaling 7.5 inches. A minimum of two 24 inch box, 1.5 inch trees shall be required for each oak tree removed.
- BR-8.** Replacement trees should be seasonally maintained (browse protection, weed reduction and irrigation, as needed) and monitored annually for at least 7 years. Replacement trees shall be of local origin, and of the same species as was impacted or removed.
- BR-9. Within one week of ground disturbance or tree removal/trimming activities,** if work occurs between March 15 and August 15, nesting bird surveys shall be conducted. To avoid impacts to nesting birds, grading and construction activities that affect trees

and grasslands shall not be conducted during the breeding season from March 15 to August 15. If construction activities must be conducted during this period, nesting bird surveys shall take place within one week of habitat disturbance. If surveys do not locate nesting birds, construction activities may be conducted. If nesting birds are located, no construction activities shall occur within 100 feet of nests until chicks are fledged. Construction activities shall observe a 300-foot buffer for occupied raptor nests. A 500-foot buffer shall be observed from occupied nests of all special status species (refer to BR-12 and BR-13). A pre-construction survey report shall be submitted to the lead agency immediately upon completion of the survey. The report shall detail appropriate fencing or flagging of the buffer zone and make recommendations on additional monitoring requirements.

- BR-10.** To prevent disturbance to nesting eagles, if construction is planned between January 30th and August 15th, a pre-construction survey should be conducted to determine if eagles are present. If eagles are not present after March 15th, work could commence. If eagles are present on the nest, work within 500 feet of the occupied nest should be delayed until after either adult eagles have left the nest, or eagle chicks have fledged and are no longer dependant on the nest as determined by a qualified biologist. At the commencement of work, a qualified biologist should monitor the eagles. If commencement of construction disturbs the eagles, the qualified monitor would be authorized to stop construction activity within range of the nest that causes disturbance to the eagles. Work within that area could commence once the eagle chicks have fledged and are no longer dependant on the nest.
- BR-11.** **If the project design cannot avoid shining navarretia on the property**, a mitigation and monitoring plan shall be developed by the project biologist to replace lost navarretia habitat at a 1:1 ratio on-site. The mitigation plan will provide details on appropriate mitigation sites, seed collection and distribution methods, and maintenance and monitoring requirements.
- BR-12.** **Interpretive signs shall be developed in cooperation with the project biologist** to inform guests at the Resort of the sensitive biological resources including the Golden Eagle nest located on and near the property. Signage shall be placed on all sides of the rare plant occurrence, and shall have specific information about the plant and its ecology, including photographs.
- BR-13.** **All occupied Golden Eagle nests shall be mapped using GPS or survey equipment.** The mapped locations shall be placed on a copy of the grading plans with a 500-foot buffer indicated. Work shall not be allowed within the 500 foot buffer while the nest is in use by eagles. The buffer zone shall be delineated on the ground with orange construction fencing where it overlaps work areas.
- BR-14.** **Occupied nests of special status bird species that are within 500 feet of project work areas** shall be monitored bi-monthly through the nesting season to document nest success and check for project compliance with buffer zones. Once nests are deemed inactive and/or chicks have fledged and are no longer dependant on the nest, work can commence.
- BR-15.** **Interpretive signs shall be developed in cooperation with the project biologist** to inform guests at the Resort of the sensitive biological resources located on and near the property. If the golden eagle nest continues to be occupied seasonally at the time the Resort opens to the public, signs shall be placed on the hilltop to exclude entry within approximately 500 feet of the eagle nest.

BR-16. Prior to removal of any trees over 20 inches dbh, a survey shall be conducted by a qualified biologist to determine if any of the trees proposed for removal or trimming harbor sensitive bat species or maternal bat colonies. Maternal bat colonies may not be disturbed.

BR-17. The following supplemental measures for kit fox protection are from the December 17, 2008 letter from Dan Meade of Althouse and Meade, Inc. The following measures when employed on the site, would reduce potential impacts to the San Joaquin Kit Fox. The consideration of these additional measures and the substantial widening of the proposed open space corridor, adjustment to the offsite mitigation requirements may be appropriate. The reduction of the mitigation ratio for kit fox payments from four to one to three to one can be made with review and approval by the Department of Fish and Game.

1. Kit fox friendly fencing shall be used for into all fences on the property, including the masonry screen wall, if project Biologist see as necessary. For chain link, wildlife, no-climb, or other wire fences with openings, at ground level less than eight inches square, kit fox passages shall be made in the fences every 100 yards. Passsages shall be created by cutting wire and placing spreader bars to form a smooth 8-inch wide by 12-inch high, or as specified by the Endangered Species Recovery Program. In solid walls, an 8-inch diameter concrete pipe shall be placed at ground level in the wall every 100 yards.
2. Four SJKF escape dens and a chambered den shall be constructed as per guidelines provided in the Endangered Species Recovery Program. The precise location of each den shall be designated in the field by a qualified kit fox biologist.
3. BR-31. All pets on the property shall be kept on a leash at all times. Owners shall be required to clean up after their pets. Resort maintenance personnel shall conduct daily clean up on the property to remove pet waste.
4. BR 32. Lighting shall be shielded to prevent direct lighting of the riparian corridor. All lighting shall be directed down and shall be low intensity.
5. BR 33. Use of poisons including rodenticides on the property should be restricted. This is necessary to prevent primary or secondary poisoning of kit foxes and the depletion of prey populations on which they depend. All uses of such compounds should observe labels and other restrictions, mandated by the U.S. Agriculture, and other State and Federal legislation, as well as additional project-related restrictions deemed necessary by the Service. If rodent poison must be utilized, zinc phosphate should b used because of proven lower risk to kit fox. (U.S. Fish and Wildlife Service, 1999).
6. Quiet hours shall be observed after 10 pm every night to reduce disturbance. Generators will not be permitted to run at the resort at any time.
7. Speed limits. To avoid accidental injury to animals on the property a speed limit of 10 miles per hour shall be enforced on the property for all vehicles. Speed limits shall be posted at the entrance gate and throughout roadways on the property.
8. To enhance habitat for use by kit fox vegetation management shall be conducted on neighborhood properties, including the City sewer facility on the north bank of the Heur Heuro Creek adjacent to the Paso Robles Motorcoach Resort

property. Work shall consist of removal of overgrown vegetation and removal of barrier fence when appropriate.

9. Neighborhood fencing improvements shall be conducted where fencing is a barrier to kit fox movement on properties adjacent to the Paso Robles Motorcoach property. Improvements will consist of either replacement of fences with kit fox friendly fencing, or creation of kit fox passages in existing fences every 100 yards where feasible.

BR-18. Prior to issuance of grading and/or construction permits, the applicant shall submit evidence to the County of San Luis Obispo, Department of Planning and Building, Environmental and Resource Management Division (County) (see contact information below) that states that one or a combination of the following three San Joaquin kit fox mitigation measures has been implemented:

- a. Provide for the protection in perpetuity, through acquisition of fee or a conservation easement of **219.3 (73.1 disturbed area x3)** acres of suitable habitat in the kit fox corridor area (e.g. within the San Luis Obispo County kit fox habitat area, northwest of Highway 58), either on-site or off-site, and provide for a non-wasting endowment to provide for management and monitoring of the property in perpetuity. Lands to be conserved shall be subject to the review and approval of the California Department of Fish and Game (Department) and the County.

This mitigation alternative (a.) requires that all aspects of this program must be in place before County permit issuance or initiation of any ground disturbing activities.

- b. Deposit funds into an approved in-lieu fee program, which would provide for the protection in perpetuity of suitable habitat in the kit fox corridor area within San Luis Obispo County, and provide for a non-wasting endowment for management and monitoring of the property in perpetuity.

Mitigation alternative (b) above, can be completed by providing funds to The Nature Conservancy (TNC) pursuant to the Voluntary Fee-Based Compensatory Mitigation Program (Program). The Program was established in agreement between the Department and TNC to preserve San Joaquin kit fox habitat, and to provide a voluntary mitigation alternative to project proponents who must mitigate the impacts of projects in accordance with the California Environmental Quality Act (CEQA). The fee, payable to "The Nature Conservancy", would total **\$548,250**. This fee is calculated based on the current cost-per-unit of \$2500 per acre of mitigation, which is scheduled to be adjusted to address the increasing cost of property in San Luis Obispo County; your actual cost may increase depending on the timing of payment. This fee must be paid after the Department provides written notification about your mitigation options but prior to County permit issuance and initiation of any ground disturbing activities.

- c. Purchase **219.3** credits in a Department-approved conservation bank, which would provide for the protection in perpetuity of suitable habitat within the kit fox corridor area and provide for a non-wasting endowment for management and monitoring of the property in perpetuity.

Mitigation alternative (c) above, can be completed by purchasing credits from the Palo Prieto Conservation Bank (see contact information below). The Palo Prieto Conservation Bank was established to preserve San Joaquin kit fox habitat, and to provide a voluntary mitigation alternative to project proponents who must mitigate

the impacts of projects in accordance with the California Environmental Quality Act (CEQA). The cost for purchasing credits is payable to the owners of The Palo Prieto Conservation Bank, and would total **\$548,250**. This fee is calculated based on the current cost-per-credit of \$2500 per acre of mitigation. The fee is established by the conservation bank owner and may change at any time. Your actual cost may increase depending on the timing of payment. Purchase of credits must be completed prior to County permit issuance and initiation of any ground disturbing activities.

BR-19. Prior to issuance of grading and/or construction permits, the applicant shall provide evidence that they have retained a qualified biologist acceptable to the City. The retained biologist shall perform the following monitoring activities:

- i. **Prior to issuance of grading and/or construction permits and within 30 days prior to initiation of site disturbance and/or construction**, the biologist shall conduct a pre-activity (i.e. pre-construction) survey for known or potential kit fox dens and submit a letter to the City reporting the date the survey was conducted, the survey protocol, survey results, and what measures were necessary (and completed), as applicable, to address any kit fox activity within the project limits.
- ii. **The qualified biologist shall conduct weekly site visits during site-disturbance activities** (i.e. grading, diking, excavation, stock piling of dirt or gravel, etc.) that proceed longer than 14 days, for the purpose of monitoring compliance with required Mitigation Measures BR-19 through BR-26. Site disturbance activities lasting up to 14 days do not require weekly monitoring by the biologist unless observations of kit fox or their dens are made on-site or the qualified biologist recommends monitoring for some other reason (see BR-19iii). When weekly monitoring is required, the biologist shall submit weekly monitoring reports to the City.
- iii. **Prior to or during project activities**, if any observations are made of San Joaquin Kit fox, or any known or potential San Joaquin kit fox dens are discovered within the project limits, the qualified biologist shall re-assess the probability of incidental take (e.g. harm or death) to kit fox. At the time a den is discovered, the qualified biologist shall contact USFWS and the CDFG for guidance on possible additional kit fox protection measures to implement and whether or not a Federal and/or State incidental take permit is needed. If a potential den is encountered during construction, work shall stop until such time the USFWS determines it is appropriate to resume work.

If incidental take of kit fox during project activities is possible, **before project activities commence**, the applicant must consult with the USFWS. The results of this consultation may require the applicant to obtain a Federal and/or State permit for incidental take during project activities. The applicant should be aware that the presence of kit foxes or known or potential kit fox dens at the project site could result in further delays of project activities.

iv. **In addition**, the qualified biologist shall implement the following measures:

1. **Within 30 days prior to initiation of site disturbance and/or construction**, fenced exclusion zones shall be established around all known and potential kit fox dens. Exclusion zone fencing shall consist of either large flagged stakes connected by rope or cord, or survey laths or wooden stakes prominently flagged with survey ribbon. Each

exclusion zone shall be roughly circular in configuration with a radius of the following distance measured outward from the den or burrow entrances:

- Potential kit fox den: 50 feet
 - Known or active kit fox den: 100 feet
 - Kit fox pupping den: 150 feet
2. All foot and vehicle traffic, as well as all construction activities, including storage of supplies and equipment, shall remain outside of exclusion zones. Exclusion zones shall be maintained until all project-related disturbances have been terminated, and then shall be removed.
 3. If kit foxes or known or potential kit fox dens are found on site, daily monitoring by a qualified biologist shall be required during ground disturbing activities.

Monitoring: Required prior to issuance of a grading and/or construction permit. Compliance will be verified by the City Planning Division.

- BR-20. Prior to issuance of grading and/or construction permits,** the applicant shall clearly delineate the following as a note on the project plans: *“Speed signs of 25 mph (or lower) shall be posted for all construction traffic to minimize the probability of road mortality of the San Joaquin kit fox”*. Speed limit signs shall be installed on the project site **within 30 days prior to initiation of site disturbance and/or construction.**
- BR-21. During the site disturbance and/or construction phase,** grading and construction activities after dusk shall be prohibited unless coordinated through the City, during which additional kit fox mitigation measures may be required.
- BR-22. Prior to issuance of grading and/or construction permit and within 30 days prior to initiation of site disturbance and/or construction,** all personnel associated with the project shall attend a worker education training program, conducted by a qualified biologist, to avoid or reduce impacts on sensitive biological resources (i.e. San Joaquin kit fox). At a minimum, as the program relates to the kit fox, the training shall include the kit fox’s life history, all mitigation measures specified by the City, as well as any related biological report(s) prepared for the project. The applicant shall notify the City shortly prior to this meeting. A kit fox fact sheet shall also be developed prior to the training program, and distributed at the training program to all contractors, employers and other personnel involved with the construction of the project.
- BR-23. During the site-disturbance and/or construction phase,** to prevent entrapment of the San Joaquin kit fox, all excavations, steep-walled holes and trenches in excess of two feet in depth shall be covered at the close of each working day by plywood or similar materials, or provided with one or more escape ramps constructed of earth fill or wooden planks. Trenches shall also be inspected for entrapped kit fox each morning prior to onset of field activities and immediately prior to covering with plywood at the end of each working day. Before such holes or trenches are filled, they shall be thoroughly inspected for entrapped kit fox. Any kit fox so discovered shall be allowed to escape before field activities resume, or removed from the trench or hole by a qualified biologist and allowed to escape unimpeded.

- BR-24.** During the site-disturbance and/or construction phase, any pipes, culverts, or similar structures with a diameter of four inches or greater, stored overnight at the project site shall be thoroughly inspected for trapped San Joaquin kit foxes before the subject pipe is subsequently buried, capped, or otherwise used or moved in any way. If during the construction phase a kit fox is discovered inside a pipe, that section of pipe will not be moved. If necessary, the pipe may be moved only once to remove it from the path of activity, until the kit fox has escaped.
- BR-25.** During the site-disturbance and/or construction phase, all food-related trash items such as wrappers, cans, bottles, and food scraps shall be disposed of only in closed containers. These containers shall be regularly removed from the site. Food items may attract San Joaquin kit foxes onto the project site, consequently exposing such animals to increased risk of injury or mortality. No deliberate feeding of wildlife shall be allowed.
- BR-26.** Prior to, during and after the site-disturbance and/or construction phase, use of pesticides or herbicides shall be in compliance with all local, State and Federal regulations. This is necessary to minimize the probability of primary or secondary poisoning of endangered species utilizing adjacent habitats, and the depletion of prey upon which San Joaquin kit foxes depend.
- BR-27.** During the site-disturbance and/or construction phase, any contractor or employee that inadvertently kills or injures a San Joaquin kit fox or who finds any such animal either dead, injured, or entrapped shall be required to report the incident immediately to the applicant and City. In the event that any observations are made of injured or dead kit fox, the applicant shall immediately notify the USFWS and CDFG by telephone. In addition, formal notification shall be provided in writing within three working days of the finding of any such animal(s). Notification shall include the date, time, location and circumstances of the incident. Any threatened or endangered species found dead or injured shall be turned over immediately to CDFG for care, analysis, or disposition.
- BR-28.** Prior to final inspection, or occupancy, whichever comes first, should any long internal or perimeter fencing be proposed or installed, the applicant shall do the following to provide for kit fox passage:
- i. If a wire strand/pole design is used, the lowest strand shall be no closer to the ground than 12 inches.
 - ii. If a more solid wire mesh fence is used, 8" x 12" openings near the ground shall be provided every 100 yards
 - iii. Upon fence installation, the applicant shall notify the City to verify proper installation. Any fencing constructed after issuance of a final permit shall follow the above guidelines

Monitoring (San Joaquin Kit Fox Measures BR-18 to BR-26): Compliance will be verified by the City of Paso Robles, Planning Division in consultation with the California Department of Fish and Game. As applicable, each of these measures shall be included on construction plans.

BR-29. A pre-construction survey shall be conducted within thirty days of beginning work on the project to identify if badgers are using the site. The results of the survey shall be sent to the project manager, CDFG, and the City of Paso Robles.

If the pre-construction survey finds potential badger dens, they shall be inspected to determine whether they are occupied. The survey shall cover the entire property, and shall examine both old and new dens. If potential badger dens are too long to completely inspect from the entrance, a fiber optic scope shall be used to examine the den to the end. Inactive dens may be excavated by hand with a shovel to prevent re-use of dens during construction. If badgers are found in dens on the property between February and July, nursing young may be present. To avoid disturbance and the possibility of direct take of adults and nursing young, and to prevent badgers from becoming trapped in burrows during construction activity, no grading shall occur within 100 feet of active badger dens between February and July. Between July 1 and February 1 all potential badger dens shall be inspected to determine if badgers are present. During the winter badgers do not truly hibernate, but are inactive and asleep in their dens for several days at a time. Because they can be torpid during the winter, they are vulnerable to disturbances that may collapse their dens before they rouse and emerge. Therefore, surveys shall be conducted for badger dens throughout the year. If badger dens are found on the property during the pre-construction survey, the CDFG wildlife biologist for the area shall be contacted to review current allowable management practices.

Hazard Mitigation Measures

H-1 – Airport and Aircraft Safety: *Development of any new land use on the project site shall not create an undue public safety risk from overflight of aircraft. The eastern portion of project site is in Airport Safety Zone 3 for turning and sideline zones and the western portion is Safety Zone 4 for outer approach and departure zones. All development plan, proposed use, or subdivision on the project site is subject to the nonresidential land use densities and open space requirements as provided in Chapter 4 of the Paso Robles ALUP which are excerpted below (Table 5, ALUP, 2007).*

<i>Handley Property Airport Safety Areas</i>	<i>Maximum Land Use Density (persons/acre)</i>	<i>Maximum Single Acre Land Use Density (persons/acre)</i>	<i>Maximum Percent Oper Space (% gross area)</i>
<i>Safety Zone 3</i>	<i>60</i>	<i>120</i>	<i>25²</i>
<i>Safety Zone 4</i>	<i>40</i>	<i>120</i>	<i>20²</i>

¹ *No structures, congregations of equipment or vehicles, or public venues shall be located within 250 feet of any extended runway centerline and within 6000 feet of the corresponding runway end.*

² *When feasible, development should be planned in a manner that maintains maximum open space within 50 feet of any extended runway centerline.*

H-2 - Airspace Protection: *No object or structure may be erected, and no plant allowed to grow, to penetrate any "imaginary surface" as defined in Federal Aviation Regulations Part 77. Any proposed feature approaching these surfaces will be referred to the airport manager for review and recommendation. Building within the height limits of this specific plan will not approach the FAA imaginary surfaces.*

H-3 - Operations Interference: *No use shall be established which produces visually significant quantities of smoke.*

H-4 - Bird Attractants: *No use shall be established and no activity conducted which attracts birds to the extent of creating a significant hazard of bird strikes. Examples are outdoor storage or disposal of food or grain, or large, artificial water features. This provision is not intended to prevent enhancement or protection of existing wetlands, the mitigation of impacts to wetlands or construction of required detention basins.*

H-5 Avigation Easements: *At the time of subdivision development, avigation easements shall be recorded for each affected parcel in a form approved by the County of San Luis Obispo Airport Land Use Commission.*

H-6 Real Estate Disclosure: *All owners, potential purchasers, occupants (whether as owners or renters), and potential occupants (whether as owners or renters) shall receive full and accurate disclosure concerning the noise, safety, or overflight impacts associated with airport operations prior to entering any contractual obligation to purchase, lease, rent, or otherwise occupy any property or properties within the airport area. The format of the disclosure shall be approved by the County of San Luis Obispo Airport Land Use Commission.*

Cultural Resources Mitigation Measures

CR-1: Prior to issuance of development permits, the applicant shall retain a qualified historic archaeologist to monitor initial grubbing and grading on the site and to develop a recovery program if necessary. The monitor shall have the authority to stop work in the event potentially significant cultural resources are discovered.

CR-2: In the event archaeological resources are unearthed or discovered during any construction activities, the following standards apply:

- a. Construction activities shall cease, and the Community Development Director shall be notified so that the extent and location of discovered materials may be recorded by a qualified archaeologist, and disposition of artifacts may be accomplished in accordance with state and federal law.
- b. In the event archaeological resources are found to include human remains, or in any other case where human remains are discovered during construction, the County Coroner is to be notified in addition to the Community Development Director so that proper disposition may be accomplished.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF PASO ROBLES
APPROVING GENERAL PLAN AMENDMENT 09-001
MODIFYING THE GENERAL PLAN DESIGNATION OF
PROPERTY LOCATED AT THE NORTHERN END OF GOLDEN HILL ROAD
FROM AGRICULTURE WITH AN AIRPORT OVERLAY (AG/AP) TO
PARKS AND OPEN SPACE WITH AN AIRPORT OVERLAY (POS/AP)
APPLICANT – PASO 160, LLC – PR MOTORCOACH
(APNs 025-435-005, 006 AND 007)

WHEREAS, an application to amend the Land Use Map was filed as General Plan Amendment 09-001, to amend the General Plan Land Use Map; and

WHEREAS, the property is located northern end of Golden Hill Road, (APNs 025-435-005, -006 and -007), as shown in Exhibit A, and the applicant is the property owner Paso 160, LLC; and

WHEREAS, the current Land Use designation of the subject property is Agriculture with an Airport Overlay (AG/AP); and

WHEREAS, Lot Line Adjustment 07-093, would reconfigure the site, where Parcel 1 would be 58.8 acres, Parcel 2 would be 68.6 acres and Parcel 3 would be 21.4 acres; and

WHEREAS, General Plan Amendment 09-001 proposes the following changes:

- **Parcels 1:** retain the current AG land use designation;
- **Parcel 2:** amend the land use designation from Agriculture (AG) to Parks & Open Space (POS);
- **Parcels 3:** retain the current AG land use designation;

WHEREAS, at its meeting on February 24, 2009, the Planning Commission took the following actions:

- a. Considered the facts and analysis, as presented in the staff reports prepared for this amendment;
- b. Conducted public hearings to obtain public testimony on the parts of this amendment;
- c. Considered public testimony from all parties;
- d. Based on the information contained in the staff report and the Initial Study, the Planning

Attachment 7

Commission recommended the City Council approve the proposed amendment indicating the proposed amendment to the General Plan would be appropriate for this property.

WHEREAS, at its meeting of March 17, 2009, the City Council took the following actions:

- a. Considered the facts and analysis, as presented in the staff reports prepared for this amendment, including the recommendations of the Planning Commission;
- b. Conducted a public hearing to obtain public testimony on this amendment;
- c. Based on its independent judgment, found that there was no substantial evidence that this amendment would have significant adverse effect on the environment and approved a Mitigated Negative Declaration for this General Plan amendment in accordance with the California Environmental Quality Act;
- d. Based on its independent judgment, found that the proposed General Plan Amendment 09-001 which would amend the land use designation of this property to POS/AP would be consistent with goals and policies of the General Plan by providing additional tourist-oriented and recreational opportunities, and would be an appropriate land use designation for this property.

NOW, THEREFORE, BE IT RESOLVED, by the City Council of the City of El Paso de Robles, California, finds that the amendment to the General Plan Land Use Element Map is compatible with the surrounding land uses in the vicinity. The City Council also finds that the proposed amendment would support implementation of the 2006 Economic Strategy.

PASSED AND ADOPTED by the City Council of the City of Paso Robles this 17th day of March, 2009 by the following vote:

AYES:

NOES:

ABSTAIN:

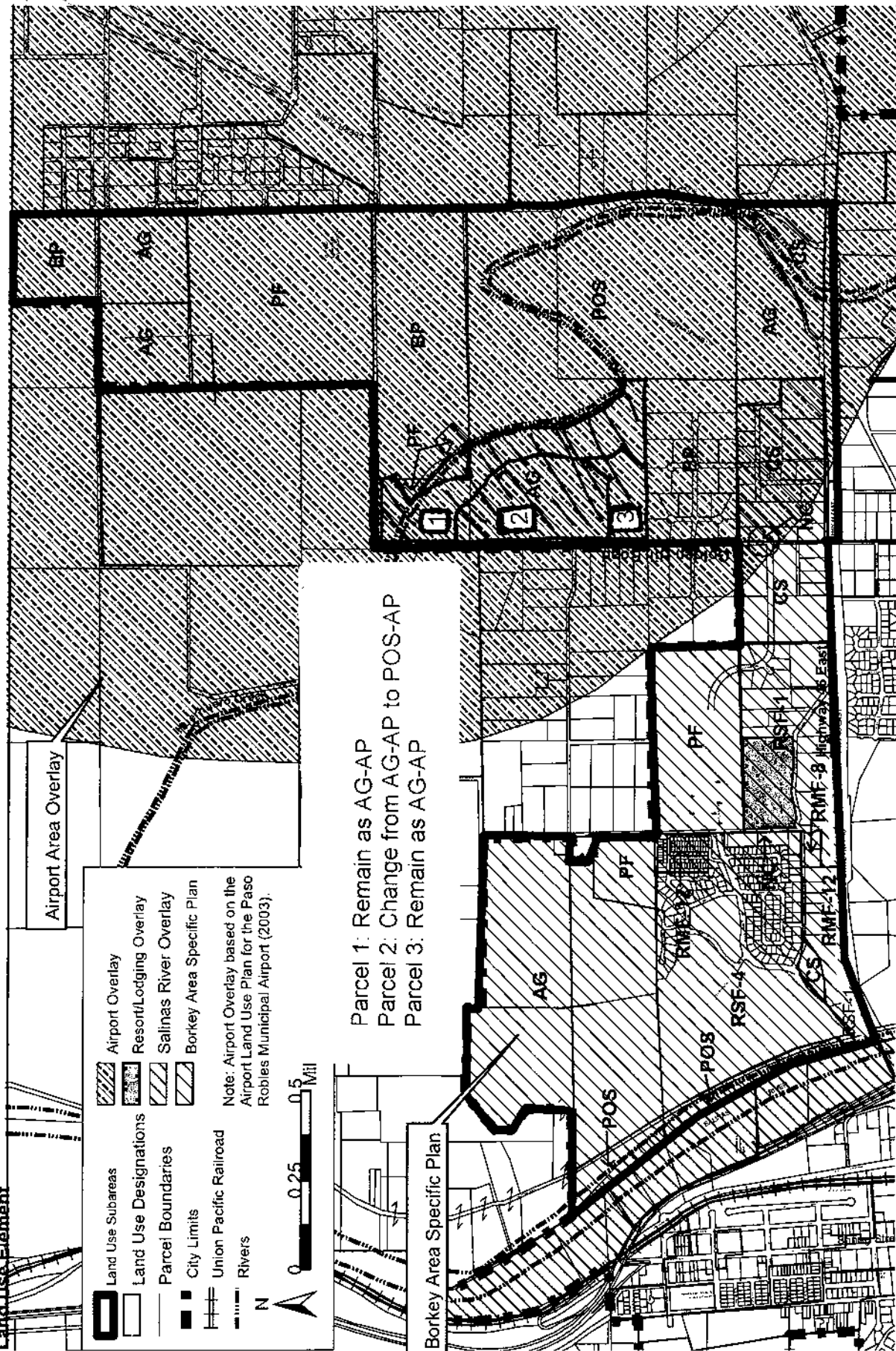
ABSENT:

Duane Picanco, Mayor

ATTEST:

Cathy David, Deputy City Clerk

Land Use Element



Parcel 1: Remain as AG-AP
 Parcel 2: Change from AG-AP to POS-AP
 Parcel 3: Remain as AG-AP

Borkey Area Specific Plan

Source: City of El Paso de Robles, 2003

General Plan Land Use Subarea 3

Figure LU-6C

ORDINANCE NO. XXX N.S.

AN ORDINANCE OF THE CITY OF EL PASO DE ROBLES
AMENDING TITLE 21, ZONING, OF THE MUNICIPAL CODE
REZONING PROPERTY TO PARKS AND OPEN SPACE WITH AN AIRPORT
OVERLAY FOR PROPERTY LOCATED AT THE NORTHERN END OF
GOLDEN HILL ROAD, (APNS 025-431-037, 038 AND 039)
APPLICANT – PASO 160, LLC- PR MOTORCOACH
ZONING MAP AMENDMENT 09-001

WHEREAS, the current Zoning of the subject property is Residential Agriculture Planned Development with an Airport Overlay (RA-PD/AP); and

WHEREAS, Lot Line Adjustment 07-093, would reconfigure the site, where Parcel 1 would be 58.8 acres, Parcel 2 would be 68.6 acres and Parcel 3 would be 21.4 acres; and

WHEREAS, Rezone 09-001 proposes the following zoning designation changes to the three parcels:

- **Parcels 1:** change the zoning designation from Residential Agriculture (RA) to Agriculture with Airport Overlay (AG/AP);
- **Parcel 2:** change the zoning designation from Residential Agriculture (RA) to Parks & Open Space with Airport Overlay (POS/AP);
- **Parcels 3:** change the zoning designation from Residential Agriculture (RA) to Agriculture with a Planned Development and Airport Overlay (AG-PD/AP);

WHEREAS, at its meeting of February 24, 2009 the Planning Commission took the following actions regarding this ordinance:

- a. Considered the facts and analysis, as presented in the staff report prepared for this project;
- b. Conducted a public hearing to obtain public testimony on the proposed ordinance;
- c. Based on the information contained in the staff report and Initial Study prepared for the proposed Zoning Map Amendment that the proposed rezoning would be appropriate for this property, and recommended the City Council approve the proposed Zoning Map Amendment; and

Attachment 8

Ordinance approving RZ 09-001
GPA 08-001, Rezone 08-001, PD 09-001
LLA 07-0293 & CUP 08-001
(PR Motorcoach)

WHEREAS, based on consideration of information received at its meeting of March 17, 2009, the City Council took the following actions regarding this Ordinance:

- a. Considered the facts and analysis, as presented in the staff report prepared for this project;
- b. Conducted a public hearing to obtain public testimony on the proposed Ordinance;
- c. Considered the recommendation from the Planning Commission's February 24, 2009 public meeting.
- d. Determined that the proposed rezoning would be appropriate for this property and would not conflict with policies in the City's General Plan Land Use Element, and an amendment to the land use designation is being considered concurrently to provide for consistency between the General Plan and property zoning.
- e. Introduced said Ordinance for the first reading; and

WHEREAS, on April 7, 2009, the City Council held second reading of said Ordinance.

NOW, THEREFORE, the City Council of the City of El Paso de Robles does hereby ordain as follows:

SECTION 1. The zoning map amendments are hereby established on the official Zoning Map as shown in Exhibit A.

SECTION 2. Publication. The City Clerk shall cause this Ordinance to be published once within fifteen (15) days after its passage in a newspaper of general circulation, printed, published and circulated in the City in accordance with Section 36933 of the Government Code.

SECTION 3. Severability. If any section, subsection, sentence, clause, or phrase of the Ordinance is, for any reason, found to be invalid or unconstitutional, such finding shall not affect the remaining portions of this Ordinance.

The City Council hereby declares that it would have passed this Ordinance by section, subsection, sentence, clause, or phrase irrespective of the fact that any one or more sections, subsections, sentences, clauses, or phrases are declared unconstitutional.

SECTION 5. Inconsistency. To the extent that the terms or provisions of this Ordinance may be inconsistent or in conflict with the terms or conditions of any prior City ordinance(s), motion, resolution, rule, or regulation governing the same subject matter thereof, such inconsistent and conflicting provisions of prior ordinances, motions, resolutions, rules, and regulations are hereby repealed.

Introduced at a regular meeting of the City Council held on March 17, 2009, and passed and adopted by the City Council of the City of El Paso de Robles on the 7th day of April, 2009, by the following vote:

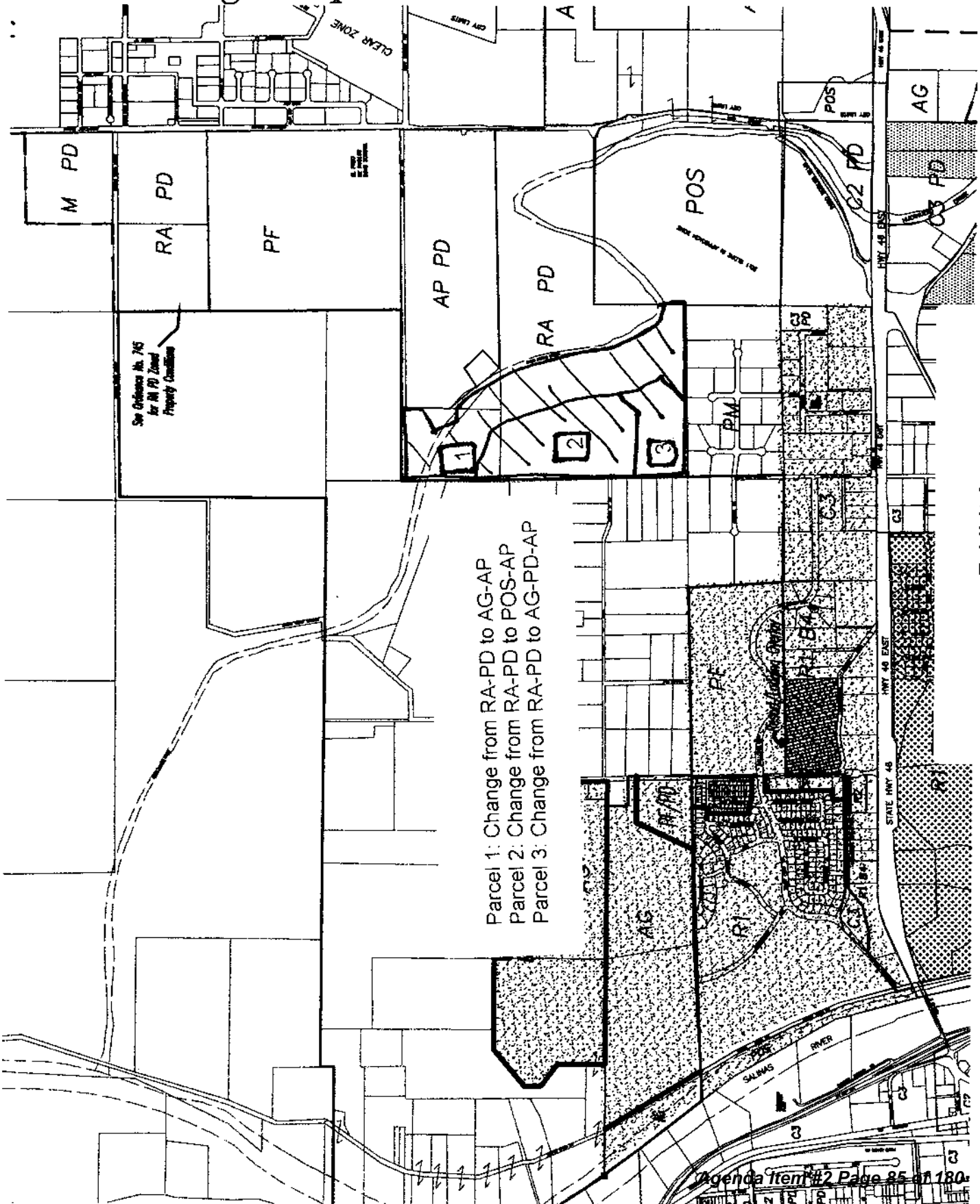
AYES:
NOES:
ABSTAIN:
ABSENT:

Duane Picanco, Mayor

ATTEST:

Cathy David, Deputy City Clerk

Zoning Map for Subarea 3



See Ordinance No. 745
for AP PD Zoned
Property Conditions

- Parcel 1: Change from RA-PD to AG-AP
- Parcel 2: Change from RA-PD to POS-AP
- Parcel 3: Change from RA-PD to AG-PD-AP

Exhibit A
Rezone 09-001
(Paso Robles Motorcoach)

RESOLUTION NO.

A RESOLUTION OF
THE CITY COUNCIL OF THE CITY OF PASO ROBLES
APPROVING PLANNED DEVELOPMENT 08-001
NORTH END OF GOLDEN HILL ROAD, APN 025-435-005, 006 and 007
APPLICANT – PASO 160, LLC –
PASO ROBLES MOTORCOACH RESORT

WHEREAS, PD 08-001 has been filed by North Coast Engineering on behalf of Paso 160, LLC to construct a 332 space RV resort on the 160 acre site located at the northern terminus of Golden Hill Road, North of Highway 46 East; and

WHEREAS, the 160 acre site is currently comprised of three parcels, the applicants are proposing to reconfigure the three parcels via Lot Line Adjustment 07-093, where Parcel 1 would be 58.8 acres, Parcel 2 would be 68.6 acres and Parcel 3 would be 21.4 acres; and

WHEREAS, PD 08-001 proposes to develop the Paso Robles Motorcoach project on Parcel 2; and

WHEREAS, pursuant to the Statutes and Guidelines of the California Environmental Quality Act (CEQA), and the City's Procedures for Implementing CEQA, an Initial Study was prepared and circulated for public review and comment; and

WHEREAS, the Planning Commission held a duly noticed public hearing on February 24, 2009 on this project to accept public testimony on PD 08-001 and associated environmental review; and

WHEREAS, the Planning Commission on a 4-1 vote (1 abstention, 1 absent) recommended that the City Council approve PD 08-001; and

WHEREAS, the City Council held a duly noticed public hearing on March 17, 2009 on this project to accept public testimony on PD 08-001; and

WHEREAS, based on the information and analysis contained in the Initial Study, the City Council makes a determination that the proposed project will not result in significant environmental impacts and it is appropriate for the City Council to adopt a Mitigated Negative Declaration; and

WHEREAS, based upon the facts and analysis presented in the staff report and the attachments thereto, the public testimony received, and subject to the Conditions of Approval listed below, the City Council makes the following findings:

1. The Paso Robles Motorcoach Resort is consistent with the adopted codes, policies, standards and plans of the City; since the project has gone through the development review process including, environmental review and the processing of a Conditional Use Permit as required by Table 21.16.200 for Recreational Parks in the Parks and Open Space zoning districts; and

2. The Paso Robles Motorcoach Resort will not be detrimental to the health, safety, morals, comfort, convenience and general welfare of the residents and or businesses in the surrounding area, or be injurious or detrimental to property and improvements in the neighborhood or to the general welfare of the City; since the project will be required to comply with the recommended conditions of approval, including any environmental mitigation measures, and comply with any building and fire codes; and
3. The Paso Robles Motorcoach Resort accommodates the aesthetic quality of the City as a whole, especially where development will be visible from the gateways to the City, scenic corridors and the public right-of-way; in this particular case, the project site is not located in a City gateway area or a scenic corridor and has minimal frontage to the public street, however, based on the project being designed to fit the subject site and based on the site plan, architecture and landscaping, the proposed development will accommodate the aesthetic quality of the City as a whole; and
4. The Paso Robles Motorcoach Resort is compatible with, and is not detrimental to, surrounding land uses and improvements, provides an appropriate visual appearance, and contributes to the mitigation of any environmental and social impacts, because the project has been designed to provide significant buffers, including setbacks, screen walls and landscaping from the residential neighborhood to the west, and additionally as a result of the site planning, building architecture and environmental mitigation, and included with this project.
5. The Paso Robles Motorcoach Resort is compatible with existing scenic and environmental resources such as hillsides, oak trees, vistas, etc. as a result of the project being designed to limit the amount of grading and oak tree impacts by developing in the flatter areas of the site, which allows for the preservation of the existing hillsides and oak trees; and
6. The Paso Robles Motorcoach Resort contributes to the orderly development of the City as a whole, since the project will utilize the existing infrastructure in Golden Hill Road, consisting of sewer water and other utilities; and
7. The Paso Robles Motorcoach Resort as conditioned would meet the intent of the General Plan and Zoning Ordinance by providing a hotel/resort type use in close proximity to golf courses and commercial recreation.
8. The Paso Robles Motorcoach Resort would be consistent with the Economic Strategy, since it would allow for the expansion and diversification of hotel-projects, by providing an end-destination full-service resort.

NOW, THEREFORE, BE IT RESOLVED, that the City Council of the City of El Paso de Robles approves PD 08-001, subject to the following conditions:

PLANNING:

1. Planned Development 08-001, along with Conditional Use Permit 08-001, GPA 09-001, Rezone 09-001 allows for the development of the 332 space Paso Robles Motorcoach Resort on Parcel 2 of Lot Line Adjustment 07-093.

2. The project shall be constructed in substantial conformance with the Conditions of Approval established by this Resolution and it shall be constructed in substantial conformance with the following Exhibits:

EXHIBIT	DESCRIPTION
A	Standard Conditions
B	Cover Sheet
C	Tentative Lot Line Adjustment
D	Site Plan
E	Phasing Plan
F1-F3	Prelim. Grading/Drainage
G	Grading Concepts
H	Creek Restoration Area
I	Underground Plan
J1-J2	Site Cross Sections
K	Details
L1-L4	Golden Hill Road
M1-M3	Registration Building Details
N1-N3	Pool House Details
O1-O3	Clubhouse Details
P1-P2	Guardhouse Details
Q1-Q2	Misc. Items
R	Outdoor Lighting
S1-S10	Landscape Plans

3. The maximum length of stay for any RV space is 30 consecutive days.
4. Prior to the issuance of a building permit, staff shall review the following items to insure substantial compliance with the above listed Exhibits:
 - a. Final site details such as landscaping, decorative paving, benches, exterior lighting and any other site planning details;
 - b. Architectural elevations, including final materials, colors and details;
 - c. Equipment such as back flow devices, transformers, a/c condensers and appropriate screening methods for both views and noise. Back flow and double check-valves shall not be visible from Golden Hill Road;
 - d. Final grading and drainage plans.
 - e. Signage
5. The project is proposed to be developed in 7 phases (see Exhibit E). In the event that the applicant wishes to change the phasing order, after verification by the City Engineer that there are no concerns, the DRC may approve the phasing change request.
6. Landscape screening shall be reviewed by Planning Staff after two years from the time of installation to insure that landscaping is in good healthy condition and being maintained in an acceptable manner.
7. Prior to the issuance of a grading permit, the applicants will need to provide an Oak Tree Replacement plan that indicates the location and timing of the planting of the required oak tree replacement trees.

8. In the event if in the future there is request to add additional impervious concrete or asphalt to the project, the request will need to be approved by both the Engineering and Planning Divisions to insure the addition of the impervious surface is consistent with the Low Impact Development plan for this project.
9. Prior to or in conjunction with the recordation of LLA 07-093, an Agriculture Conservation Easement shall be established on Parcels 1 and 3. Any proposal for the development of Parcel 3 will need to go through the development plan (PD) process approved by the Planning Commission.
10. To the extent allowable by law, Owner agrees to hold City harmless from costs and expenses, including attorney's fees, incurred by City or held to be the liability of City in connection with City's defense of its actions in any proceeding brought in any State or Federal court challenging the City's actions with respect to the project. Owner understands and acknowledges that City is under no obligation to defend any legal actions challenging the City's actions with respect to the project.

ENGINEERING:

11. Low impact development best management practices shall be incorporated into the project grading plans in accordance with City standards at the time of permitting.
12. The project will be subject to traffic impact and other development impact fees in effect at the time of occupancy of the project. Traffic mitigation will include the deposit of \$1,600,000 towards construction of a bridge over the Huer Huero proportionally applied to the incremental development of the project. When transportation impact fees are updated by council action, the final fee obligation shall be equal to that defined by the new fee structure.
- 13 a. Golden Hill Road shall be constructed in general conformance to the preliminary plans, plans approved by the City Engineer, and in accordance with the phasing plan proposed. The section of Golden Hill Road extending to the project entrance shall be constructed prior to occupancy of the first phase.
 - b. At the time of occupancy of Phase I, the applicant shall enter into an agreement to construct Golden Hill Road from Phase I to the Huer Huero bridge in accordance with the phasing plan. The agreement will provide that the City may, at the discretion of the City, accept a cash deposit, in lieu of construction, based upon a construction cost estimate approved by the City Engineer.
 - c. In consideration of an off-setting cash deposit, the City may limit the construction of Golden Hill Road, from Phase I to the secondary point of access, to standards approved by Emergency Services at the time of development of Phase 4.
 - d. In consideration of an off-setting cash deposit, the City may defer the construction of Golden Hill Road from the secondary access to the Huer Huero bridge to the time of construction of the bridge.
14. A 12-inch water main shall be extended in Golden Hill Road in accordance with plans approved by the City Engineer and in accordance with the phasing plan proposed.
15. Prior to development of phases 5, 6, 7 and 8, the applicant shall provide a floodplain study prepared by a civil engineer demonstrating compliance with the City's floodplain ordinance.

EMERGENCY SERVICES

16. Prior to the start of construction, documentation shall be submitted to Emergency Services showing that required fire flows can be provided to meet all project demands.
17. Provide fire hydrants at not greater than five hundred (500) foot intervals.
18. Provide central station monitored fire sprinkler system for all buildings greater than five thousand (5,000) square feet.
19. Provide fire department connection to the fire sprinkler system on the address side of the building for all buildings with fire sprinklers.
20. Provide exterior fire alarm enunciator panel in weather proof enclosure on the address side of the building for all buildings with fire sprinklers.
21. Provide Knox Box fire department rapid entry device on address side of the building for all buildings that will be accessed by the public and/or that have fire sprinkler systems.

PASSED AND ADOPTED THIS 17th day of March, 2009 by the following Roll Call Vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

MAYOR DUANE PICANCO

ATTEST:

CATHY DAVID, DEPUTY CITY CLERK

EXHIBIT A OF RESOLUTION

**CITY OF EL PASO DE ROBLES STANDARD DEVELOPMENT CONDITIONS
FOR PLANNED DEVELOPMENTS / CONDITIONAL USE PERMITS**

PROJECT #: PD 08-001 et al
APPROVING BODY: CITY COUNCIL
DATE OF APPROVAL: MARCH 17, 2009
APPLICANT: PR MOTORCOACH
LOCATION: NORTH END OF GOLDEN HILL ROAD

The following conditions that have been checked are standard conditions of approval for the above referenced project. The checked conditions shall be complied with in their entirety before the project can be finalized, unless otherwise specifically indicated. In addition, there may be site specific conditions of approval that apply to this project in the resolution.

COMMUNITY DEVELOPMENT DEPARTMENT - The applicant shall contact the Community Development Department, (805) 237-3970, for compliance with the following conditions:

A. GENERAL CONDITIONS:

- 1. This project approval shall expire on March 17, 2012 (See Planned Development Approval Resolution) unless a time extension request is filed with the Community Development Department prior to expiration.
- 2. The site shall be developed and maintained in accordance with the approved plans and unless specifically provided for through the Planned Development process shall not waive compliance with any sections of the Zoning Code, all other applicable City Ordinances, and applicable Specific Plans.
- 3. Prior to occupancy, all conditions of approval shall be completed to the satisfaction of the City Engineer and Community Developer Director or his designee.
- 4. Any site specific condition imposed by the Planning Commission in approving this project may be modified or eliminated, or new conditions may be added, provided that the Planning Commission shall first conduct a public hearing in the same manner as required for the approval of this project. No such modification shall be made unless the Commission finds that such modification is necessary to protect the

(Adopted by Planning Commission Resolution 94-038)

Exhibit A

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public interest and/or neighboring properties, or, in the case of deletion of an existing condition, that such action is necessary to permit reasonable operation and use for this approval.

- 5. This project is subject to the California Environmental Quality Act (CEQA) which requires the applicant submit a \$_____.00 filing fee for the Notice of Determination payable to "County of San Luis Obispo". The fee should be submitted to the Community Development Department within 24 hours of project approval which is then forwarded to the San Luis Obispo County Clerk. Please note that the project may be subject to court challenge unless the required fee is paid.
- 6. The site shall be kept in a neat manner at all times and the landscaping shall be continuously maintained in a healthy and thriving condition.
- 7. All signs shall be subject to review and approval as required by Municipal Code Section 21.19 and shall require a separate application and approval prior to installation of any sign.
- 8. All outdoor storage shall be screened from public view by landscaping and walls or fences per Section 21.21.110 of the Municipal Code.
- 9. All trash enclosures shall be constructed of decorative masonry block compatible with the main buildings. Gates shall be view obscuring and constructed of durable materials such as painted metal or chain link with plastic slatting.
- 10. All existing and/or new ground-mounted appurtenances such as air-conditioning condensers, electrical transformers, backflow devices etc., shall be screened from public view through the use of decorative walls and/or landscaping subject to approval by the Community Development Director or his designee. Details shall be included in the building plans.
- 11. All existing and/or new roof appurtenances such as air-conditioning units, grease hoods, etc. shall be screened from public view. The screening shall be architecturally integrated with the building design and constructed of compatible materials to the satisfaction of the Community Development Director or his designee. Details shall be included in the building plans.
- 12. All existing and/or new lighting shall be shielded so as to be directed downward in such a manner as to not create off-site glare or adversely impact adjacent properties. The style, location and height of the lighting fixtures shall be submitted with the building plans and shall be subject to approval by the Community Development Director or his designee.

- 13. All existing and/or new landscaping shall be installed with automatic irrigation systems.
- 14. All walls/fences and exposed retaining walls shall be constructed of decorative materials which include but are not limited to splitface block, slumpstone, stuccoed block, brick, wood, crib walls or other similar materials as determined by the Development Review Committee, but specifically excluding precision block.
- 15. The following areas shall be placed in the Landscape and Lighting District:

The developer shall install all improvements and landscape areas. City acceptance on behalf of the Landscape and Lighting District shall be subject to the approval of the Public Works Street Department (237-3864).

- 16. All parking lot landscape planters shall have a minimum outside dimension of six feet and shall be separated from parking and driving areas by a six inch high solid concrete curb.
- 17. The following areas shall be permanently maintained by the property owner, Homeowners' Association, or other means acceptable to the City:

- 18. It is the property owner's responsibility to insure that all construction of private property improvements occur on private property. It is the owner's responsibility to identify the property lines and insure compliance by the owner's agents.

B. THE FOLLOWING CONDITIONS SHALL BE COMPLETED PRIOR TO THE ISSUANCE OF BUILDING PERMITS:

- 1. Two sets of the revised Planning Commission approved plans incorporating all Conditions of Approval, standard and site specific, shall be submitted to the Community Development Department prior to the issuance of building permits.

(Adopted by Planning Commission Resolution 94-038)

- 2. Prior to the issuance of building permits, the
 - Development Review Committee shall approve the following:
 - Planning Division Staff shall approve the following:
 - a. A detailed site plan indicating the location of all structures, parking layout, outdoor storage areas, walls, fences and trash enclosures;
 - b. A detailed landscape plan;
 - c. Detailed building elevations of all structures indicating materials, colors, and architectural treatments;
 - d. Other: See site specific conditions in PD 08-001 Resolution
- 3. The applicant shall meet with the City's Crime Prevention Officer prior to the issuance of building permits for recommendations on security measures to be incorporated into the design of the structures to be constructed. The applicant is encouraged to contact the Police Department at (805) 237-6464 prior to plan check submittal.

C. THE FOLLOWING CONDITIONS SHALL BE COMPLETED PRIOR TO OCCUPANCY:

- 1. Occupancy of the facility shall not commence until such time as all Uniform Building Code and Uniform Fire Code regulations have been complied with. Prior to occupancy, plans shall be submitted to the Paso Robles Fire Department and the Building Division to show compliance. The building shall be inspected by the appropriate department prior to occupancy.
- 2. All public or private manufactured slopes located adjacent to public right-of-ways on property in excess of six (6) feet in vertical height and of 2.5:1 or greater slope shall be irrigated and landscaped for erosion control and to soften their appearance as follows: one 15-gallon tree per each 250 square feet of slope area, one 1-gallon or larger size shrub per each 100 square feet of slope area, and appropriate ground cover. Trees and shrubs shall be staggered in clusters to soften and vary the slope plane. Slope planting shall include a permanent irrigation system be installed by the developer prior to occupancy. In lieu of the above planting ratio, the applicant may submit a slope planting plan by a licensed landscape architect or contractor providing adequate landscaping, erosion control and slope retention measures; the slope planting plan is subject to approval by the Development Review Committee. Hydroseeding may be considered on lots of 20,000 square feet or greater.

(Adopted by Planning Commission Resolution 94-038)

PUBLIC WORKS DEPARTMENT - The applicant shall contact the Engineering Division, (805) 237-3860, for compliance with the following conditions:

APPLICANT: PR RV PREPARED BY: JF

REPRESENTATIVE: NCE CHECKED BY: _____

PROJECT: PD 08-001 TO PLANNING: _____

All conditions marked are applicable to the above referenced project for the phase indicated.

D. PRIOR TO ANY PLAN CHECK:

- 1. The applicant shall enter into an Engineering Plan Check and Inspection Services Agreement with the City.

E. PRIOR TO ISSUANCE OF A GRADING PERMIT:

- 1. Prior to approval of a grading plan, the developer shall apply through the City, to FEMA and receive a Letter of Map Amendment (LOMA) issued from FEMA. The developer's engineer shall provide the required supporting data to justify the application.
- 2. The proposed structures and grading shall not encroach into the 100-year floodway as specified in Municipal Code Chapter 21.14 "Flood Damage Prevention Regulations".
- 3. Any existing Oak trees located on the project site shall be protected and preserved as required in City Ordinance No. 553, Municipal Code No. 10.01 "Oak Tree Preservation", unless specifically approved to be removed. An Oak tree inventory shall be prepared listing the Oak trees, their disposition, and the proposed location of any replacement trees required. In the event an Oak tree is designated for removal, an approved Oak Tree Removal Permit must be obtained from the City, prior to removal.
- 4. A complete grading and drainage plan prepared by a registered civil engineer shall be included with the improvement plans. Drainage calculations shall be submitted, with provisions made for on-site detention/ retention if adequate disposal facilities are not available, as determined by the City Engineer.

- 5. A Preliminary Soils and/or Geology Report shall be prepared by a registered engineer for the property to determine the presence of expansive soils or other soils problems and shall make recommendations regarding grading of the proposed site.

F. PRIOR TO ANY SITE WORK:

- 1. All off-site public improvement plans shall be prepared by a registered civil engineer and shall be submitted to the City Engineer for review and approval. The improvements shall be designed and placed to the Public Works Department Standards and Specifications.
- 2. The applicant shall submit a composite utility plan signed as approved by a representative of each public utility, together with the improvement plans. The composite utility plan shall also be signed by the Water, Fire, Wastewater, and Street Division heads.
- 3. Any grading anticipated during the rainy season (October 15 to April 15) will require the approval of a Construction Zone Drainage and Erosion Control Plan to prevent damage to adjacent property. Appropriateness of areas shall be subject to City Engineer approval.
- 4. Any construction within an existing street shall require a Traffic Control Plan. The plan shall include any necessary detours, flagging, signing, or road closures requested. Said plan shall be prepared and signed by a registered civil or traffic engineer.
- 5. Landscape and irrigation plans for the public right-of-way shall be incorporated into the improvement plans and shall require a signature of approval by the Department of Public Works, Street Superintendent and the Community Development Department.
- 6. The owner shall offer to dedicate and improve the following street(s) to the standard indicated:

Golden Hill Road	Arterial	A-1
Street Name	City Standard	Standard Drawing No.
- 7. The owner shall offer to dedicate to the City the following easement(s). The location and alignment of the easement(s) shall be to the description and satisfaction of the City Engineer:
 - a. Public Utilities Easement;
 - b. Water Line Easement;

(Adopted by Planning Commission Resolution 94-038)

- c. Sewer Facilities Easement;
- d. Landscape Easement;
- e. Storm Drain Easement.

G. PRIOR TO ISSUANCE OF A BUILDING PERMIT:

- 1. A final soils report shall be submitted to the City prior to the final inspection and shall certify that all grading was inspected and approved, and that all work has been done in accordance with the plans, preliminary report, and Chapter 70 of the Uniform Building Code.
- 2. The applicants civil and soils engineer shall submit a certification that the rough grading work has been completed in substantial conformance to the approved plans and permit.
- 3. When retaining walls are shown on the grading plan, said walls shall be completed before approval of the rough grade, and prior to issuance of any building permits, unless waived by the Building Official and the City Engineer.
- 4. All property corners shall be staked for construction control, and shall be promptly replaced if destroyed.
- 5. Building permits shall not be issued until the water system has been completed and approved, and a based access road installed sufficient to support the City's fire trucks per Fire Department recommendation.
- 6. The developer shall annex to the City's Landscape and Lighting District for payment of the operating and maintenance costs of the following:
 - a. Street lights;
 - b. Parkway and open space landscaping;
 - c. Wall maintenance in conjunction with landscaping;
 - d. Graffiti abatement;
 - e. Maintenance of open space areas.
- 7. Prior to the issuance of a Building Permit for a building within Flood Insurance Rate Map (FIRM) - in zones A1-A30, AE, AO, AH, A, V1-V30, VE and V - the developer shall provide an Elevation Certificate in accordance with the National Flood Insurance Program. This form must be completed by a land surveyor, engineer or architect licensed in the State of California.
- 8. Prior to the issuance of a Building Permit for a building within Flood Insurance Rate Map (FIRM) in zones A1-A30, AE, AO, AH, A, V1-V30, VE and V, the developer

(Adopted by Planning Commission Resolution 94-038)

shall provide a Flood Proofing Certificate in accordance with the National Insurance Program. This form must be completed by a land surveyor, engineer or architect licensed in the State California.

H. PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY:

- 1. The applicant shall pay any current and outstanding fees for Engineering Plan Checking and Construction Inspection services and any outstanding annexation fees.
- 2. No buildings shall be occupied until all public improvements are completed and approved by the City Engineer, and accepted by the City Council.
- 3. All final property corners and street monuments shall be installed before acceptance of the public improvements.
- 4. All top soil removed shall be stockpiled and evenly distributed over the slopes and lots upon completion of rough grading to support hydroseeding and landscaping. All slope areas shall be protected against erosion by hydroseeding or landscaping.
- 5. The applicant shall install all street names, traffic signs and traffic striping as directed by the City Engineer.
- 6. If the adjoining existing City street is inadequate for the traffic generated by the project, or will be severely damaged by the construction, the applicant shall remove the entire roadway and replace it with a minimum full half-width street plus a 12' wide travel lane and 8' wide graded shoulder adequate to provide for two-way traffic. (A finding of "rough proportionality" has been made in the resolution for this condition).
- 7. If the development includes a phased street construction along the project boundary for future completion by the adjacent property owner, the applicant shall provide a minimum half-width street plus a 12' wide travel lane and 4' wide graded shoulder adequate for two-way traffic. (A finding of "rough proportionality" has been made in the resolution for this condition).
- 8. When the project fronts on an existing street, the applicant shall pave-out from the proposed curb to the edge of pavement if the existing pavement section is adequate, and shall feather the new paving out to the centerline for a smooth transition. If the existing pavement is inadequate, the roadway shall be replaced to centerline and the remaining pavement shall be overlaid. (A finding of "rough proportionality" has been made in the resolution for this condition).
- 9. Any utility trenching in existing streets shall be overlaid to restore a smooth riding

(Adopted by Planning Commission Resolution 94-038)

surface as required by the City Engineer. Boring and jacking rather than trenching may be required on newly constructed or heavily traveled City streets.

- 10. The applicant shall install all utilities (sewer, water, gas, electricity, cable TV and telephone) underground (as shown on the composite utility plan). Street lights shall be installed at locations as required by the City Engineer. All existing overhead utilities adjacent to or within the project shall be relocated underground except for electrical lines 77 kilovolts or greater. All utilities shall be extended to the boundaries of the project. All underground construction shall be completed and approved by the public utility companies, and the subgrade shall be scarified and compacted, before paving the streets.

- 11. Prior to paving any street the water and sewer systems shall successfully pass a pressure test. The sewer system shall also be tested by a means of a mandrel and video inspection with a copy of the video tape provided to the City. No paving shall occur until the City has reviewed and viewed the sewer video tape and has determined that the sewerline is acceptable. Any repair costs to the pipeline including trench paving restoration shall be at the developer's expense.

- 12. A blackline clear Mylar (0.4 MIL) copy and a blue line print of as-built improvement plans, signed by the engineer of record, shall be provided to the City Engineer prior to the final inspection. A reduced copy (i.e. 1" = 100') of the composite utility plan shall be provided to update the City's Atlas Map.

- 13. All construction refuse shall be separated (i.e. concrete, asphalt concrete, wood gypsum board, etc.) and removed from the project in accordance with the City's Source Reduction and Recycling Element.

PASO ROBLES FIRE DEPARTMENT - The applicant shall contact the Fire Department, (805) 237-3973, for compliance with the following conditions:

I. GENERAL CONDITIONS

- 1. Fire hydrants shall be installed at intervals as required by the Fire Chief and City Engineer. The maximum spacing for single family residential shall be 500 feet. The maximum spacing for multi-family and commercial/ residential shall be 300 feet. On-site hydrants shall be placed as required by the Fire Chief.
- 2. Building permits shall not be issued until the water system, including hydrants, has been tested and accepted and a based access road installed sufficient to support the City's fire apparatus (HS-20 truck loading). The access road shall be kept clear to a minimum of 24 feet at all times and shall be extended to each lot and shall be maintained to provide all weather driving conditions.
- 3. No buildings shall be occupied until all improvements are completed and accepted by the City for maintenance.
- 4. If the development includes phased street construction, temporary turn-arounds shall be provided for streets that exceed 150 feet in length. The temporary turn around shall meet City requirements as set forth in the Public Works Department Standards and Specifications.
- 5. All open space areas to be dedicated to the City shall be inspected by the Fire Department prior to acceptance. A report shall be submitted recommending action needed for debris, brush and weed removal and tree trimming. The developer shall clean out all debris, dead limbs and trash from areas to be recorded as open space prior to acceptance into a Benefit Maintenance District.
- 6. Any open space included in a private development shall be subject to the approval of a vegetation management plan approved by the Fire Chief.
- 7. Each tract or phase shall provide two sources of water and two points of access unless otherwise determined by the Fire Chief and Public Works Director.
- 8. Provisions shall be made to update the Fire Department Run Book.

(Adopted by Planning Commission Resolution 94-038)



SITE STATISTICS - OVERVIEW

RECORD OWNER PASO RD LLC
P.O. BOX 2922
PASO ROBLES, CA 92947

APPLICANT DOUG MCCOY & KEN WUNDE
P.O. BOX 2922
PASO ROBLES, CA 92947

ENGINEER NORTH COAST ENGINEERING, INC.
1000 S. J. STREET, SUITE 100
PASO ROBLES, CA 92947
CHRISTY GABLER
P.L.C. 03/2009

EXISTING AREA AND EXISTING ZONING	EXISTING PARCELS			PROPOSED PARCELS			TOTAL		
	AC	% PD	RA PD	AS	R/RESORT	AG	AS	RA PD	AG
EXISTING GENERAL PLAN	30	0	0	30	0	0	30	0	0
EXISTING ZONING	30	0	0	30	0	0	30	0	0
PROPOSED GENERAL PLAN	0	0	0	0	0	0	0	0	0
PROPOSED ZONING	0	0	0	0	0	0	0	0	0
AGRICULTURAL ZONING WITH AGRICULTURAL RESPECTIVE USE DESIGNATION	0	0	0	0	0	0	0	0	0
OPEN SPACE AND PUBLIC OPEN SPACE	0	0	0	0	0	0	0	0	0
TOTAL	30	0	0	30	0	0	30	0	0

LEGEND

- GOLDEN HILL ROAD RIGHT-OF-WAY
- DYPER OF REGULATION
- RESORT OPEN SPACE AREA
- RECREATION AREAS
- AGRICULTURAL ZONING DESIGNATED TO THE CITY FOR RESPECTIVE USE
- AGRICULTURAL ZONING WITH AGRICULTURAL RESPECTIVE USE DESIGNATION



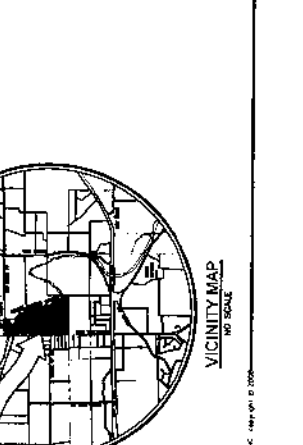
OTORCOACH RESORT LAYOUT PLAN

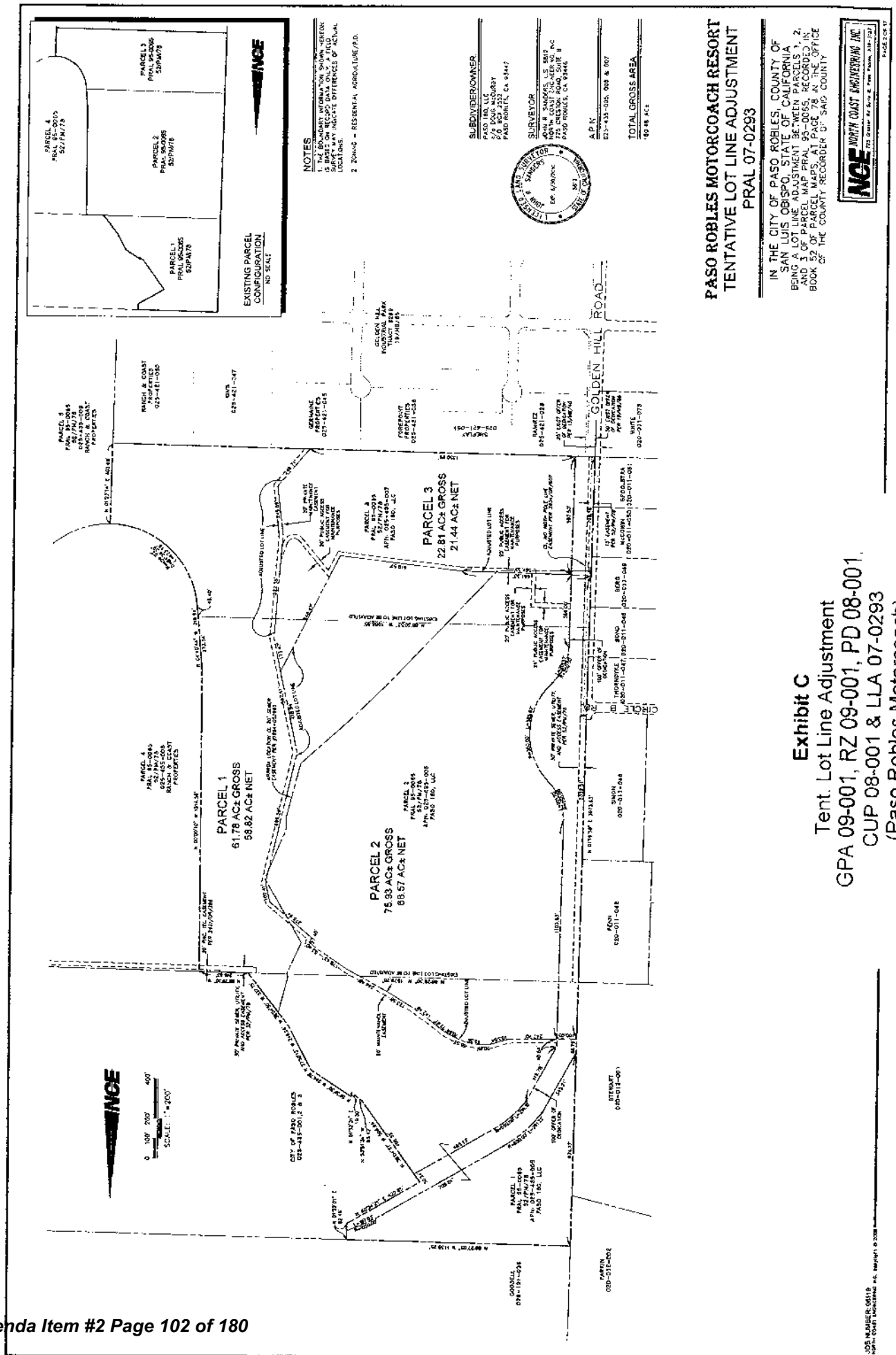
NORTH COAST ENGINEERING INC.
1000 S. J. STREET, SUITE 100
PASO ROBLES, CA 92947
PHONE: (805) 237-3337

PROJECT NO: 2009-001
SHEET NO: 1 OF 11

EXHIBIT B
Cover Sheet
GPA 09-001, RZ 09-001, PD 08-001, CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

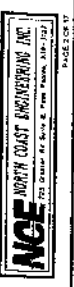
- SHEET INDEX**
- COVER SHEET
 - PHASING PLAN
 - PRELIMINARY GRADING & DRAINAGE PLAN - NORTH
 - PRELIMINARY GRADING & DRAINAGE PLAN - MIDSECTION
 - PRELIMINARY GRADING & DRAINAGE PLAN - SOUTH
 - GRADING CONCEPTS
 - UNDERGROUND PLAN
 - UNDERGROUND PLAN AREA
 - SITE CROSS SECTIONS
 - DETAILS
 - GOLDEN HILL ROAD - 1400 TO 13+00
 - GOLDEN HILL ROAD - 13+00 TO 26+00
 - GOLDEN HILL ROAD - 26+00 TO 38+00
 - GOLDEN HILL ROAD - 38+00 TO 51+00





**PASO ROBLES MOTORCOACH RESORT
TENTATIVE LOT LINE ADJUSTMENT
PRAL 07-0293**

IN THE CITY OF PASO ROBLES, COUNTY OF SAN LUIS OBISPO, STATE OF CALIFORNIA, BEING A LOT LINE ADJUSTMENT BETWEEN PARCELS 1, 2, AND 3 OF PARCEL MAP PRAL 93-0055, RECORDED IN BOOK 52 OF PARCEL MAPS, AT PAGE 78 IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.



NOTES
1. THE BOUNDARY INFORMATION SHOWN HEREON IS BASED ON RECORD DATA ONLY. FIELD LOCATIONS MAY INDICATE DIFFERENCES OF ACTUAL LOCATIONS.
2. ZONING - RESIDENTIAL AGRICULTURE (R.A.)

SUBDIVIDER/OWNER
PASO 180, LLC
2/8 BOON MCNEARY
275 REDDING ROAD, SUITE B
PASO ROBLES, CA 93447

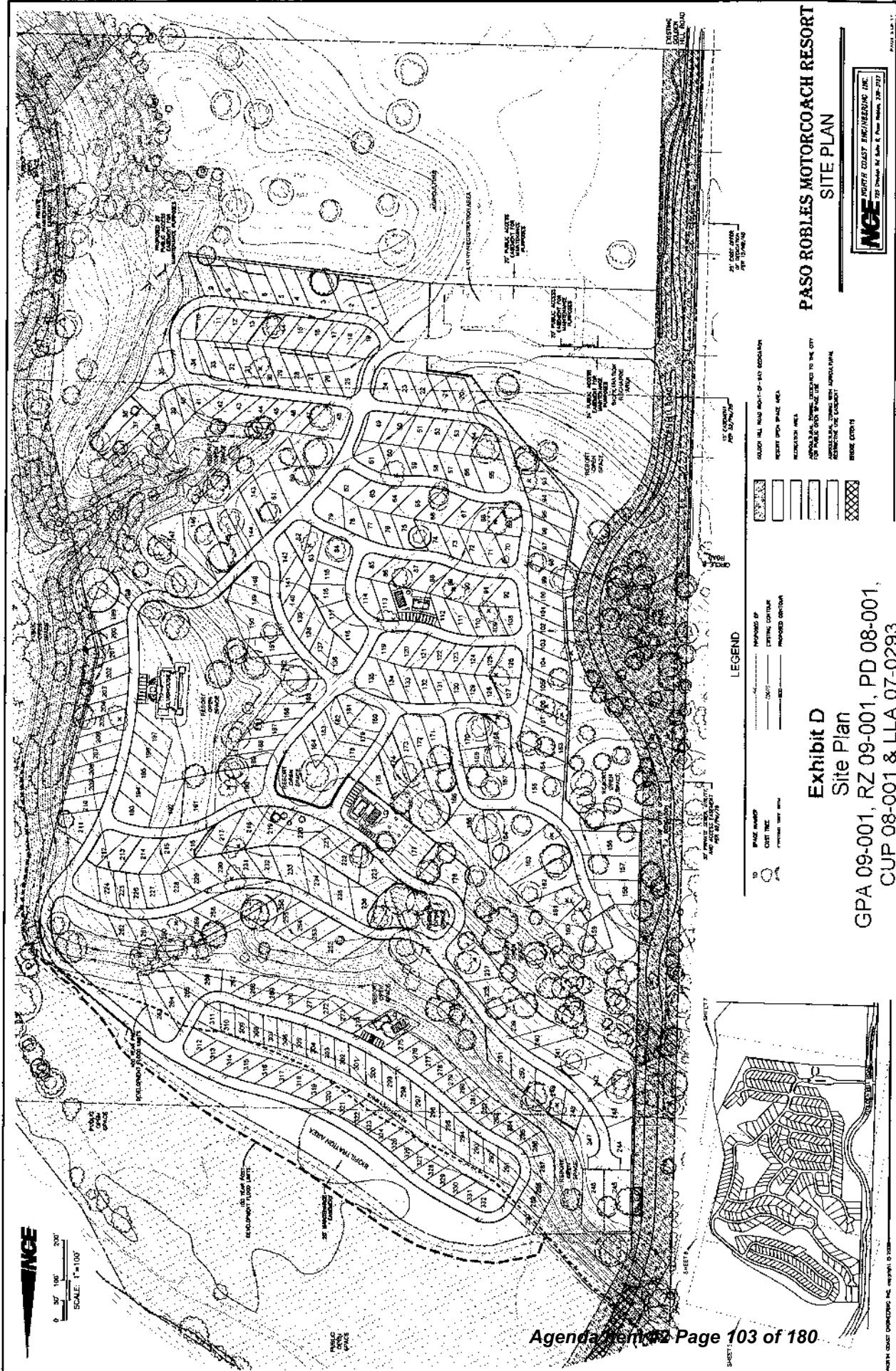
SURVEYOR
NORTH COAST ENGINEERING, INC.
275 REDDING ROAD, SUITE B
PASO ROBLES, CA 93447

A.P.N.
023-433-005, 008 & 007

TOTAL GROSS AREA
160.48 AC±



Exhibit C
Tent. Lot Line Adjustment
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)



PASO ROBLES MOTORCOACH RESORT
SITE PLAN



- LEGEND**
- PROPOSED OF CONTOUR
 - EXISTING CONTOUR
 - PROPOSED CONTOUR
 - PROPOSED SPACE AREA
 - RECREATION AREA
 - PUBLIC AREA RESERVED TO THE CITY
 - PUBLIC AREA
 - EXISTING IMPROVEMENT
 - DRIVE ELEVATION

Exhibit D
Site Plan

GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)

NCE
 SCALE: 1" = 100'

DATE: 08/20/2013 10:54 AM, PROJECT: PASO ROBLES MOTORCOACH RESORT, SHEET: 103 OF 180

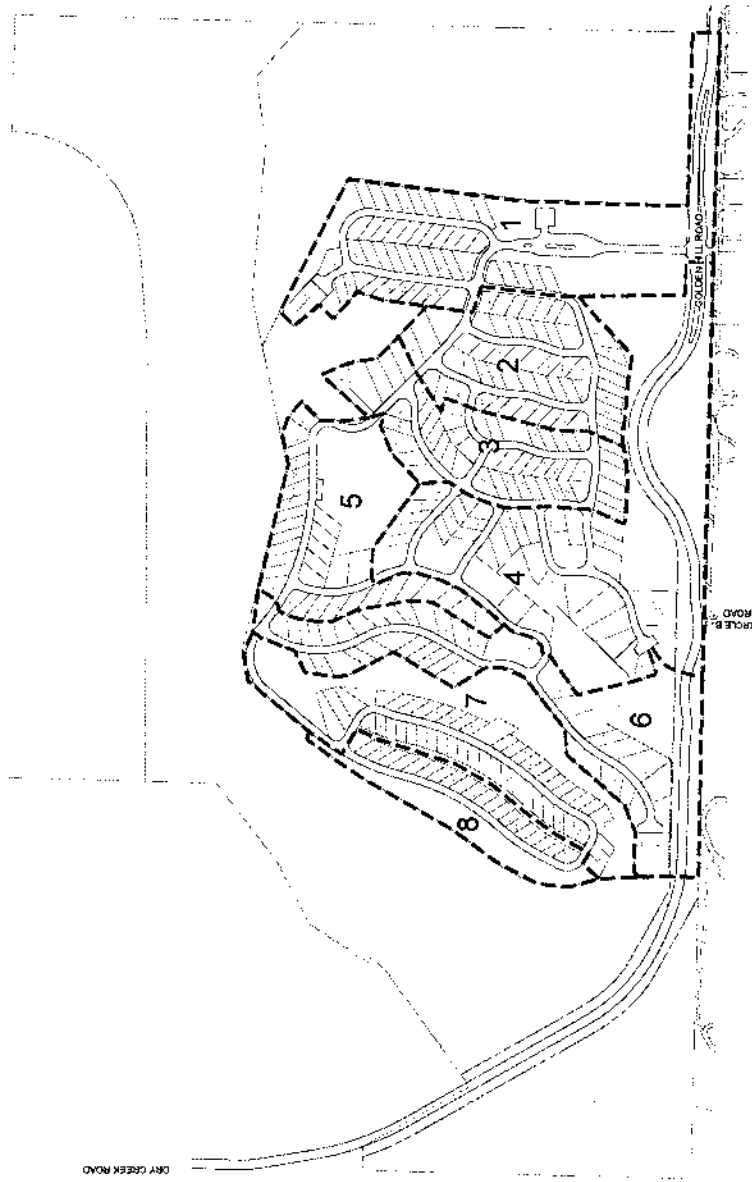
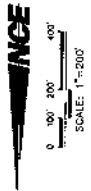
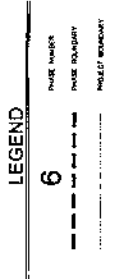
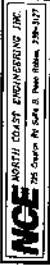


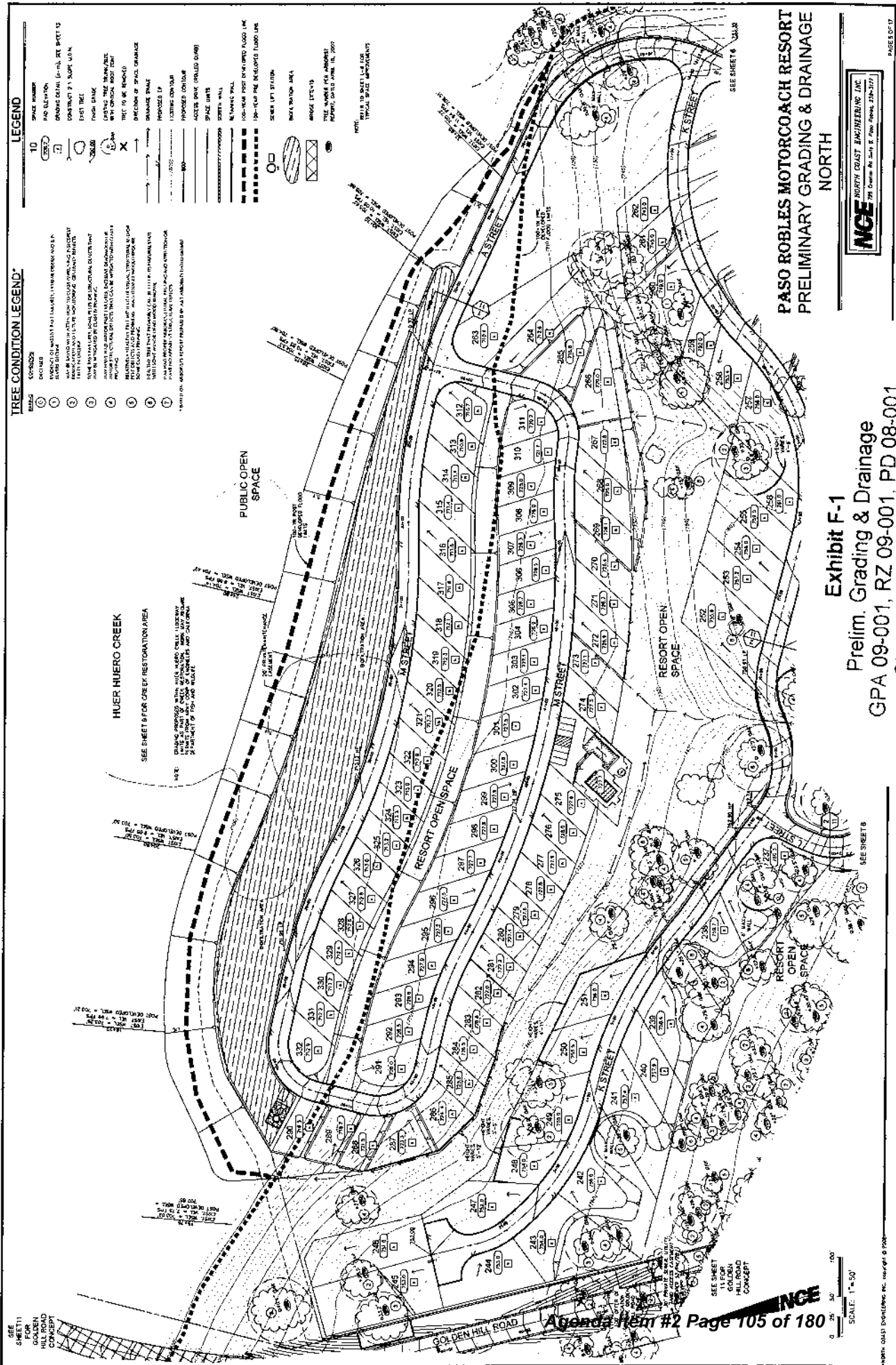
Exhibit E
Phasing Plan
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)

Phase	Space Count	Total
1	45	45
2	55	100
3	51	152
4	38	190
5	33	223
6	36	262
7	15	277
8	21	332



PASO ROBLES MOTORCOACH RESORT
PRELIMINARY PHASING PLAN





TREE CONDITION LEGEND:

10 SPACE NUMBER

11 SPACE NUMBER

12 SPACE NUMBER

13 SPACE NUMBER

14 SPACE NUMBER

15 SPACE NUMBER

16 SPACE NUMBER

17 SPACE NUMBER

18 SPACE NUMBER

19 SPACE NUMBER

20 SPACE NUMBER

21 SPACE NUMBER

22 SPACE NUMBER

23 SPACE NUMBER

24 SPACE NUMBER

25 SPACE NUMBER

26 SPACE NUMBER

27 SPACE NUMBER

28 SPACE NUMBER

29 SPACE NUMBER

30 SPACE NUMBER

31 SPACE NUMBER

32 SPACE NUMBER

33 SPACE NUMBER

34 SPACE NUMBER

35 SPACE NUMBER

36 SPACE NUMBER

37 SPACE NUMBER

38 SPACE NUMBER

39 SPACE NUMBER

40 SPACE NUMBER

41 SPACE NUMBER

42 SPACE NUMBER

43 SPACE NUMBER

44 SPACE NUMBER

45 SPACE NUMBER

46 SPACE NUMBER

47 SPACE NUMBER

48 SPACE NUMBER

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52 SPACE NUMBER

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86 SPACE NUMBER

87 SPACE NUMBER

88 SPACE NUMBER

89 SPACE NUMBER

90 SPACE NUMBER

91 SPACE NUMBER

92 SPACE NUMBER

93 SPACE NUMBER

94 SPACE NUMBER

95 SPACE NUMBER

96 SPACE NUMBER

97 SPACE NUMBER

98 SPACE NUMBER

99 SPACE NUMBER

100 SPACE NUMBER

**PASO ROBLES MOTORCOACH RESORT
PRELIMINARY GRADING & DRAINAGE**

NORTH

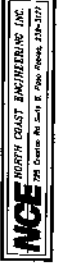


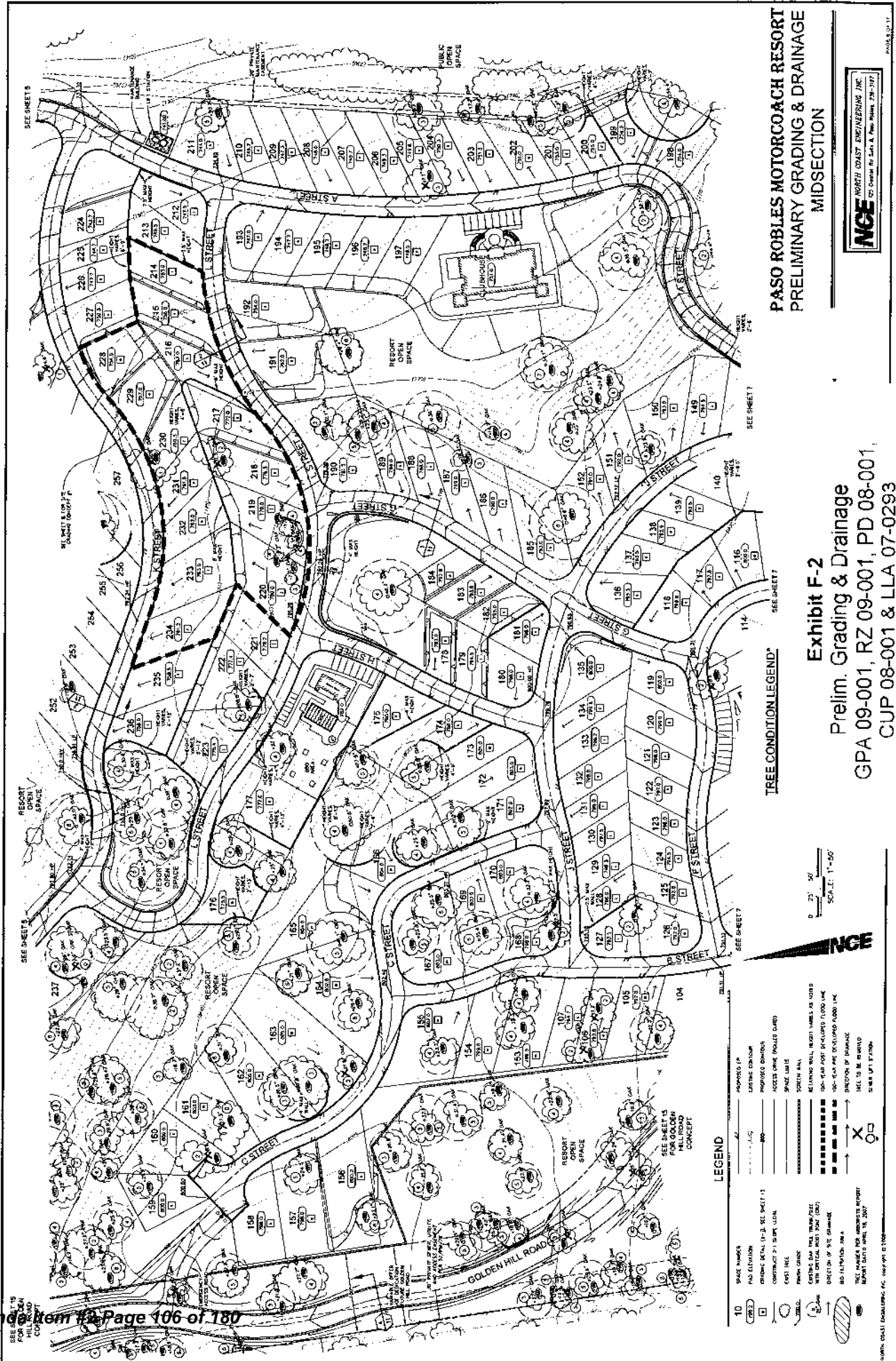
Exhibit F-1
 Prelim. Grading & Drainage
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)

SEE SHEET 11 FOR GOLDEN HILL ROAD CONCEPT

SEE SHEET 11 FOR GOLDEN HILL ROAD CONCEPT

INCE

SCALE: 1" = 50'



**PASO ROBLES MOTORCOACH RESORT
PRELIMINARY GRADING & DRAINAGE
MIDSECTION**

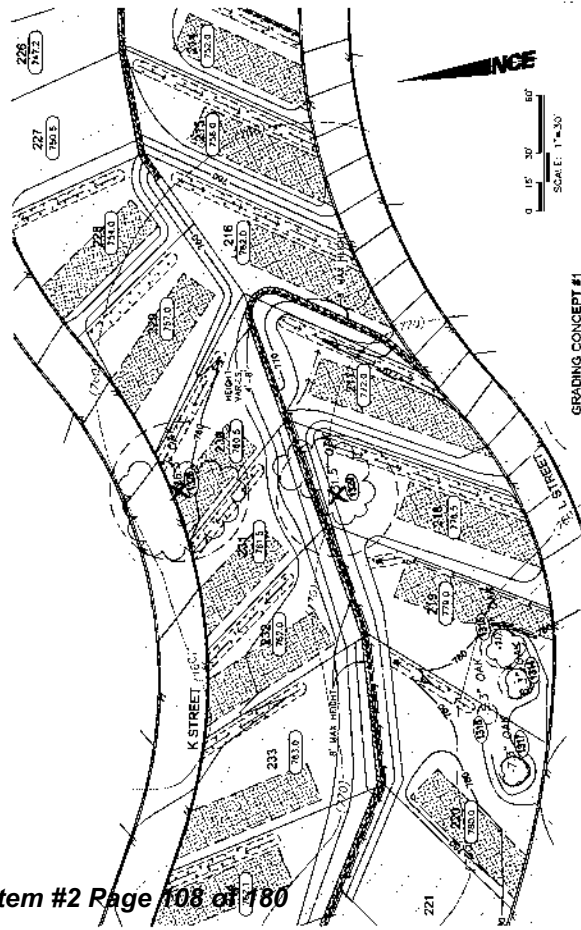
NCE
NORTH COAST ENGINEERING, INC.
225 Camino Del Sur, E. Paso Robles, 92907

**Exhibit F-2
Prelim. Grading & Drainage
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)**

Agenda Item 12 Page 106 of 180

LEGEND

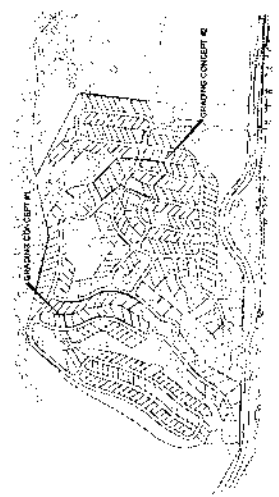
	10' SPACE MARKER		PROPOSED STRUCTURE
	EXISTING CONDITION		ACCESS DRIVE (PAVED DRIVE)
	21' STRIP, USED		SPACE LIMIT
	10'-14' STRIP PROPOSED FLOOD LINE		100-150 YEAR PROPOSED FLOOD LINE
	100-150 YEAR PROPOSED FLOOD LINE		DIRECTION OF DRAINAGE
	100-150 YEAR PROPOSED FLOOD LINE		100-150 YEAR PROPOSED FLOOD LINE
	100-150 YEAR PROPOSED FLOOD LINE		100-150 YEAR PROPOSED FLOOD LINE
	100-150 YEAR PROPOSED FLOOD LINE		100-150 YEAR PROPOSED FLOOD LINE



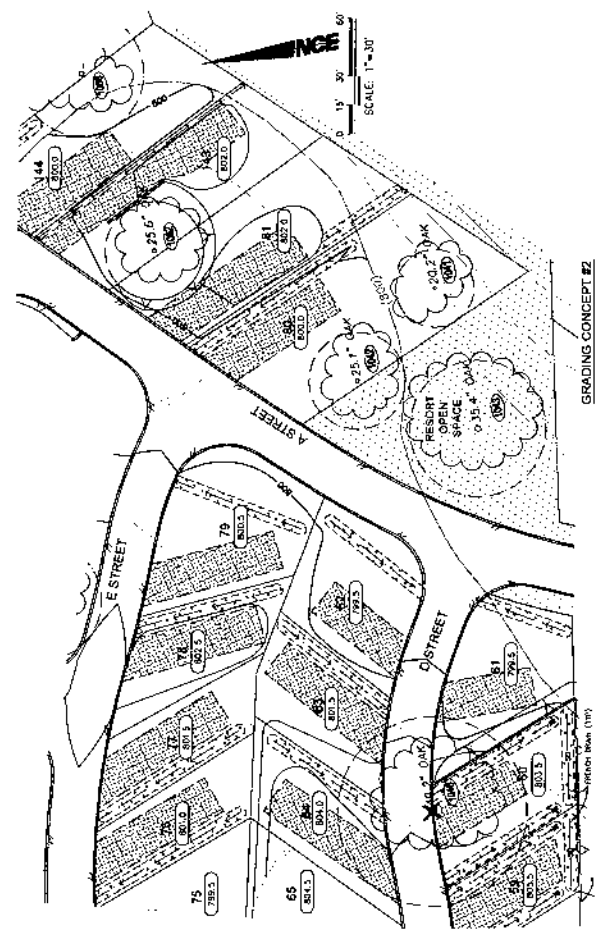
GRADING CONCEPT #1

- LEGEND**
- █ BLOCK RETAINING WALL
 - ▨ PERMEABLE WALL
 - ▨ PERMEABLE INTERLOCKING CONCRETE PAVING
 - ▨ LANDSCAPED/RO-SWALE
 - ▨ DRAINAGE SWALE
 - 100 SITE NUMBER
 - PAD ELEVATION
 - TREE NUMBER
 - EAST GRADE CONTOURS
 - FINISH GRADE CONTOURS

- NOTES**
1. THESE FINISHES REPRESENT A GRADING CONCEPT FOR EACH SAMPLE AREA OF THE PROJECT. FINAL DESIGN MAY VARY FROM GRADING PROPOSED.
 2. SHUT SAMPLE AREAS 7 AND L-8 FOR LANDSCAPE CONCEPTS FOR THESE SHUT SAMPLE AREAS.



DETAIL INDEX
1"=100'



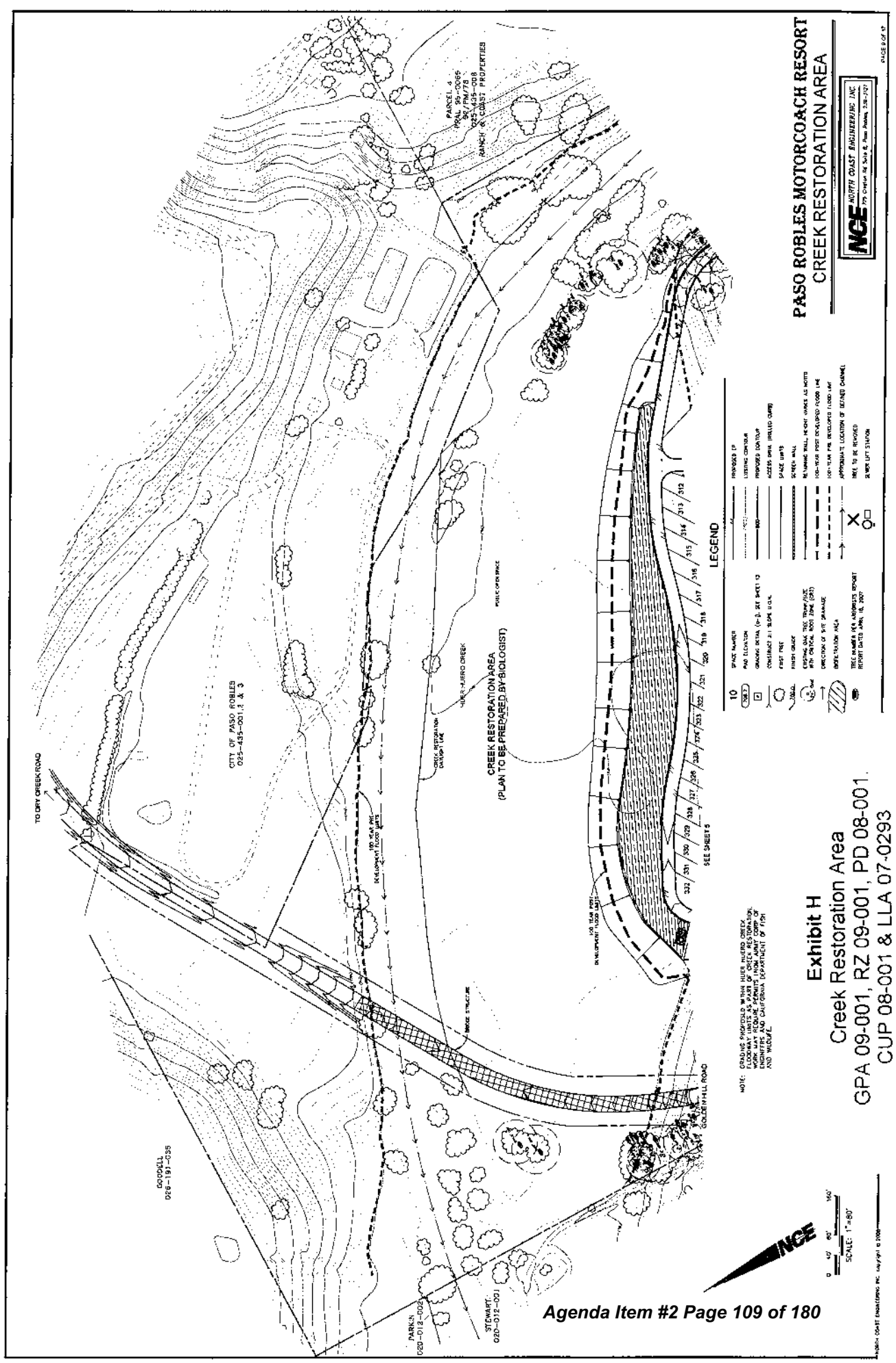
GRADING CONCEPT #2

Exhibit G
Grading Concepts
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)

PASO ROBLES MOTORCOACH RESORT
GRADING CONCEPTS

NGE
 NORTH COAST ENGINEERING, INC.
 1775 CHINA LANE, SUITE 8, PASO ROBLES, CA 93426
 (805) 231-2722

PAGE 8 OF 17

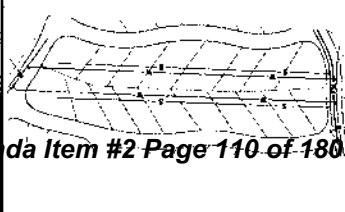


PASO ROBLES MOTORCOACH RESORT CREEK RESTORATION AREA

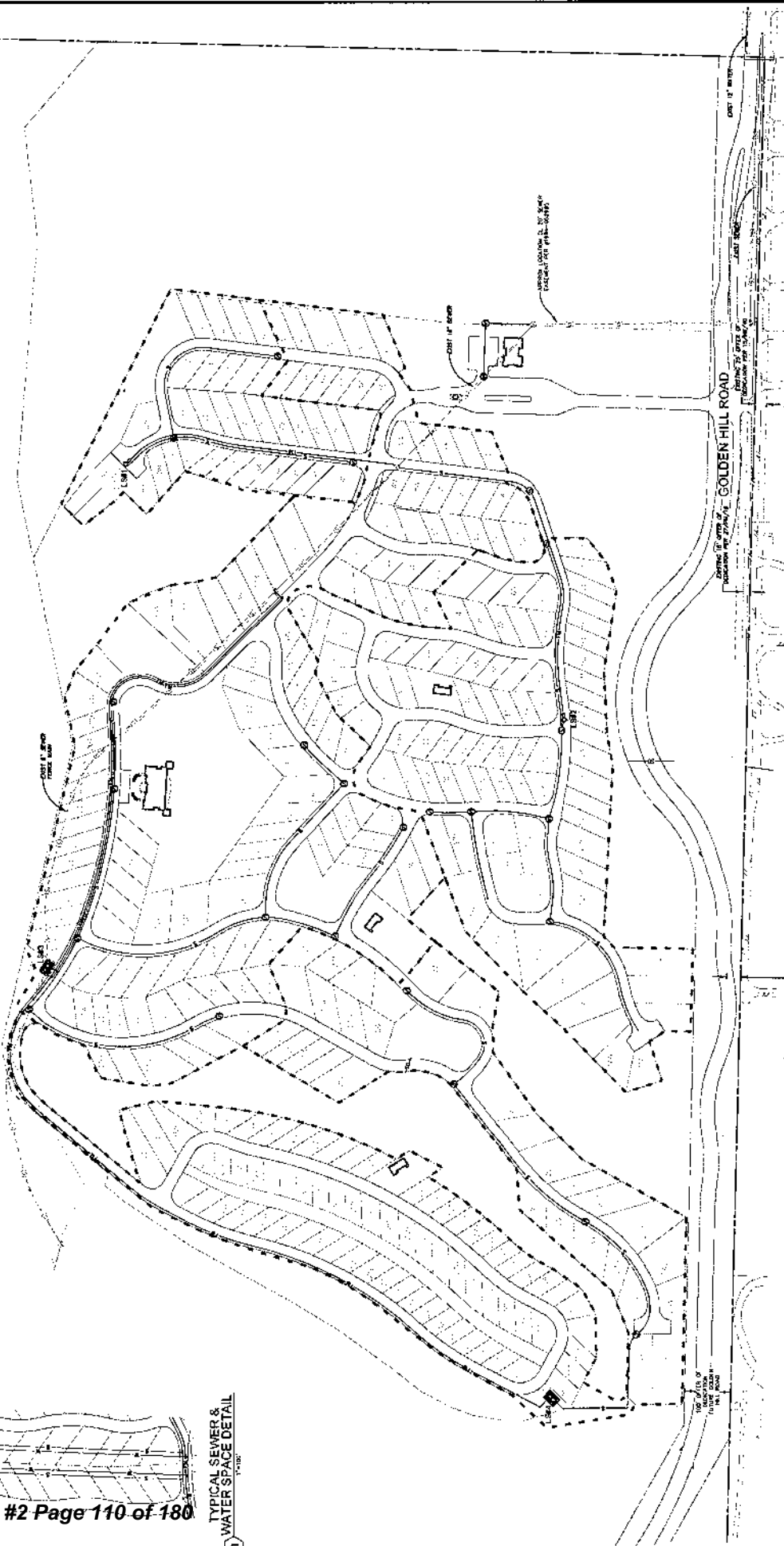
NGE NORTH COAST ENGINEERING, INC.
777 CENTRAL EX BLVD. E. STE. 100A, PASO ROBLES, CA 93426

- PROPOSED L.P.
- EXISTING CHANNEL
- PROPOSED CHANNEL
- PROPOSED DIVERSION
- ACCESS DRIVE, UNIMPAVED DRIVE
- SPACE LIMITS
- DRAINAGE WALL
- RETAINING WALL
- 100-YEAR FLOOD ELEVATION (AS NOTED)
- 100-YEAR FLOOD ELEVATION (FLOOD LINE)
- APPROXIMATE LOCATION OF BEHIND CHANNEL
- PILE TO BE REMOVED
- PILE CUT STATION

Exhibit H
Creek Restoration Area
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)



TYPICAL SEWER & WATER SPACE DETAIL

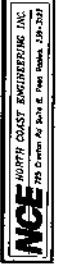


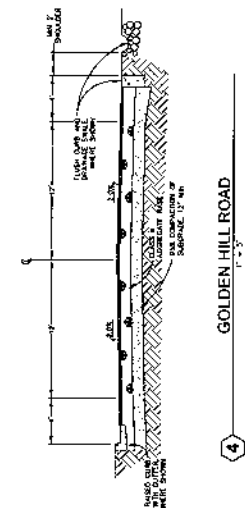
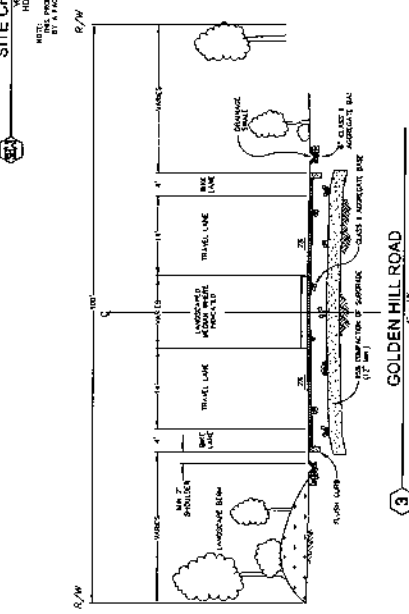
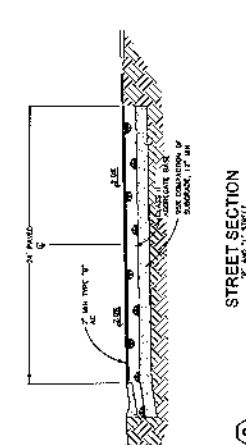
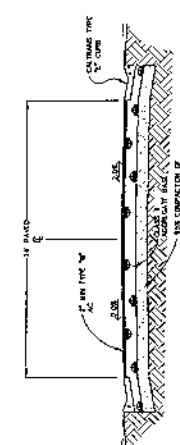
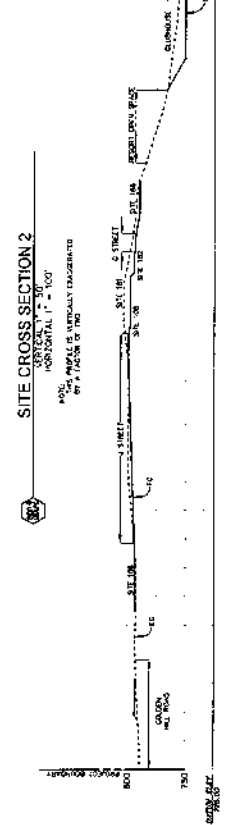
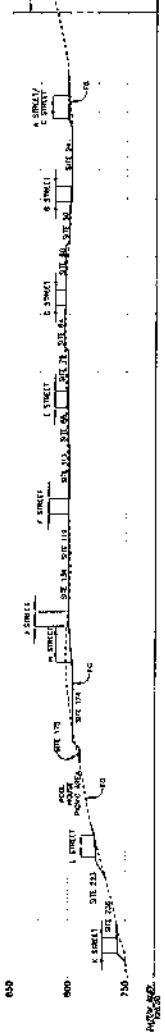
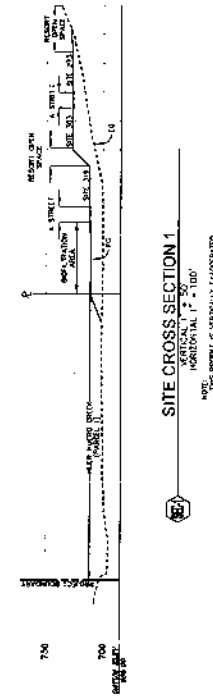
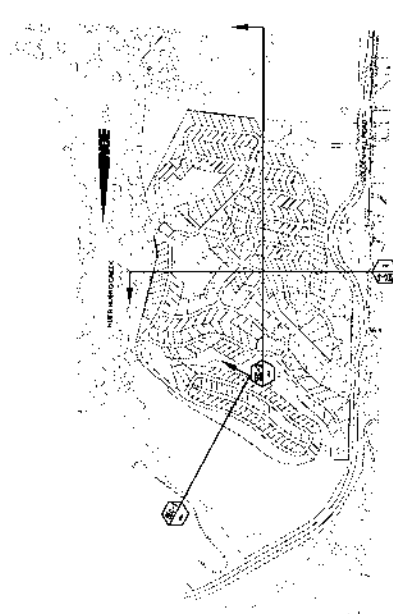
LEGEND

- 1. ALL SEWER LINES TO BE CONSIDERED AS A LOCAL SYSTEM WITH LOCAL PUMP
- 2. ONLY LOCAL PUMP LINES ARE SHOWN ON THIS PLAN. LOCAL PUMP LINES ARE NOT TO BE SHOWN ON THIS PLAN.
- 3. LOCAL PUMP SPANS 10' FROM ONE QUART TO THE NEXT.

Exhibit 1
Underground Plan
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)

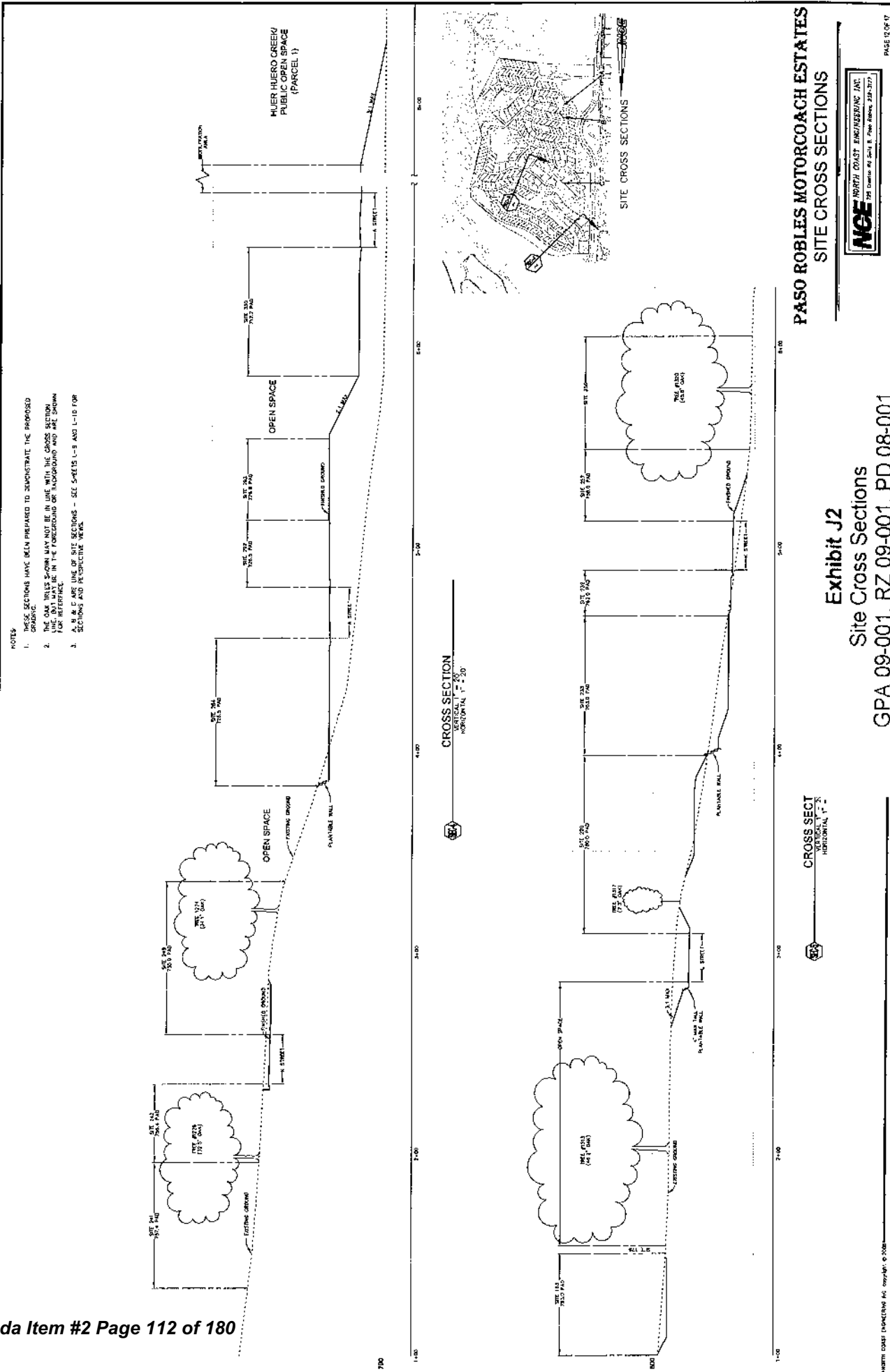
PASO ROBLES MOTORCOACH RESORT
PRELIMINARY UNDERGROUND PLAN





CH ESTATES
 IONS
 ENGINEERS
 ARCHITECTS
 10000 N. 100th St.
 Suite 200
 Overland Park, KS 66213
 PHONE: 913.241.1100
 FAX: 913.241.1101
 WWW: CHESTATES.COM

Exhibit J1
 Site Cross Sections
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)



<p>TYPE "a" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>	<p>TYPE "b" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>	<p>TYPE "c" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>	<p>TYPE "d" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>
<p>TYPE "e" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>	<p>TYPE "f" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>	<p>TYPE "g" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>	<p>TYPE "h" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>
<p>TYPE "i" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>			
<p>TYPE "j" NATURAL SPACE 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>			
<p>TYPE "k" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>			
<p>TYPE "l" SPACE GRADING 1" = 20' * RETAINING WALLS MAY BE SUBSTITUTED FOR SLOPE</p>			

NOTE:
1. THE SLOPE OF EACH PROPOSED SITE IS TO MATCH EXISTING WITHIN 1% TOLERANCE. THIS DOES NOT MEAN THAT THE SLOPE OF THE SITE IS TO BE FLAT. THE SLOPE OF THE SITE IS TO BE FLAT WITHIN 1% TOLERANCE.
2. ALL SLOPES SHALL BE 2:1 UNLESS OTHERWISE NOTED.
3. ALL SLOPES SHALL BE 2:1 UNLESS OTHERWISE NOTED.
4. ALL SLOPES SHALL BE 2:1 UNLESS OTHERWISE NOTED.
5. ALL SLOPES SHALL BE 2:1 UNLESS OTHERWISE NOTED.

PASO ROBLES MOTORCOACH ESTATES
DETAILS

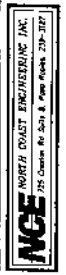
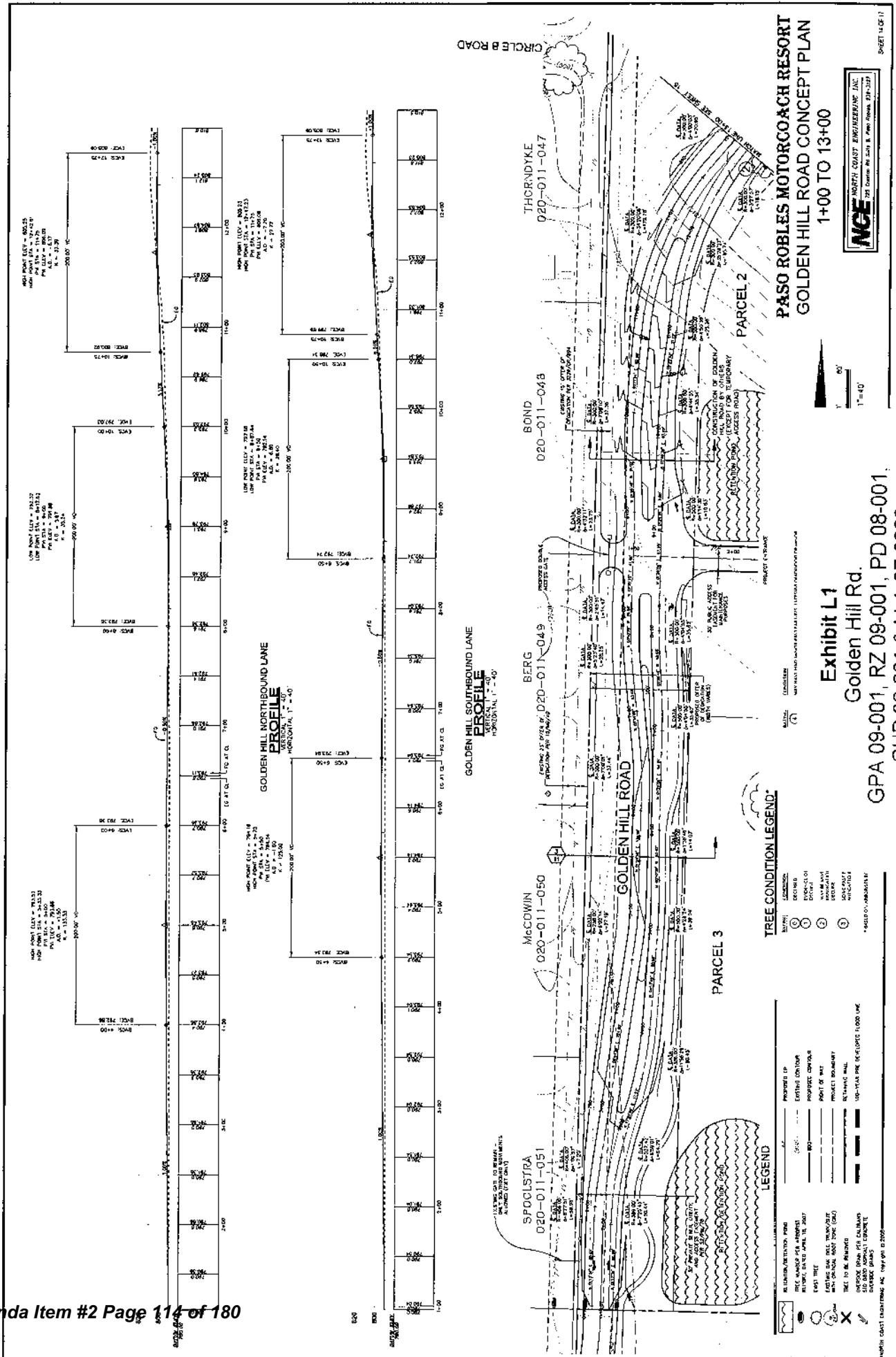


Exhibit K
Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)



NGE NORTH COAST ENGINEERING INC.
 122 Ocean Ave. Suite 8, Santa Monica, CA 90401
 (310) 316-1111

PASO ROBLES MOTORCOACH RESORT
GOLDEN HILL ROAD CONCEPT PLAN
 1+00 TO 13+00

Exhibit L1
 Golden Hill Rd.
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)

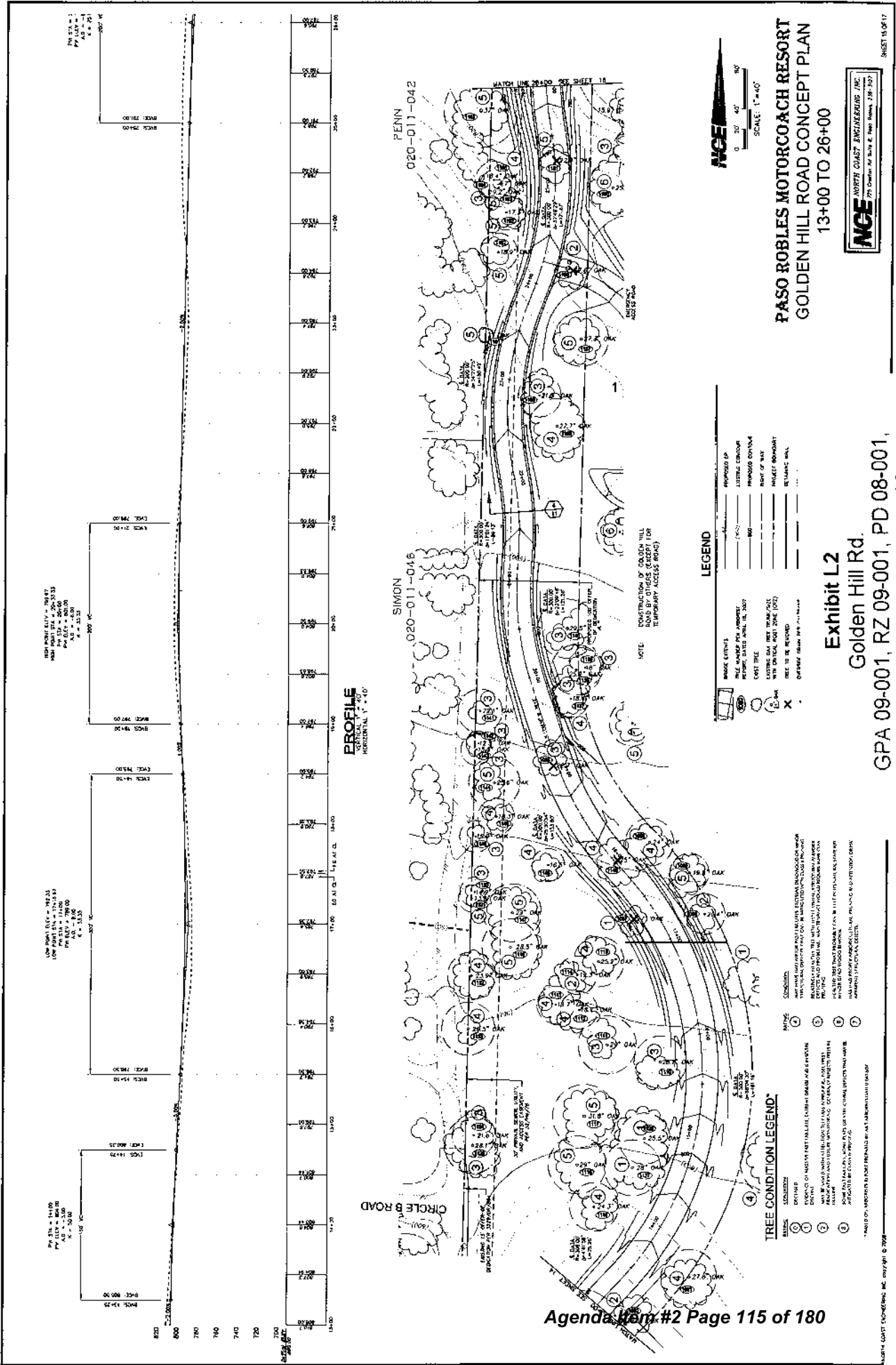
TREE CONDITION LEGEND:

SYMBOL	CONDITION
(1)	DECEASED
(2)	GOOD
(3)	POOR
(4)	POOR
(5)	POOR
(6)	POOR
(7)	POOR
(8)	POOR
(9)	POOR
(10)	POOR

*FIELD CONDITIONS ONLY

LEGEND

(Symbol)	PROPOSED SP
(Symbol)	ENTIRE DITCH
(Symbol)	PROPOSED DITCH
(Symbol)	POINT OF VIEW
(Symbol)	PROJECT BOUNDARY
(Symbol)	EXISTING MAIL
(Symbol)	100-YEAR ONE PERCENT FLOOD LINE
(Symbol)	EXISTING MAIL
(Symbol)	100-YEAR ONE PERCENT FLOOD LINE
(Symbol)	EXISTING MAIL
(Symbol)	100-YEAR ONE PERCENT FLOOD LINE

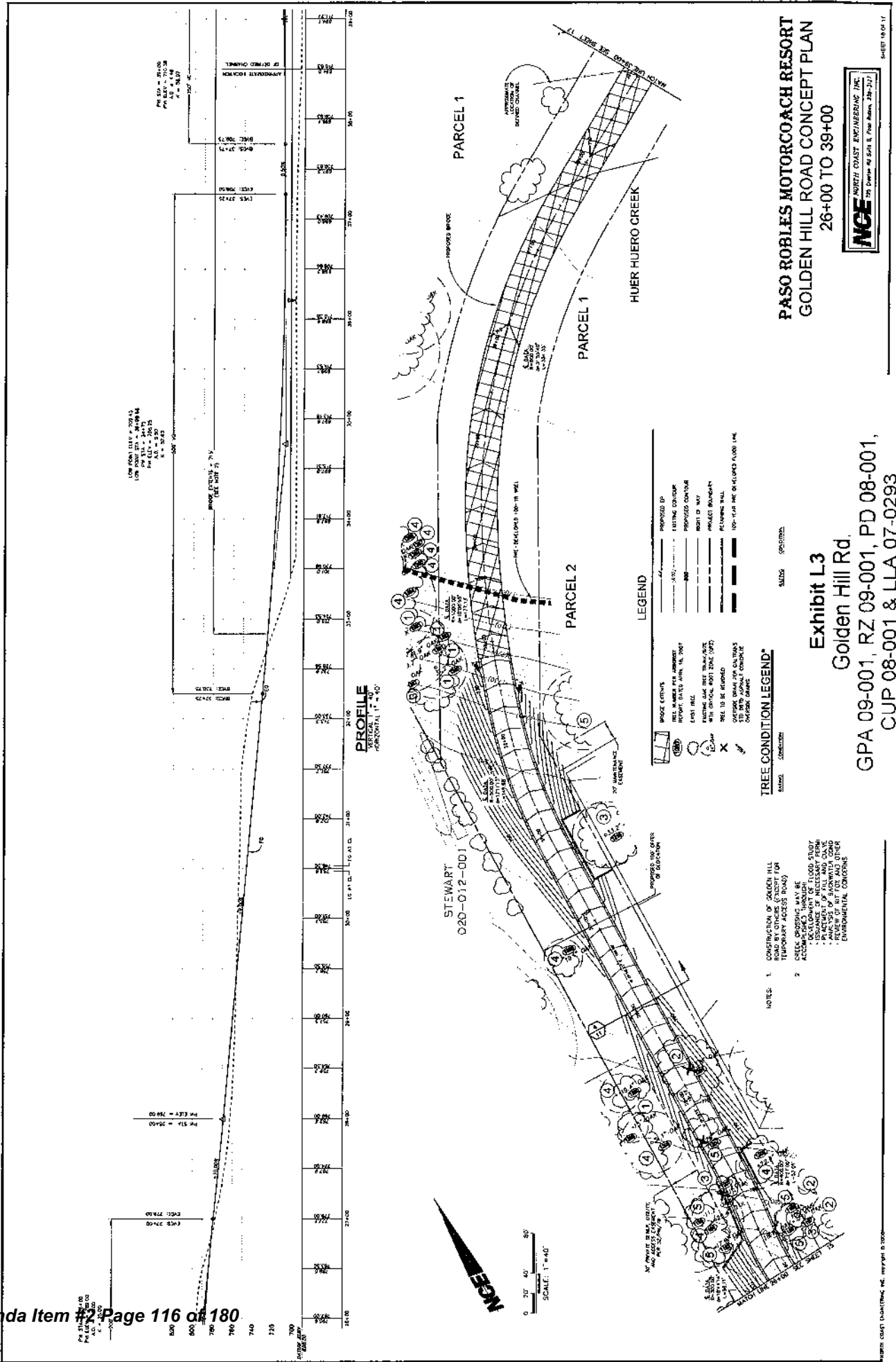


PASO ROBLES MOTORCOACH RESORT
GOLDEN HILL ROAD CONCEPT PLAN
13+00 TO 26+00

Exhibit L2
 Golden Hill Rd.
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)



SHEET 16 OF 17



PASO ROBLES MOTORCOACH RESORT
GOLDEN HILL ROAD CONCEPT PLAN
 26+00 TO 39+00

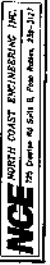
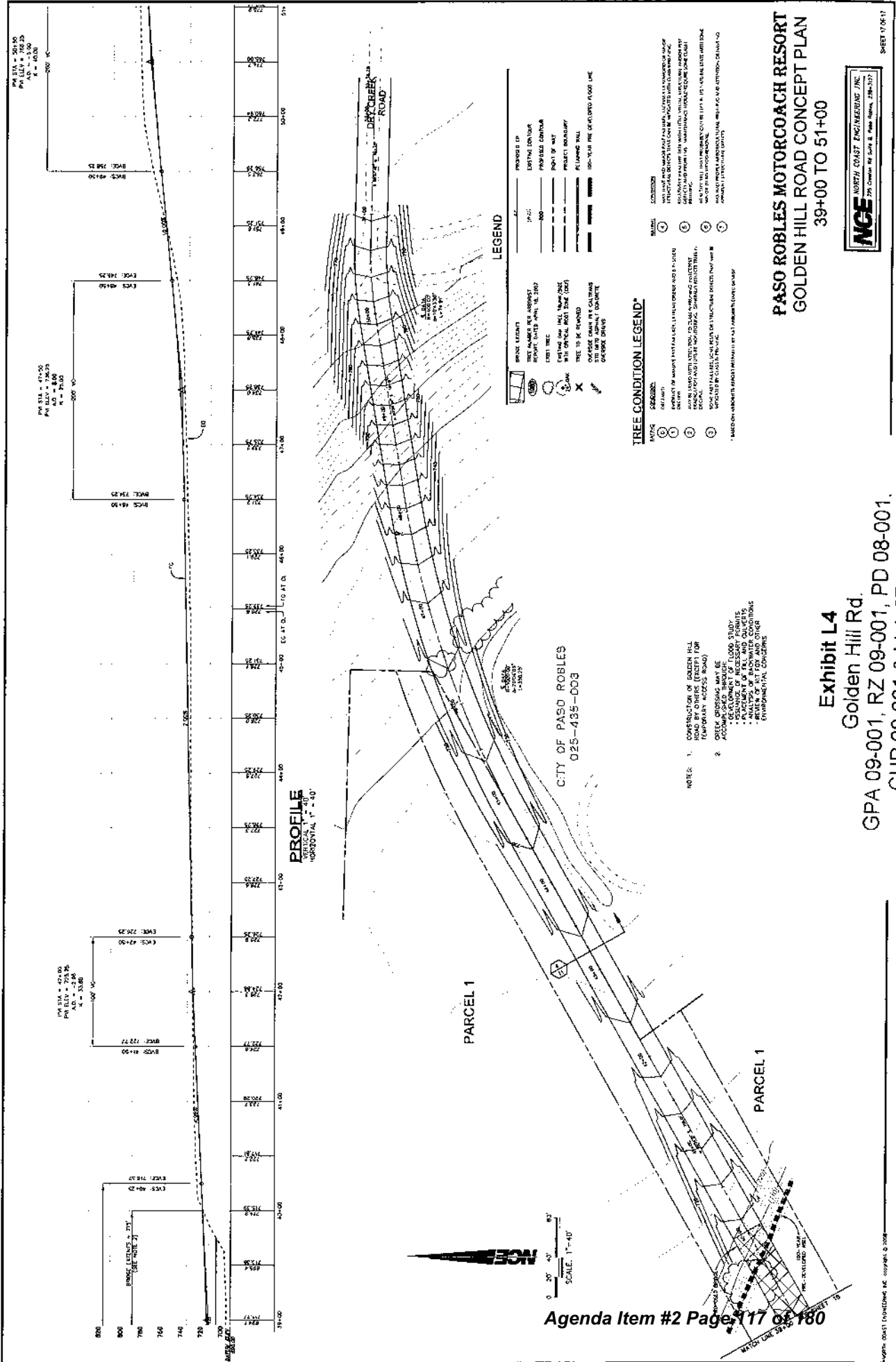


Exhibit L3
 Golden Hill Rd.
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)



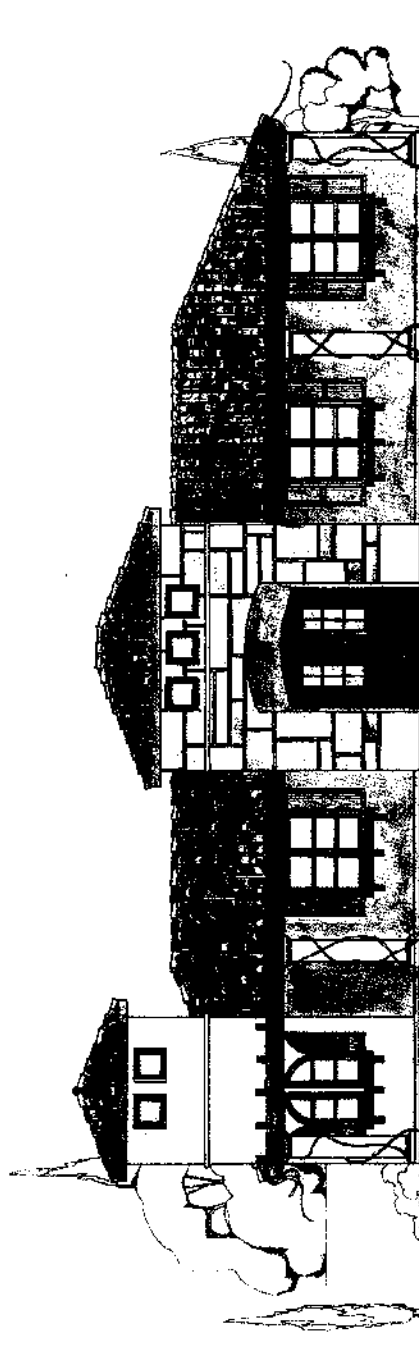
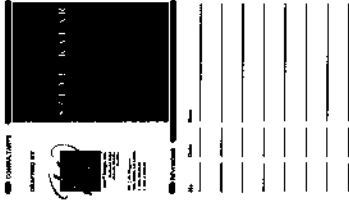
PASO ROBLES MOTORCOACH RESORT
GOLDEN HILL ROAD CONCEPT PLAN
 39+00 TO 51+00



Exhibit L4
 Golden Hill Rd.
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)

PASO ROBLES
MOTORCOACH
RESORT
GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 239-4560
FAX (805) 239-9912



REGISTRATION

REGISTRATION
COLOR &
MATERIALS
SCHEDULE

01-087 FEB 7, 2017
PROJECT: PASO ROBLES MOTORCOACH RESORT
SECTION: A1

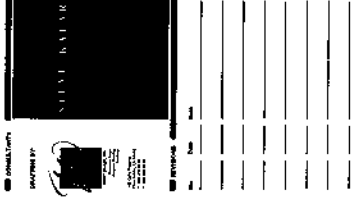
A1

Exhibit M1
Registration Building Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

PASO ROBLES
MOTORCOACH
RESORT

GOLDEN HILL RD
PASO ROBLES, CA 93946

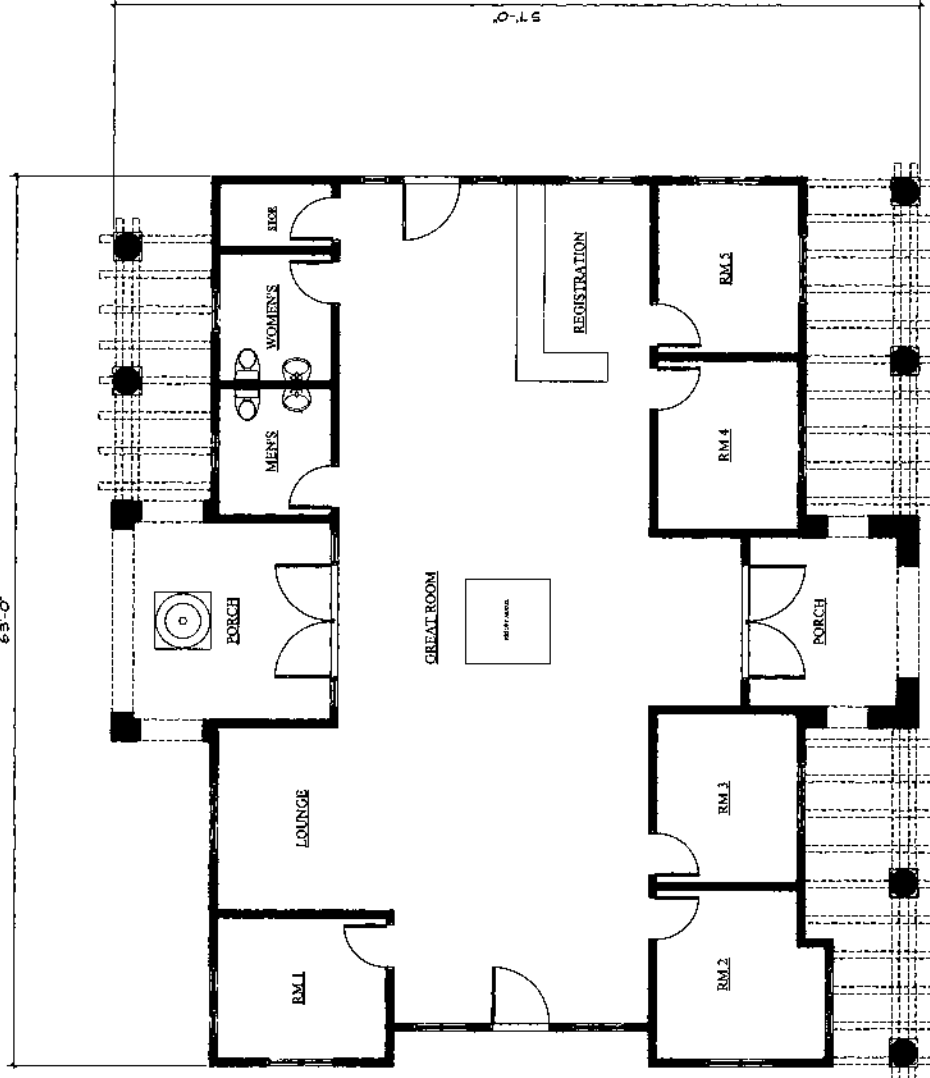
(805) 239-4560
FAX (805) 239-9912



REGISTRATION
FLOOR PLAN

01-107
FEB 7, 2007

A2

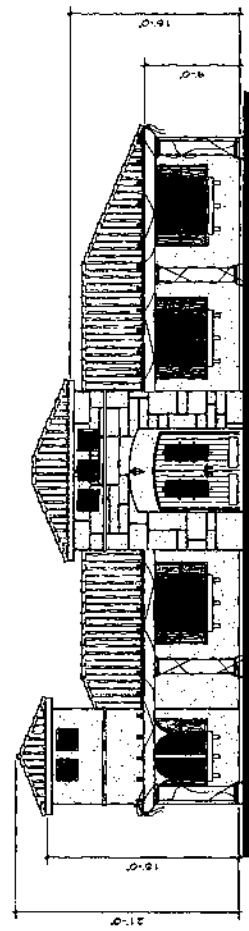


SCALE: 1/4" = 1'-0"

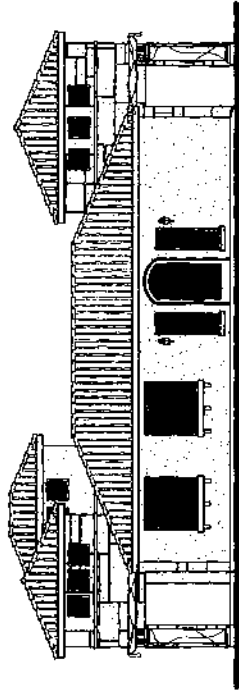
FLOOR PLAN

AREA CALC.	
AREA	PERCENT
TOTAL AREA	TOTAL PERCENT
TOTAL PERCENT	TOTAL PERCENT

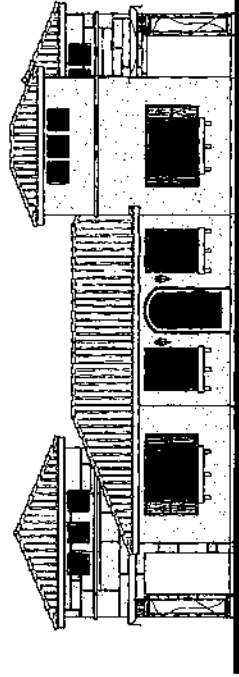
Exhibit M2
Registration Building Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)



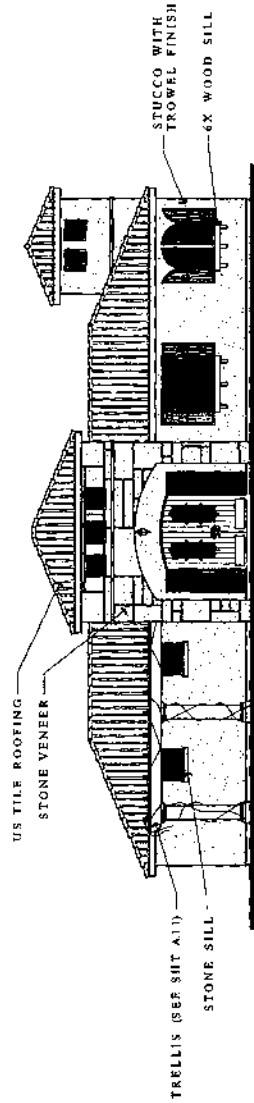
EAST ELEVATION



NORTH ELEVATION



SOUTH ELEVATION



WEST ELEVATION

SCALE: 3/16" = 1'-0"

PASO ROBLES
MOTORCOACH
RESORT
GOLDEN HILL RD.
PASO ROBLES, CA 93446

(805) 239-4560
FAX (805) 239-9912



No.	Date	Revised

DATE: FEB 7, 2007
PROJECT: PASO ROBLES
REGISTRATION
ELEVATIONS

01-107
FEB 7, 2007

A3

Exhibit M3
Registration Building Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

PASO ROBLES
MOTORCOACH
RESORT

GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 239-4560

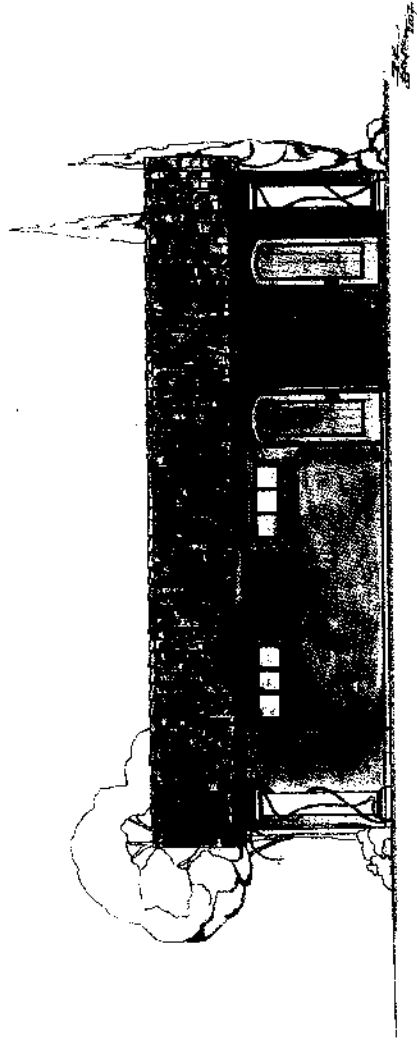
FAX (805) 239-9912

CONTRACT NO. _____
DATE _____
PROJECT NO. _____
SHEET NO. _____
SHEET TOTAL _____
SHEET TITLE _____
SHEET SCALE _____
SHEET AREA _____

POOLHOUSE
COLOR &
MATERIALS
SCHEDULE

01-107 FEB 7, 2007

A4



POOLHOUSE

Exhibit N1
Pool House Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

PASO ROBLES
MOTORCOACH
RESORT

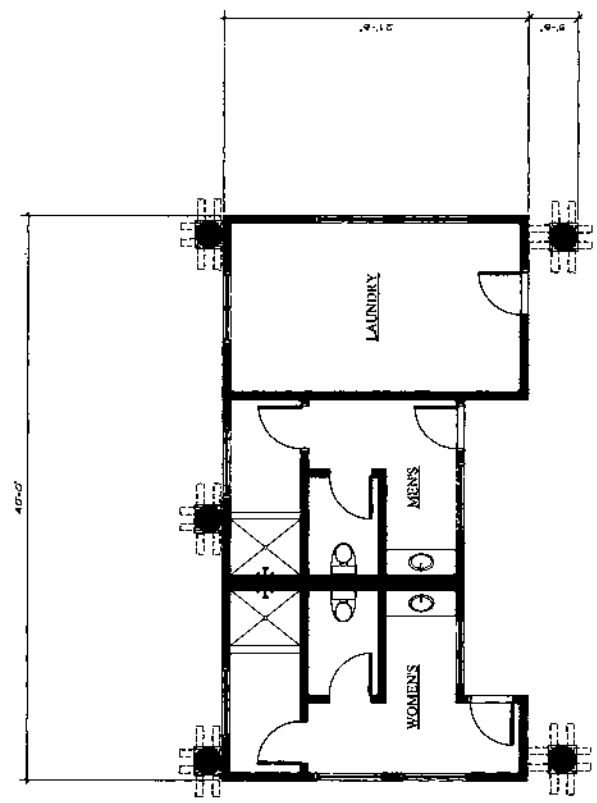
GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 239-4560
FAX (805) 239-9912

ARCHITECT
 CONTRACTOR
 ENGINEER
 PLUMBER
 ELECTRICIAN
 MECHANICAL
 OTHER

SHEET TITLE
**POOLHOUSE
FLOOR PLAN**

91-87 FEB 7, 2007
 PROJECT NO. 91-87
 SHEET NO. **A5**



FLOOR PLAN



SCALE: 1/4" = 1'-0"

AREA CALC.	
PERIMETER	144'-0"
AREA	1,600'-0"
PERIMETER	144'-0"
AREA	1,600'-0"

Exhibit N2
 Pool House Details
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)

PASO ROBLES
MOTORCOACH
RESORT

GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 239-4560

FAX (805) 239-9912

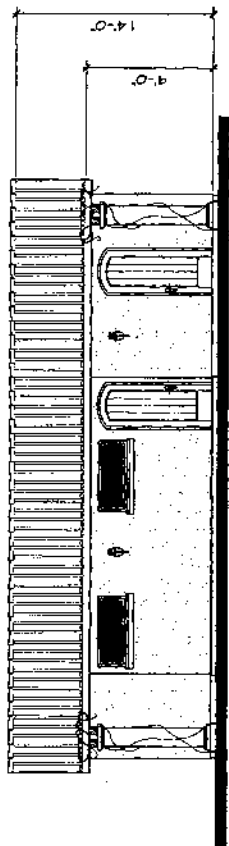


No.	Description	Date

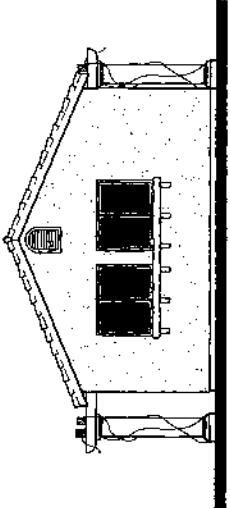
PROJECT NO. _____
**POOLHOUSE
ELEVATIONS**

DATE: FEB 7, 2017
DRAWN BY: _____
CHECKED BY: _____

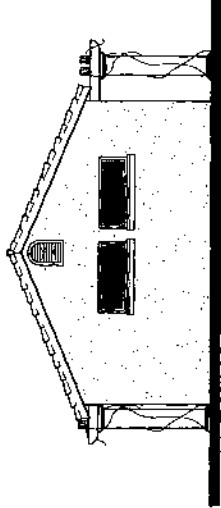
A6



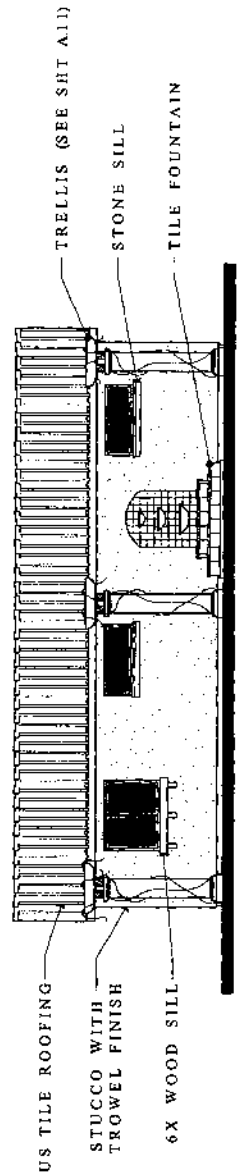
SOUTH ELEVATION



EAST ELEVATION



WEST ELEVATION



NORTH ELEVATION

SCALE: 1/4" = 1'-0"

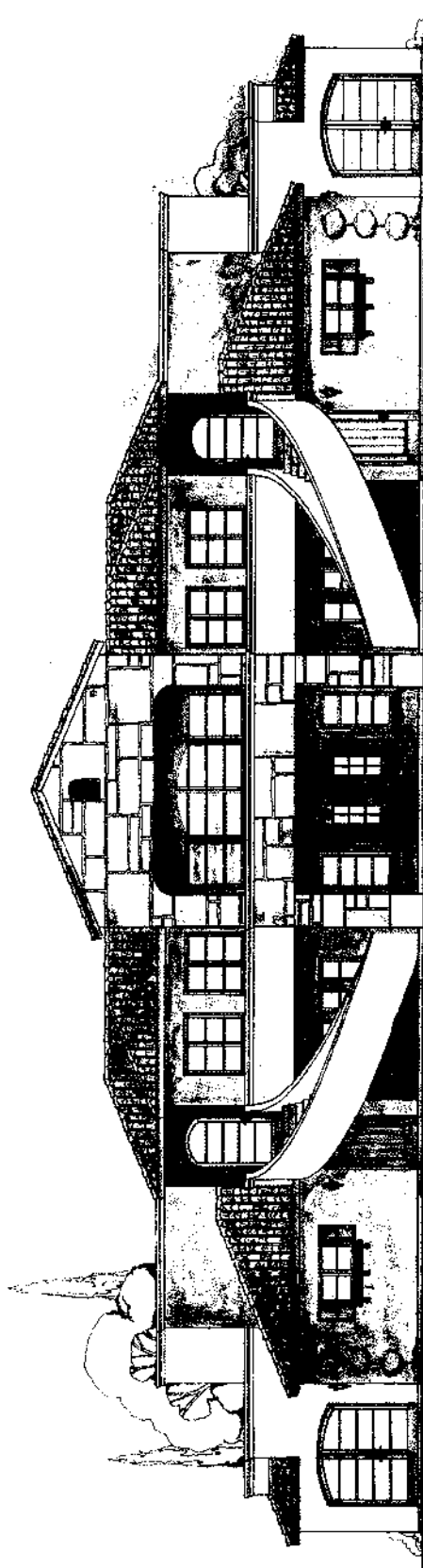
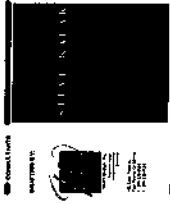
Exhibit N3
Pool House Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

PASO ROBLES
MOTORCOACH
RESORT

GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 239-4560

FAX (805) 239-9912



CLUBHOUSE

Exhibit O1
Clubhouse Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

CLUBHOUSE
COLOR &
MATERIALS
SCHEDULE

91-107 FEB 7, 2002

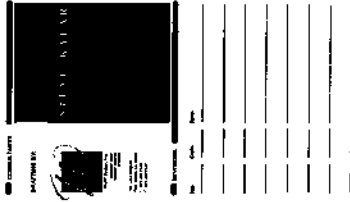
A7

PASO ROBLES
MOTORCOACH
RESORT

GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 239-4560

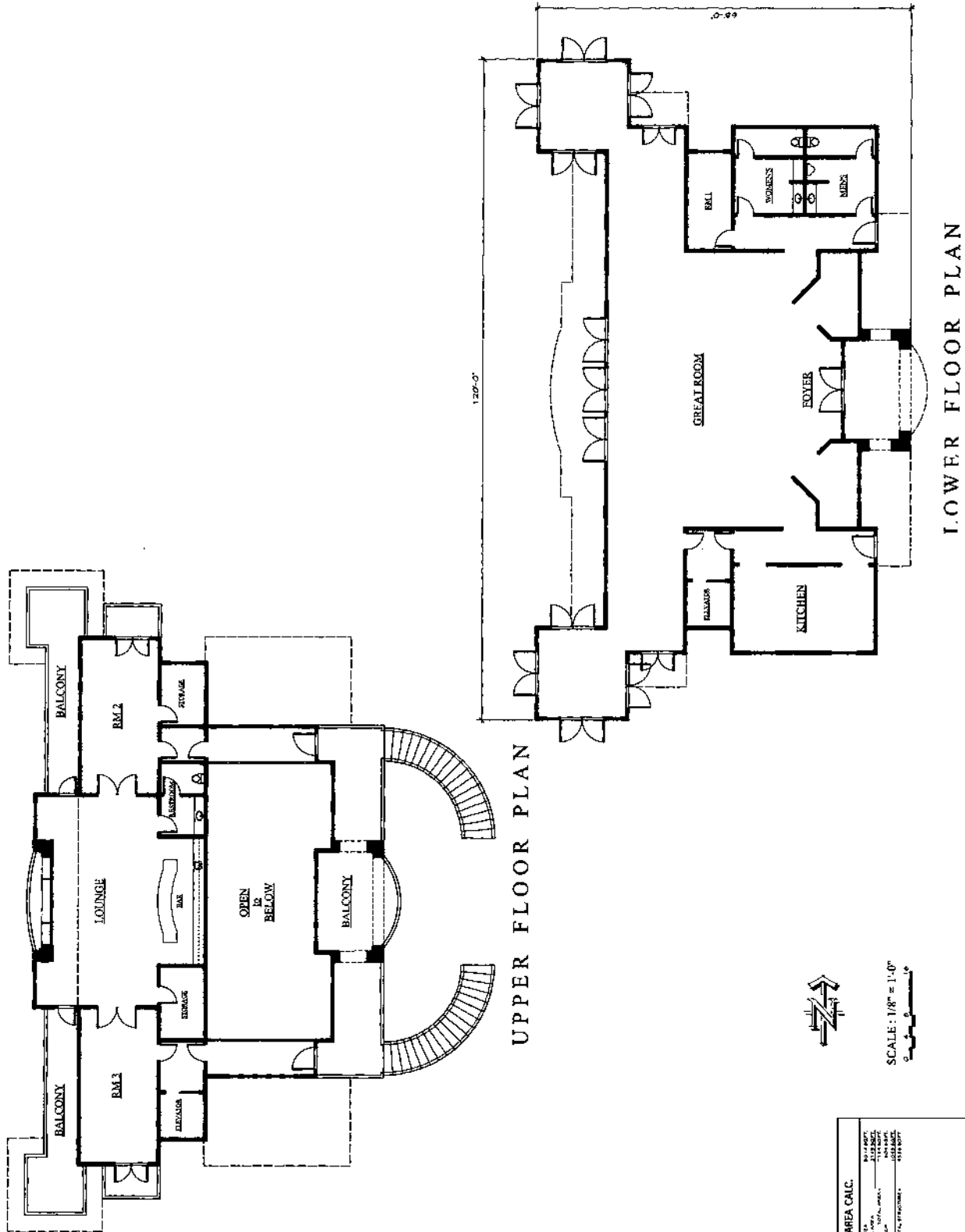
FAX (805) 239-9912



CLUBHOUSE
FLOOR PLANS

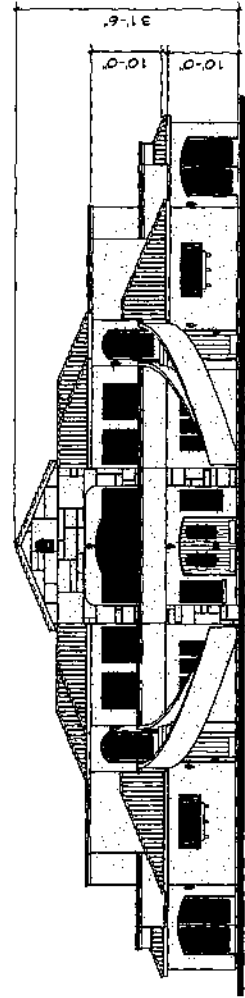
91-107 FEB 7, 2007

A8

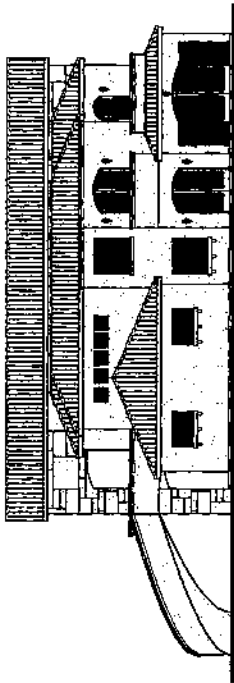


AREA CALC.	
NET AREA	10,120.00
GROSS AREA	10,120.00
CEILING AREA	10,120.00
FLOOR AREA	10,120.00
TOTAL FLOOR AREA	10,120.00

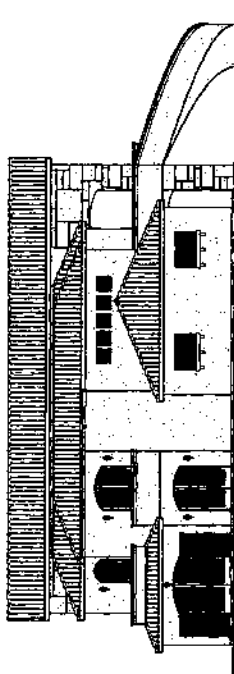
Exhibit O2
Clubhouse Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)



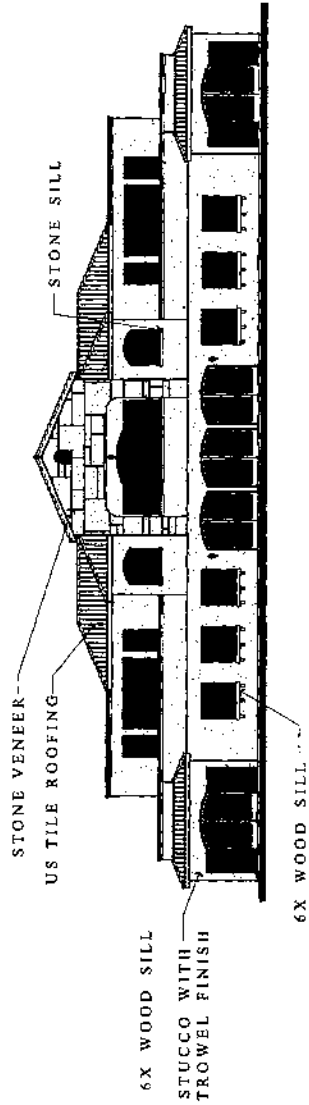
EAST ELEVATION



NORTH ELEVATION



SOUTH ELEVATION

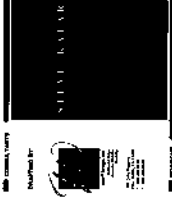


WEST ELEVATION

SCALE: 1/8" = 1'-0"

PASO ROBLES
MOTORCOACH
RESORT
GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 239-4560
FAX (805) 239-9912



CLUBHOUSE
ELEVATIONS

91-187 FEB 7, 2007

A9

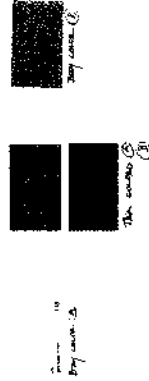
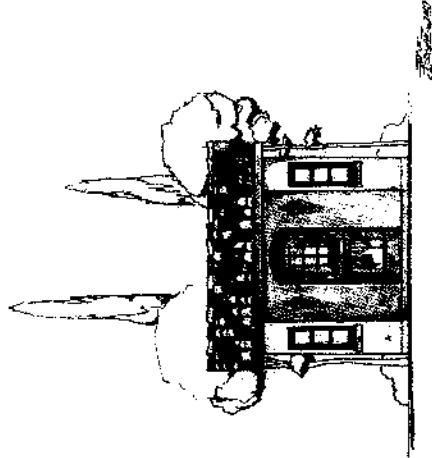
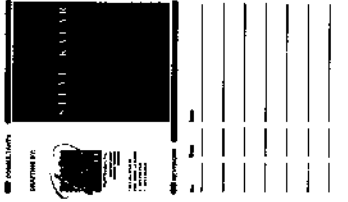
Exhibit O3
Clubhouse Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

PASO ROBLES
MOTORCOACH
RESORT

GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 236-4560

FAX (805) 239-9912



GUARDHOUSE

Exhibit P1
Guardhouse Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

GUARDHOUSE
COLOR &
MATERIALS
SCHEDULE

91-107 FEB 7, 2007

A10

**PASO ROBLES
MOTORCOACH
RESORT**
GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 239-4560
FAX (805) 239-9912

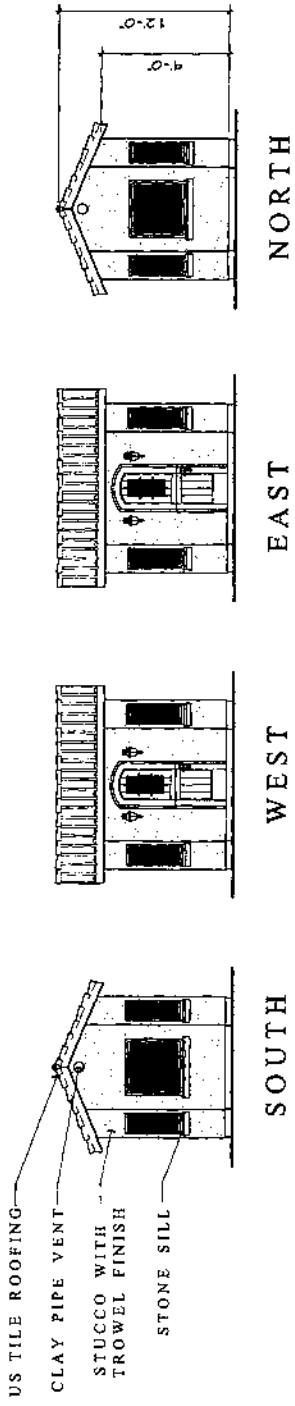


DATE	
BY	
REVISION	

**GUARDHOUSE
FLOOR PLAN &
ELEVATIONS**

01-107
FEB 7, 2007

A11



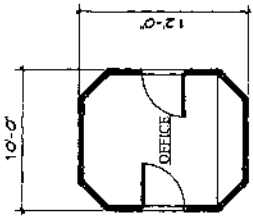
NORTH

EAST

WEST

SOUTH

ELEVATIONS



FLOOR PLAN



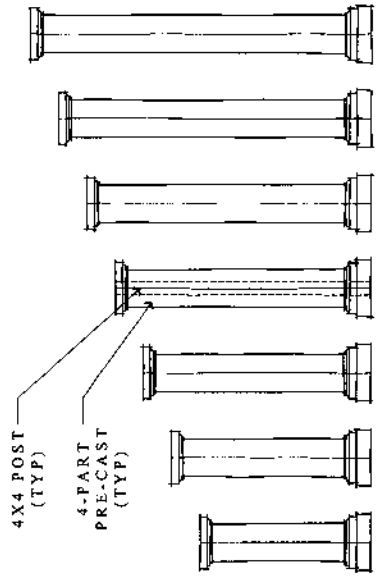
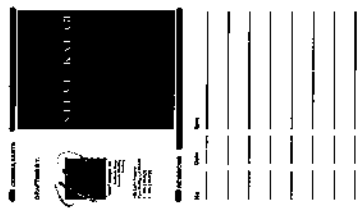
SCALE: 1/4" = 1'-0"

AREA CALC.	
PERIMETER AREA	113.82 SQ FT
PERIMETER AREA	113.82 SQ FT
PERIMETER AREA	113.82 SQ FT

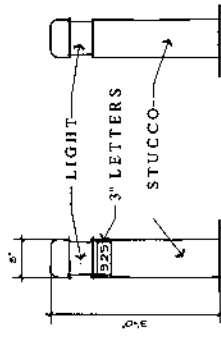
Exhibit P2
Gardhouse Details
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

PASO ROBLES
MOTORCOACH
RESORT
GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 239-4560
FAX (805) 239-9912



COLUMNS



SITE BOLLARD

NOTE:
FOR ALL STRUCTURES ALL HVAC
EQUIPMENT TO BE AT GRADE AND
SCREENED FROM VIEW WITH
LANDSCAPING

MISC ITEMS

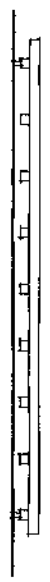
01-107 FEB 7, 2017
PROJECT: _____

A12

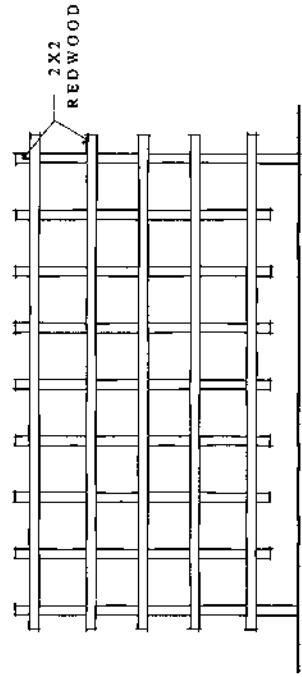
Exhibit Q1
Misc. Items
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

PASO ROBLES
MOTORCOACH
RESORT
GOLDEN HILL RD
PASO ROBLES, CA 93446

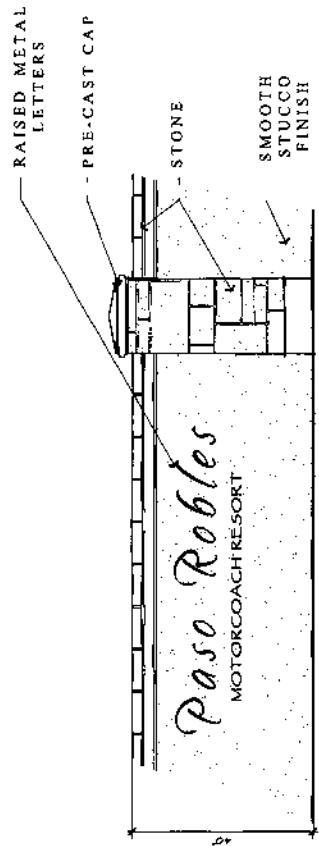
(805) 239-4560
FAX (805) 239-9912



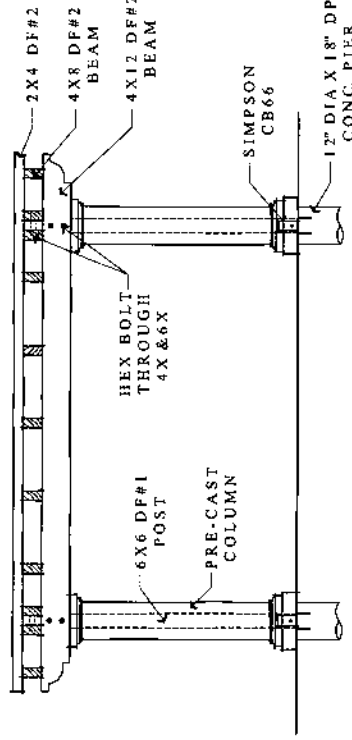
WALL TRELLIS PLAN



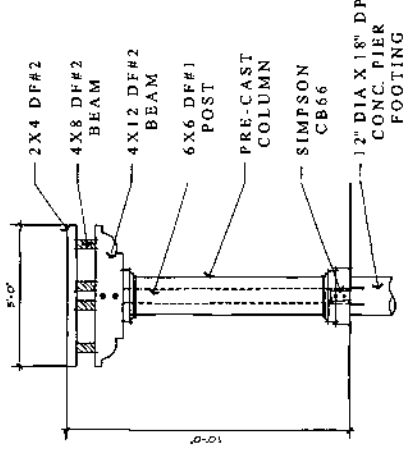
WALL TRELLIS ELEVATION



ENTRY SIGNAGE@TYPICAL WALL



TRELLIS FRONT ELEVATION



TRELLIS SIDE ELEVATION

MISC ITEMS

01-187
FEB 7, 2007

A13

Exhibit Q2
Misc. Items
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)

PASO ROBLES
MOTORCOACH
RESORT

GOLDEN HILL RD
PASO ROBLES, CA 93446

(805) 239-4560
FAX (805) 239-9912

DATE: _____
 TIME: _____
 BY: _____
 FOR: _____
 APPROVED: _____
 SIGNATURE: _____
 TITLE: _____

OUTDOOR
LIGHTING

FEB 7, 2007

A14

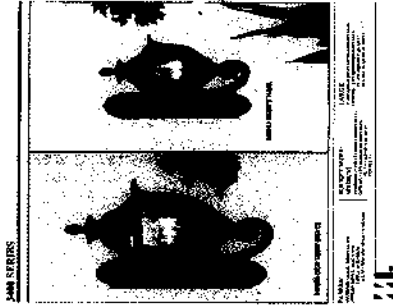
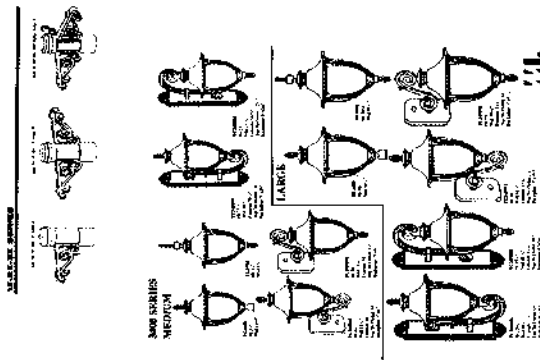
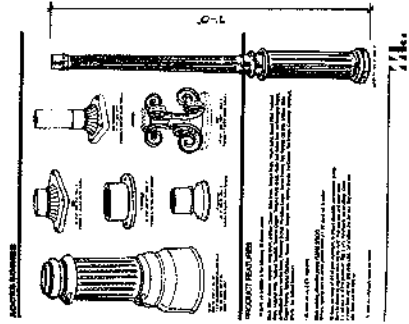
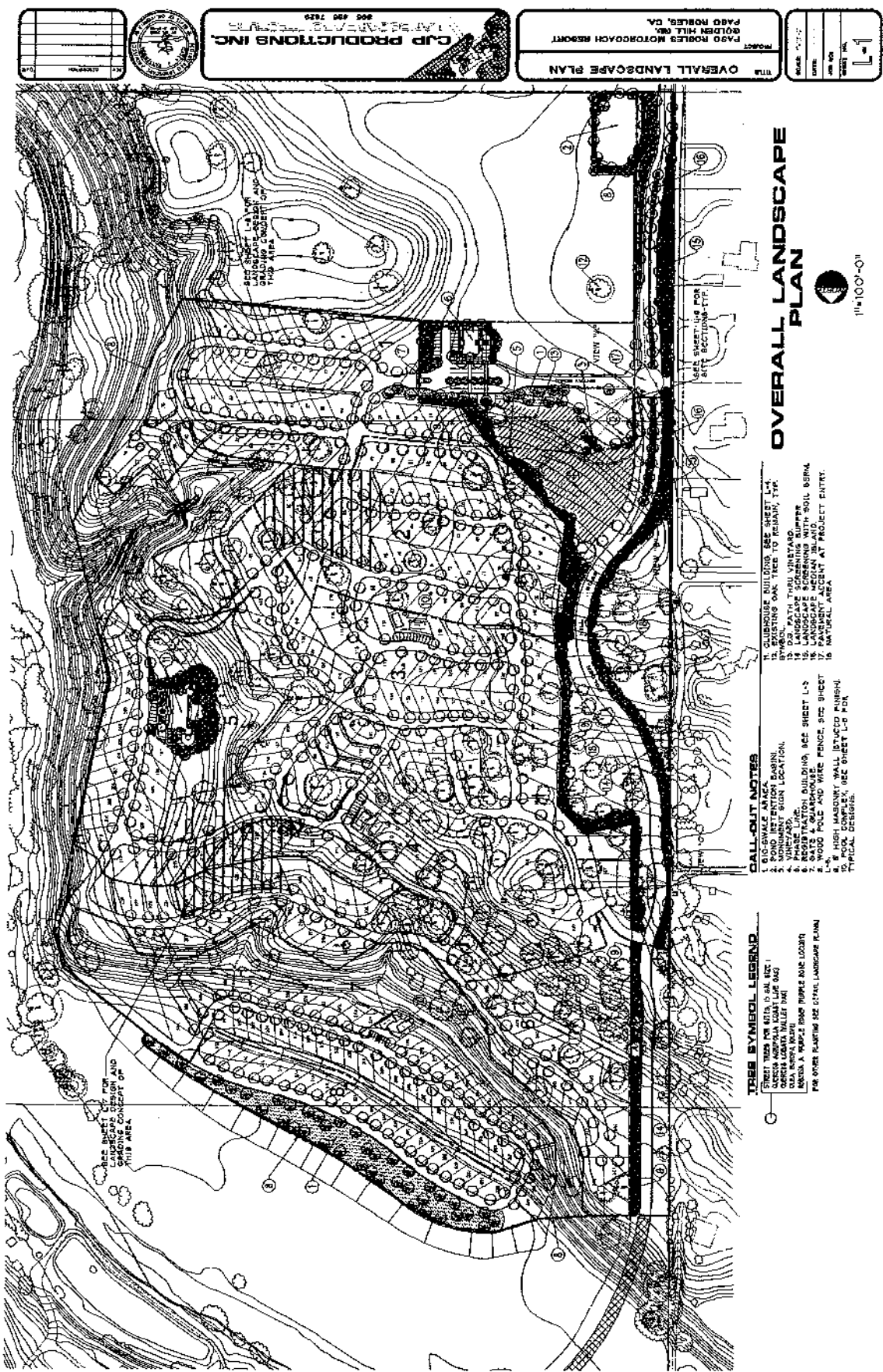


Exhibit R
Outdoor Lighting
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)



DATE	11/11/08
BY	JL
CHECKED BY	JL
SCALE	1/4\"/>



CJP PRODUCTIONS INC.
 14155 SAN ANTONIO BLVD., SUITE 100
 PASO ROBLES, CA 93555
 805.438.7825

TITLE: OVERALL LANDSCAPE PLAN
 PROJECT: PASO ROBLES MOTORCOACH RESORT
 LOCATION: GOLDEN HILL RD.
 PASO ROBLES, CA

DATE	11/11/08
BY	JL
CHECKED BY	JL
SCALE	1/4\"/>

OVERALL LANDSCAPE PLAN



- CALL-OUT NOTES**
- 1. 6" SWALE ALONG
 - 2. SOIL RETENTION BASIN
 - 3. EXISTING OAK TREE TO REMAIN, TYP.
 - 4. VINEYARD
 - 5. ROOST HOUSE BUILDING, REC SHEET L-3
 - 6. GATE & GUARDHOUSE
 - 7. WOOD POLE AND WIRE FENCE, SEE SHEET L-3
 - 8. HIGH MASONRY WALL, DETACHED FINISH TYPICAL DETAIL, REC SHEET L-3 FOR
 - 9. QUARRHOUSE BUILDING, REC SHEET L-4
 - 10. EXISTING OAK TREE TO REMAIN, TYP.
 - 11. 8" DIA. PATH THRU VINEYARD
 - 12. LANDSCAPE SCREENING BUFFER
 - 13. LANDSCAPE MEDIAN ISLAND
 - 14. LANDSCAPE MEDIAN ISLAND
 - 15. NATURAL ASCENT AT PROJECT ENTRY
 - 16. NATURAL ASCENT

- TREE SYMBOL LEGEND**
- 1. TREE TRUNK PER AREA, 1/4" DIA. TREE
 - 2. UNLESS NOTED, ALL TREE TRUNKS ARE 1/4" DIA.
 - 3. UNLESS NOTED, ALL TREE TRUNKS ARE 1/4" DIA.
 - 4. UNLESS NOTED, ALL TREE TRUNKS ARE 1/4" DIA.
 - 5. UNLESS NOTED, ALL TREE TRUNKS ARE 1/4" DIA.
 - 6. UNLESS NOTED, ALL TREE TRUNKS ARE 1/4" DIA.
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 - 11. UNLESS NOTED, ALL TREE TRUNKS ARE 1/4" DIA.
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 - 69. UNLESS NOTED, ALL TREE TRUNKS ARE 1/4" DIA.
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Exhibit S1
 Landscape Plans
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)

DATE	1/10/08
SCALE	1"=40'-0"
PROJECT	PASO ROBLES MOTORCOACH RESORT
PROJECT NO.	08-001
SHEET NO.	L-2



CUP PRODUCTIONS INC.
 LANDSCAPE ARCHITECTURE
 805 488 7828

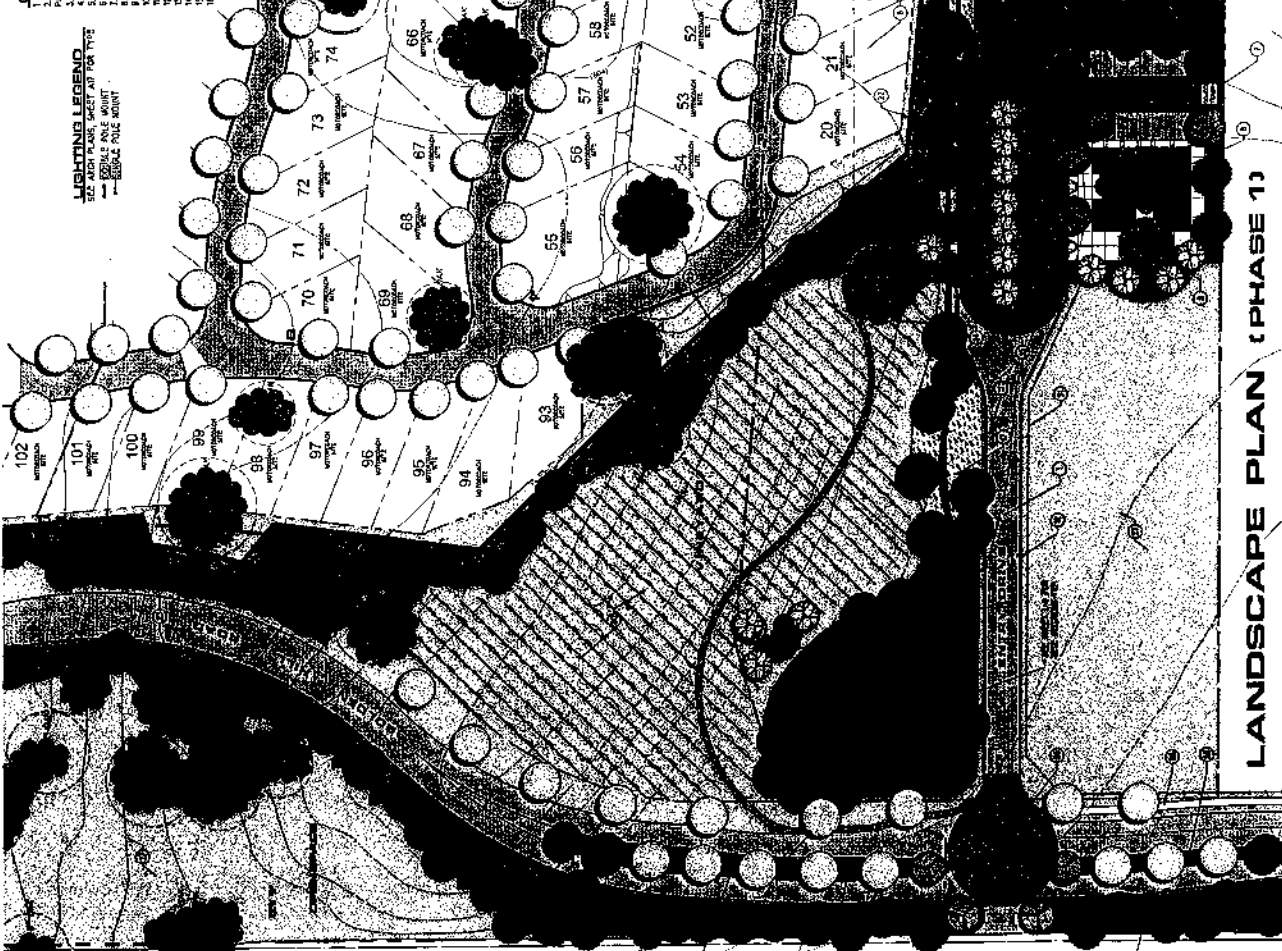
LANDSCAPE PLAN (PHASE 1)
 & LIGHTING PLAN
 PASO ROBLES MOTORCOACH RESORT
 GOLDEN HILL RD.
 PASO ROBLES, CA

DATE	1/10/08
SCALE	1"=40'-0"
PROJECT	PASO ROBLES MOTORCOACH RESORT
PROJECT NO.	08-001
SHEET NO.	L-2

- CALL-OUT NOTES**
1. AC DRIVEWAY
 2. ACCEPT PAVEMENT, FENCEABLE INTERLOCKING CONCRETE
 3. FINISH DETENTION BASIN
 4. WALKWAY
 5. SIDEWALK
 6. SIDEWALK ACCESS GATE
 7. WOOD PILE AND WIRE FENCE
 8. 4" HIGH JACOBY WALL STUCCO FINISH
 9. 2" HIGH JACOBY WALL STUCCO FINISH
 10. LANDSCAPE SCREENING WITH 50% BEDIA
 11. LANDSCAPE MEDIA ISLAND
 12. 12" HIGH CONCRETE BORDER BASES TO AS
 13. AIR-VOL BLOCKS OVER CONCRETE BOUNDARIAL BLEND WITH 1" CONCRETE CONTAINMENT CURB
 14. 12" HIGH MEDIA ISLAND
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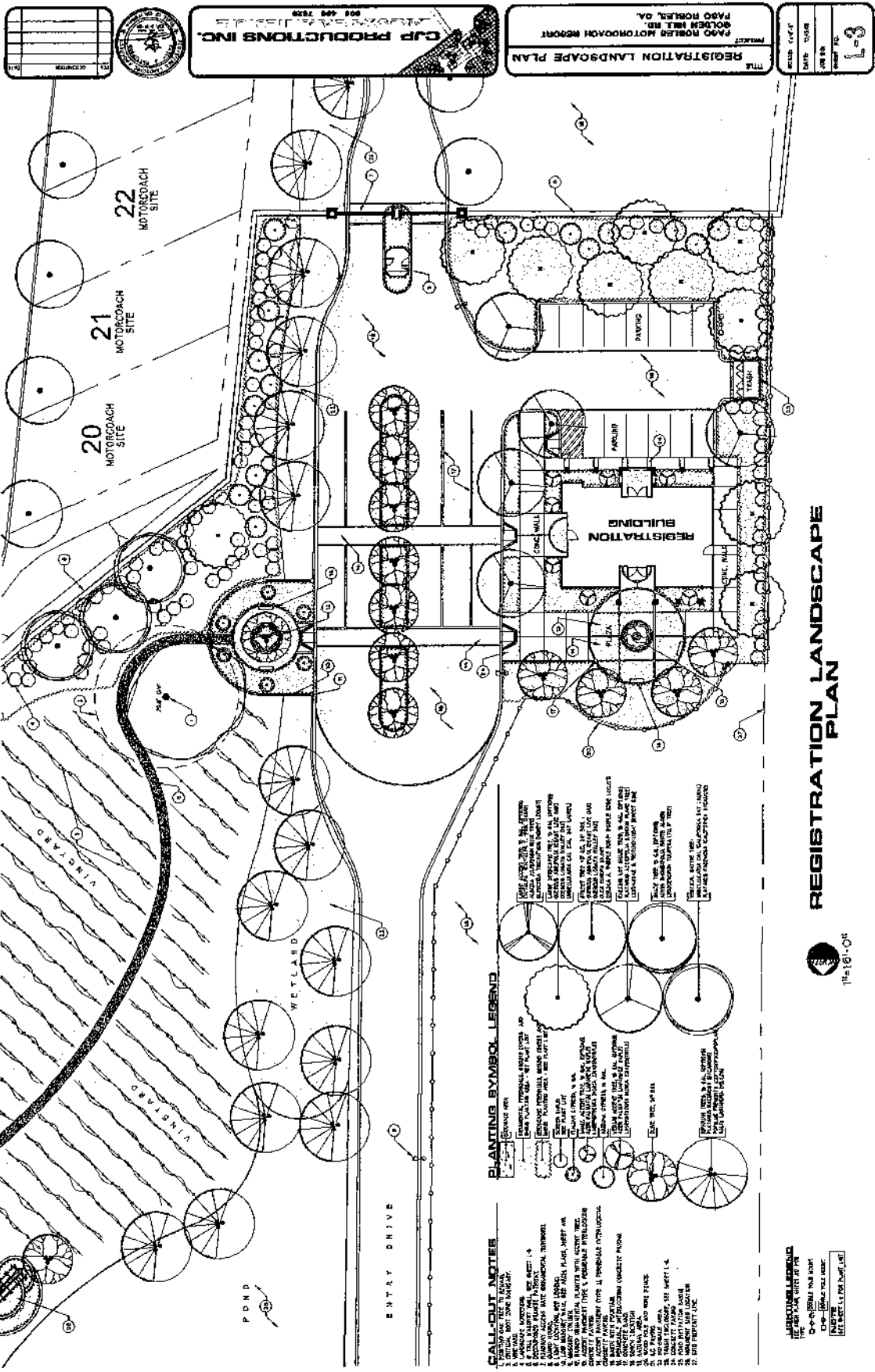
- PLANTING SYMBOL LEGEND**
- 1. 12" HIGH MEDIA ISLAND
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- LIGHTING LEGEND**
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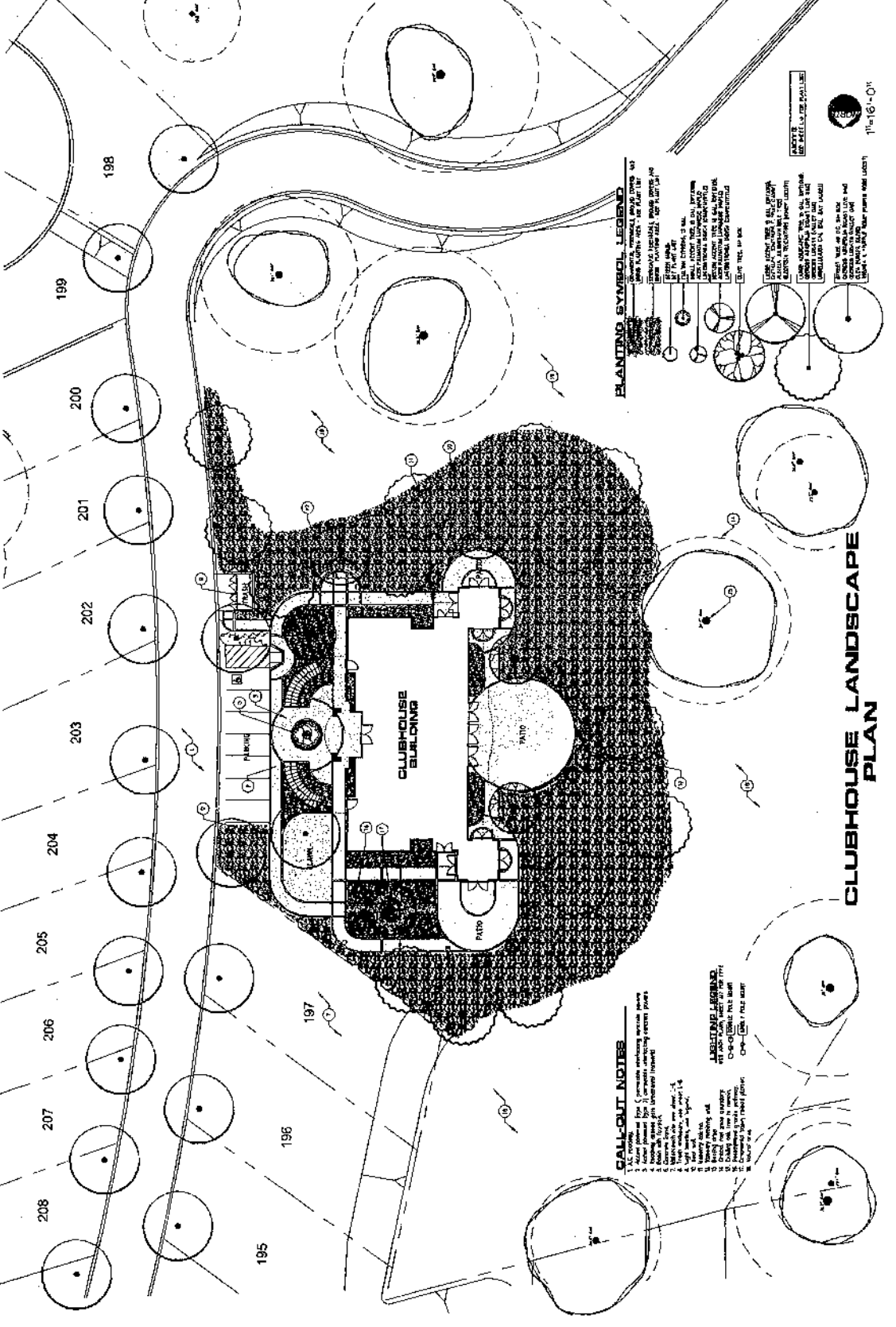


LANDSCAPE PLAN (PHASE 1)

Exhibit S2
 Landscape Plans
 GPA 09-001, RZ 09-001, PD 08-001.
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)



		TITLE CLUBHOUSE LANDSCAPE PLAN	
202 400 1528 CUP PRODUCTIONS INC.		PASO ROBLES MOTORCOACH REPORT PASO ROBLES, TX	
DATE: 07/09/10 DRAWN: WJW CHECKED: WJW SCALE: AS SHOWN SHEET NO.: 4			



PLANTING SYMBOL LEGEND

1. 1/4" SCALE: 1" = 10'-0"

2. 1/2" SCALE: 1" = 20'-0"

3. 3/4" SCALE: 1" = 30'-0"

4. 1" SCALE: 1" = 40'-0"

5. 1 1/4" SCALE: 1" = 50'-0"

6. 1 1/2" SCALE: 1" = 60'-0"

7. 1 3/4" SCALE: 1" = 70'-0"

8. 2" SCALE: 1" = 80'-0"

9. 2 1/4" SCALE: 1" = 90'-0"

10. 2 1/2" SCALE: 1" = 100'-0"

11. 2 3/4" SCALE: 1" = 110'-0"

12. 3" SCALE: 1" = 120'-0"

13. 3 1/4" SCALE: 1" = 130'-0"

14. 3 1/2" SCALE: 1" = 140'-0"

15. 3 3/4" SCALE: 1" = 150'-0"

16. 4" SCALE: 1" = 160'-0"

17. 4 1/4" SCALE: 1" = 170'-0"

18. 4 1/2" SCALE: 1" = 180'-0"

19. 4 3/4" SCALE: 1" = 190'-0"

20. 5" SCALE: 1" = 200'-0"

21. 5 1/4" SCALE: 1" = 210'-0"

22. 5 1/2" SCALE: 1" = 220'-0"

23. 5 3/4" SCALE: 1" = 230'-0"

24. 6" SCALE: 1" = 240'-0"

25. 6 1/4" SCALE: 1" = 250'-0"

26. 6 1/2" SCALE: 1" = 260'-0"

27. 6 3/4" SCALE: 1" = 270'-0"

28. 7" SCALE: 1" = 280'-0"

29. 7 1/4" SCALE: 1" = 290'-0"

30. 7 1/2" SCALE: 1" = 300'-0"

31. 7 3/4" SCALE: 1" = 310'-0"

32. 8" SCALE: 1" = 320'-0"

33. 8 1/4" SCALE: 1" = 330'-0"

34. 8 1/2" SCALE: 1" = 340'-0"

35. 8 3/4" SCALE: 1" = 350'-0"

36. 9" SCALE: 1" = 360'-0"

37. 9 1/4" SCALE: 1" = 370'-0"

38. 9 1/2" SCALE: 1" = 380'-0"

39. 9 3/4" SCALE: 1" = 390'-0"

40. 10" SCALE: 1" = 400'-0"

CALL-OUT NOTES

1. 1/4" SCALE: 1" = 10'-0"

2. 1/2" SCALE: 1" = 20'-0"

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LIGHTING LEGEND

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Exhibit S4
 Landscape Plans
 GPA 09-001, RZ 09-001, PD 08-001.
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)



CUP PRODUCTIONS INC.
 205 498 7838

EXAMPLE SITE PLANS
 TRASH ENCL. & SIGN
 PASO ROBLES MOTORCOACH RESORT
 3900 HOBBS, CA
 BOBEN HILL RD.

1-6
 SCALE: 3/8" = 1'-0"

EXAMPLE DESIGN
 for
MOTORCOACH SITE
 BACK-IN SITE 1/4"=10'-0"

- CALL-OUT NOTES** - **PLEASE NOTE:**
1. THE TRASH ENCL. SHALL BE 6' TALL WITH 1/2" WIDE POLYESTER FENCING.
 2. THE TRASH ENCL. SHALL BE 10' WIDE.
 3. THE TRASH ENCL. SHALL BE 10' LONG.
 4. THE TRASH ENCL. SHALL BE 10' HIGH.
 5. THE TRASH ENCL. SHALL BE 10' DEEP.
 6. THE TRASH ENCL. SHALL BE 10' WIDE.
 7. THE TRASH ENCL. SHALL BE 10' LONG.
 8. THE TRASH ENCL. SHALL BE 10' HIGH.
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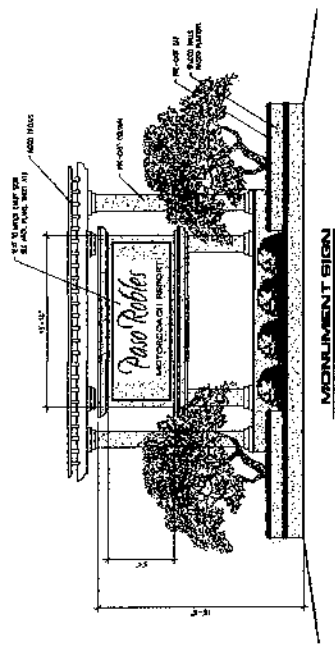
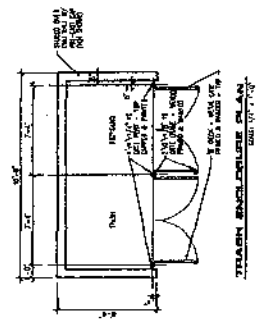
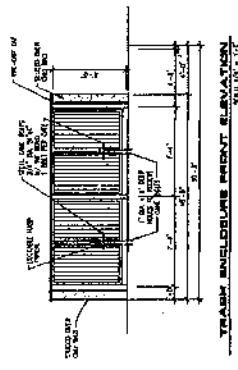
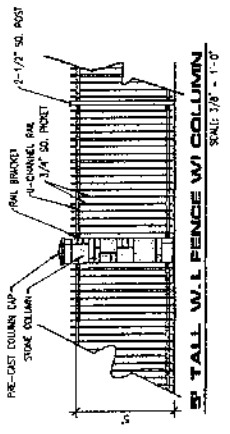
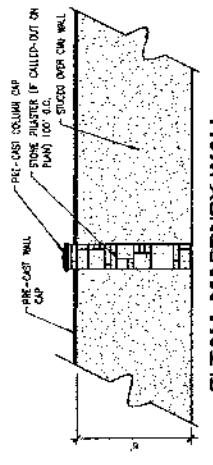
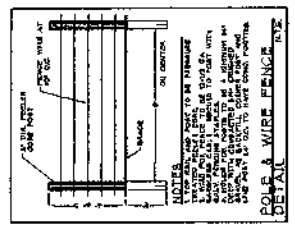
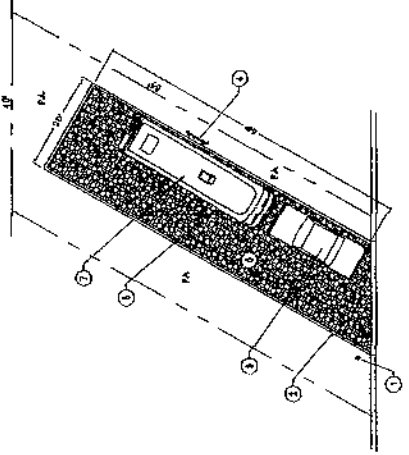


Exhibit S6
 Landscape Plans
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)



CUP PRODUCTIONS INC.
 Landscape Architecture
 4006 4th Street
 San Diego, CA 92161

LANDSCAPE PLAN FOR GRADING CONCEPT #2
 PASO ROBLES MOTORCOACH RESORT
 GARDEN HILL RD.
 PASO ROBLES, CA.

DATE:	08/21/11
SCALE:	AS SHOWN
SHEET NO.:	14
TOTAL SHEETS:	20

SYMBOL LEGEND

- [Symbol] ASPHALT
- [Symbol] CONCRETE
- [Symbol] GRAVEL
- [Symbol] IRON PIPE
- [Symbol] 12" DIA. PIPING
- [Symbol] 18" DIA. PIPING
- [Symbol] 24" DIA. PIPING
- [Symbol] 30" DIA. PIPING
- [Symbol] 36" DIA. PIPING
- [Symbol] 42" DIA. PIPING
- [Symbol] 48" DIA. PIPING
- [Symbol] 54" DIA. PIPING
- [Symbol] 60" DIA. PIPING
- [Symbol] 66" DIA. PIPING
- [Symbol] 72" DIA. PIPING
- [Symbol] 78" DIA. PIPING
- [Symbol] 84" DIA. PIPING
- [Symbol] 90" DIA. PIPING

PERSONAL INFORMATION CENTER NOTES
 SEE GRADING PLAN FOR DETAILS
 SEE MECHANICAL PLAN FOR DETAILS
 SEE ELECTRICAL PLAN FOR DETAILS
 SEE CIVIL PLAN FOR DETAILS
 SEE LANDSCAPE PLAN FOR DETAILS
 SEE GRADING CONCEPT #2 FOR DETAILS
 SEE GRADING CONCEPT #3 FOR DETAILS
 SEE GRADING CONCEPT #4 FOR DETAILS
 SEE GRADING CONCEPT #5 FOR DETAILS
 SEE GRADING CONCEPT #6 FOR DETAILS
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 SEE GRADING CONCEPT #15 FOR DETAILS
 SEE GRADING CONCEPT #16 FOR DETAILS
 SEE GRADING CONCEPT #17 FOR DETAILS
 SEE GRADING CONCEPT #18 FOR DETAILS
 SEE GRADING CONCEPT #19 FOR DETAILS
 SEE GRADING CONCEPT #20 FOR DETAILS

PLANTING LIST

ALL PLANTING LISTED IS SUBJECT TO AVAILABILITY. PLANTS NOT LISTED ARE TO BE OBTAINED THROUGH THE LANDSCAPE ARCHITECT'S CONTACT WITH THE SUPPLIER. PLANTS NOT LISTED ARE TO BE OBTAINED THROUGH THE LANDSCAPE ARCHITECT'S CONTACT WITH THE SUPPLIER. PLANTS NOT LISTED ARE TO BE OBTAINED THROUGH THE LANDSCAPE ARCHITECT'S CONTACT WITH THE SUPPLIER. PLANTS NOT LISTED ARE TO BE OBTAINED THROUGH THE LANDSCAPE ARCHITECT'S CONTACT WITH THE SUPPLIER.

IRRIGATION NOTES
 SEE MECHANICAL PLAN FOR IRRIGATION DETAILS
 SEE GRADING PLAN FOR IRRIGATION DETAILS
 SEE ELECTRICAL PLAN FOR IRRIGATION DETAILS
 SEE CIVIL PLAN FOR IRRIGATION DETAILS
 SEE LANDSCAPE PLAN FOR IRRIGATION DETAILS
 SEE GRADING CONCEPT #2 FOR IRRIGATION DETAILS
 SEE GRADING CONCEPT #3 FOR IRRIGATION DETAILS
 SEE GRADING CONCEPT #4 FOR IRRIGATION DETAILS
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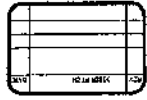


CALL-OUT NOTES

1. PLANTING TO BE OBTAINED THROUGH THE LANDSCAPE ARCHITECT'S CONTACT WITH THE SUPPLIER.
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LANDSCAPE PLAN FOR GRADING CONCEPT #2

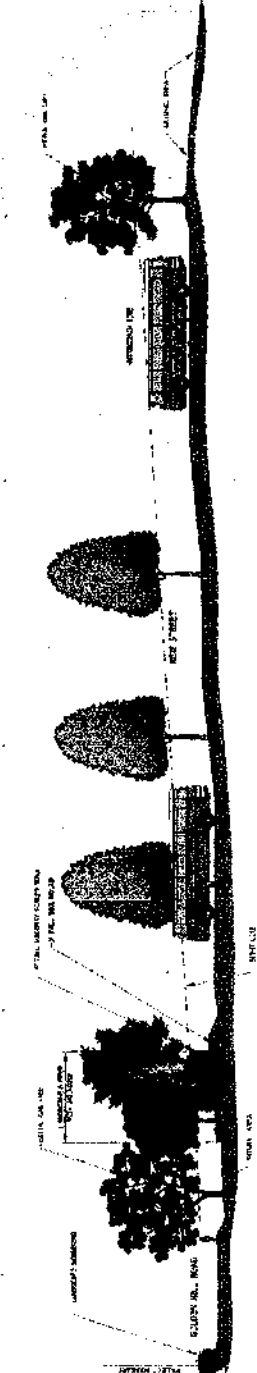
Exhibit S8
 Landscape Plans
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)



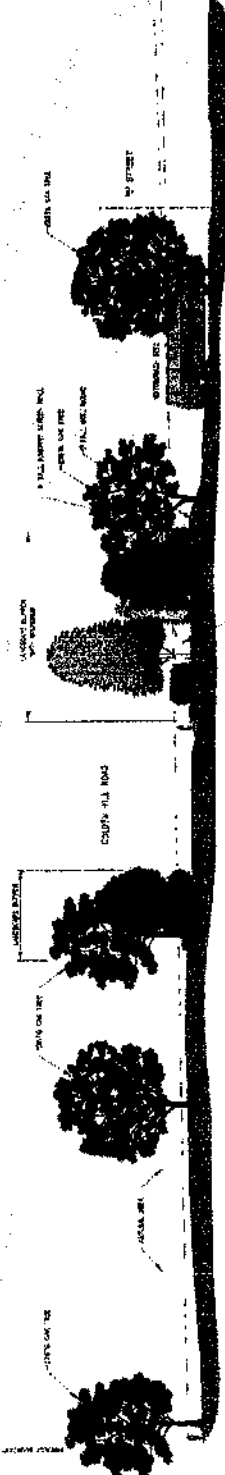
SITE SECTIONS

PROJECT	PASO ROBLES MOTORCOACH RESORT
LOCATION	GOLDEN HILL RD. PASO ROBLES, CA.

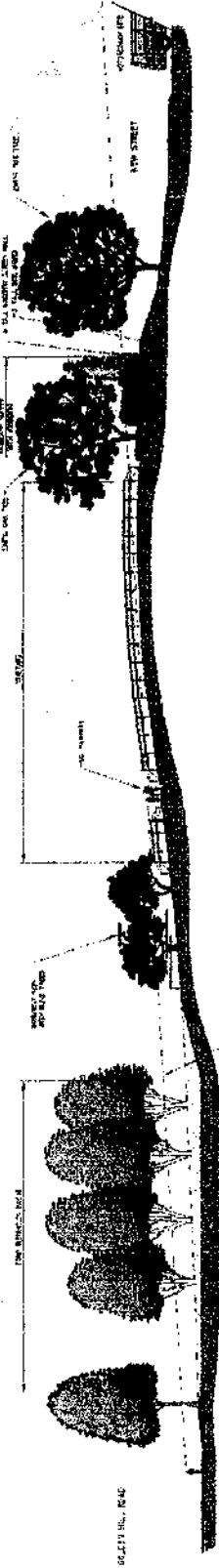
SCALE	1/4" = 1'-0"
DRAWN BY	DATE



SITE SECTION C-C
1/16" = 1'-0"



SITE SECTION B-B
1/16" = 1'-0"



SITE SECTION A-A
1/16" = 1'-0"

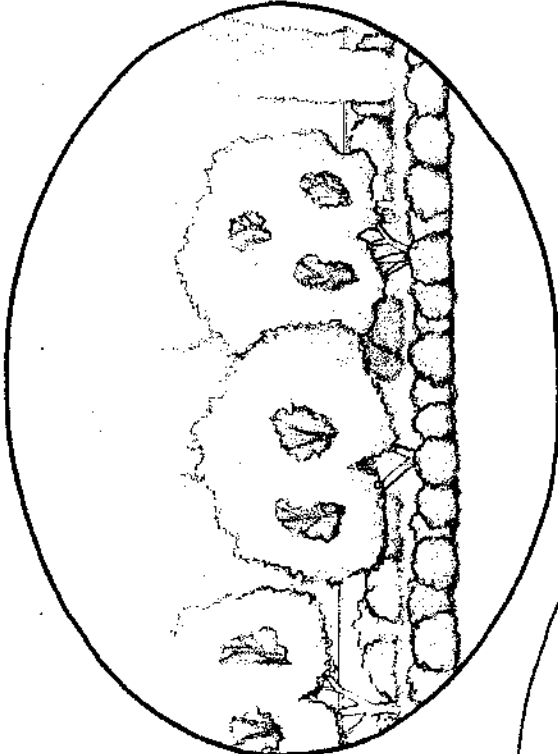
Exhibit S9
Landscape Plans
GPA 09-001, RZ 09-001, PD 08-001,
CUP 08-001 & LLA 07-0293
(Paso Robles Motorcoach)



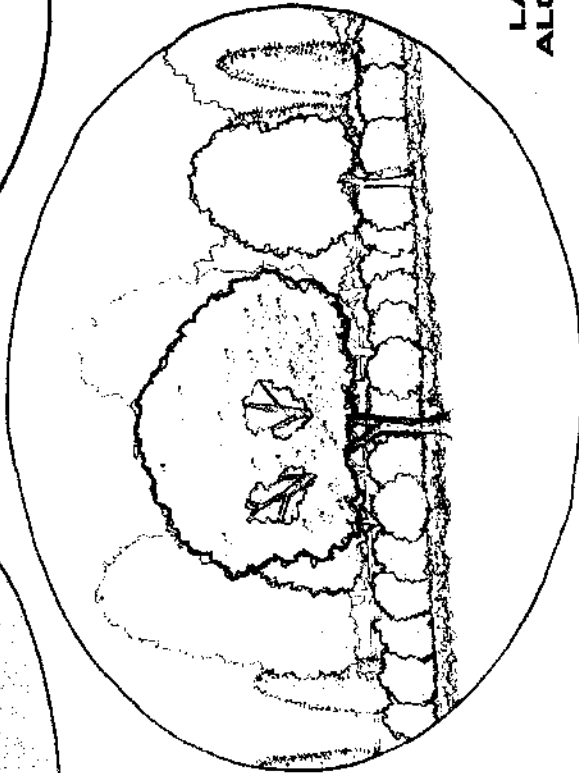
CJP PRODUCTIONS INC.
 LANDSCAPE ARCHITECTS
 2020 S. 10TH ST. SUITE 100
 PASO ROBLES, CA 93271
 TEL: 805.325.1000

LANDSCAPE VIEWS
 TITLE
 PASO ROBLES MOTORCOACH RESORT
 GOLDEN HILL RD.
 PASO ROBLES, CA
 PROJECT NO. 08-001

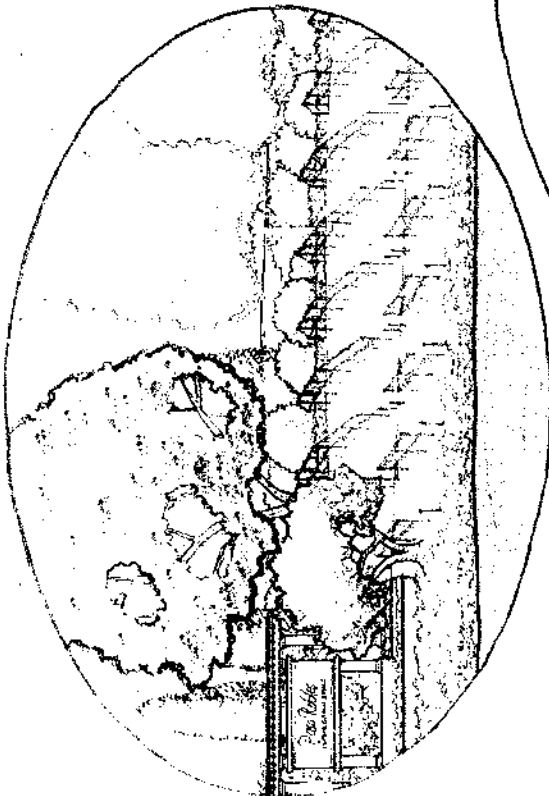
DATE	
BY	



C



B



A

**LANDSCAPE VIEWS
 ALONG LINES A, B & C**

Exhibit S10
 Landscape Plans
 GPA 09-001, RZ 09-001, PD 08-001,
 CUP 08-001 & LLA 07-0293
 (Paso Robles Motorcoach)

RESOLUTION NO.

A RESOLUTION OF
THE CITY COUNCIL OF THE CITY OF PASO ROBLES
APPROVING CONDITIONAL USE PERMIT 08-001
NORTH END OF GOLDEN HILL ROAD, APN 025-435-005, 006 and 007
APPLICANT – PASO 160, LLC –
PASO ROBLES MOTORCOACH RESORT

WHEREAS, PD 08-001 has been filed by North Coast Engineering on behalf of Paso 160, LLC to construct a 332 space RV resort on the 160 acre site located at the northern terminus of Golden Hill Road, North of Highway 46 East; and

WHEREAS, the 160 acre site is currently comprised of three parcels, the applicants are proposing to reconfigure the three parcels via Lot Line Adjustment 07-093, where Parcel 1 would be 58.8 acres, Parcel 2 would be 68.6 acres and Parcel 3 would be 21.4 acres; and

WHEREAS, PD 08-001 proposes to develop the Paso Robles Motorcoach project on Parcel 2; and

WHEREAS, according to Table 21.16.200, Permitted Use Table, transient lodging, including RV Parks in the POS zoning district with the approval of a Conditional Use Permit (CUP); and

WHEREAS, the Planning Commission held a duly noticed public hearing on February 24, 2009 on this project to accept public testimony on PD 08-001 and associated environmental review; and

WHEREAS, the Planning Commission on a 4-1 vote (1 abstention, 1 absent) recommended that the City Council approve PD 08-001; and

WHEREAS, the City Council held a duly noticed public hearing on March 17, 2009 on this project to accept public testimony on PD 08-001; and

WHEREAS, based upon the facts and analysis presented in the staff report and the attachments thereto, the public testimony received, and subject to the Conditions of Approval listed below, the City Council makes the following findings:

1. The establishment, maintenance or operation of the requested uses applied for, will not, under the circumstances of the particular case, be detrimental to the health, safety, morals, comfort, convenience and general welfare of the persons residing or working in the neighborhood of such proposed use, since the project has gone through the development review process including, environmental review and the processing of a Conditional Use Permit as required by Table 21.16.200 for hotel and motel facilities in the POS zoning districts; and
2. The proposed use will not be injurious or detrimental to property and improvements in the neighborhood or to the general welfare of the City, since the project will be required to

Attachment 10

comply with the recommended conditions of approval, including any environmental mitigation measures, and comply with any building and fire codes; and

3. The proposed use as conditioned would meet the intent of the General Plan, Zoning Ordinance and Economic Strategy by providing end-destination resort projects in the POS land use and zoning districts.

NOW, THEREFORE, BE IT RESOLVED, that the City Council of the City of El Paso de Robles does hereby approve Conditional Use Permit 08-001 subject to the following conditions:

STANDARD CONDITIONS

1. The applicant shall comply with all those standard and site specific conditions which are contained in the Resolution and its exhibits approving Planned Development 08-001 and associated Mitigated Negative Declaration.

SITE SPECIFIC CONDITIONS

2. Special events beyond typical resort activities shall be reviewed by the Planning Dept. to insure compliance with the Zoning Code and ALUP. The Police Department shall also review the activities. The number of people per acre shall be in compliance with the maximum density identified by the ALUP.
3. Any condition imposed by the Planning Commission in granting this Conditional Use Permit may be modified or eliminated, or new conditions may be added, provided that the Planning Commission shall first conduct a public hearing in the same manner as required for the granting of the original permit. No such modification shall be made unless the Commission finds that such modification is necessary to protect the public interest and/or neighboring properties, or, in the case of deletion of an existing condition, that such action is necessary to permit reasonable operation and use under the Conditional Use Permit.
4. The maximum length of stay for any RV space is 30 consecutive days.
5. Quiet hours shall be observed after 10p.m. every night to minimize noise disturbance. Generators shall not be permitted to run within the resort at any time.
6. All on-site operations shall be in conformance with the City's performance standards contained in Section 21.21.040 and as listed below:
 - a. Fire and Explosion Hazards. All activities involving, and all storage of, inflammable and explosive materials shall be provided with adequate safety devices against the hazard of fire and explosion and adequate firefighting and fire-suppression equipment and devices standard in industry and as approved by the fire department. All incineration is prohibited.
 - b. Radioactivity or Electrical Disturbance. Devices that radiate radio-frequency energy shall be so operated as not to cause interference with any activity carried on beyond the boundary line of the property upon which the device is located. Further, no radiation of

any kind shall be emitted which is dangerous to humans. All radio transmissions shall occur in full compliance with Federal Communications Commission (FCC) and other applicable regulations.

- c. Noise. No land use shall increase the ambient noise level as measured at the nearest residentially zoned property line to a level that constitutes a public nuisance.
- d. Vibration. No vibrations shall be permitted so as to cause a noticeable tremor measurable without instruments at the lot line.
- e. Smoke. Except for fireplaces and barbecues, no emission shall be permitted at any point from any chimney which would constitute a violation of standards established by the San Luis Obispo County Air Pollution Control District (APCD).
- f. Odors. Except for fireplaces and barbecues, no emission shall be permitted of odorous gases or other odorous matter in such quantities as to constitute a public nuisance.
- g. Fly Ash, Dust, Fumes, Vapors, Gases and Other Forms of Air Pollution. No emission shall be permitted which can cause damage to health, animals, vegetations or other forms of property, or which can cause any excessive soiling at any point. No emissions shall be permitted in excess of the standards established by the San Luis Obispo County Air Pollution Control District (APCD).
- h. Glare. No direct glare, whether produced by floodlight, high-temperature processes such as combustion or welding or other processes, so as to be visible from any boundary line of the property on which the same is produced shall be permitted. Sky-reflected glare from buildings or portions thereof shall be so controlled by reasonable means as are practical to the end that said sky-reflected glare will not inconvenience or annoy persons or interfere with the use and enjoyment of property in and about the area where it occurs.
- i. Liquid or Solid Wastes. No discharge shall be permitted at any point into any public sewer, private sewage disposal system or stream, or into the ground, of any materials of such nature or temperature as can contaminate any water supply, interfere with bacterial processes in sewage treatment, or otherwise cause the emission of dangerous or offensive elements, except in accord with standards approved by the California Department of Health or such other governmental agency as shall have jurisdiction over such activities. Manufacturing, processing, treatment and other activities involving use of toxic or hazardous materials shall be designed to incorporate the best available control technologies and wherever technically feasible shall employ a "closed loop" system of containment.
- j. Transportation Systems Impacts. Vehicular, bikeway and/or pedestrian traffic, directly attributable to the proposed land use, shall not increase to a significant extent without implementation of adequate mitigation measures in a form to be approved by the city engineer. In determining significance of impacts, consideration shall be given to cumulative (projected build-out) capacity of streets and highways serving the land use. Mitigation measures required may include but not be limited to curb, gutter, sidewalk,

street and/or alley, bikeway, transit related improvements and traffic signalization. Mitigation may be required as pursuant to the California Environmental Quality Act (CEQA), or as a condition of a discretionary review. (Ord. 665 N.S. § 28, 1993: (Ord. 405 N.S. § 2 (part), 1977)

PASSED AND ADOPTED THIS 17th day of March, 2009 by the following Roll Call Vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

MAYOR DUANE PICANCO

ATTEST:

CATHY DAVID, DEPUTY CITY CLERK

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL
OF THE CITY OF EL PASO DE ROBLES
TO GRANT TENTATIVE MAP APPROVAL FOR LOT LINE ADJUSTMENT 07-0293
(PR MOTORCOACH)
APN: 025-435-005, 006 & 007

WHEREAS, Tentative Lot Line Adjustment PRAL 07-0293 has been filed by North Coast Engineering on behalf of Paso 160, LLC, to subdivide an approximate 30-acre site into 3 lots and 1 remainder lot; and

WHEREAS, the 160 acre site is currently comprised of three parcels, the applicants are proposing to reconfigure the three parcels via Lot Line Adjustment 07-093, where Parcel 1 would be 58.8 acres, Parcel 2 would be 68.6 acres and Parcel 3 would be 21.4 acres; and

WHEREAS, the Planning Commission held a duly noticed public hearing on February 24, 2009 on this project to accept public testimony on PRAL 07-0293 and associated environmental review; and

WHEREAS, the Planning Commission on a 4-1 vote (1 abstention, 1 absent) recommended that the City Council approve PRAL 07-0293; and

WHEREAS, the City Council held a duly noticed public hearing on March 17, 2009 on this project to accept public testimony on PRAL 07-0293; and

WHEREAS, based on the information and analysis contained in the Initial Study, the City Council makes a determination that the proposed project will not result in significant environmental impacts and it is appropriate for the City Council to adopt a Mitigated Negative Declaration; and

WHEREAS, based upon the facts and analysis presented in the staff report, public testimony received and subject to the conditions listed below, the City Council makes the following findings as required by Government Code Sections 66474 and 65457:

Findings:

1. The proposed lot line adjustment in conjunction with PD 08-001 & CUP 08-001, would re-orient the 3 existing parcels in a manner that would accommodate the Paso Robles Motorcoach project. In conjunction with the PD & the CUP the RV Resort project would meet the intent of the General Plan and Zoning Code by providing tourist oriented, end desitination resort in the Parks and Open Space land use and zoning designations.

NOW, THEREFORE, BE IT RESOLVED, that the City Council of the City of El Paso de Robles, does hereby grant tentative map approval for PRAL 07-0293 subject to the following conditions of approval:

Attachment 11

Resolution approving LLA 07-0293
GPA 08-001, Rezone 08-001, PD 09-001
LLA 07-0293 & CUP 08-001
(PR Motorcoach)

STANDARD CONDITIONS OF APPROVAL:

1. This lot line adjustment will expire in 3 years on March 17, 2012 if a parcel map or certificates of compliance have not been recorded to finalize this approval, or if a time extension has not been requested, with appropriate fees paid, prior to the expiration date.
2. The Final Map or Certificates of Compliance shall be in substantial compliance with the attached Exhibit A. All conditions shall be complied with in a manner subject to approval of the City of El Paso de Robles.
3. A registered civil engineer or land surveyor shall prepare the lot line adjustment.
4. All new property corners shall be installed.
5. Prior to recordation of a final parcel map or certificates of compliance, the applicant shall provide on a 3.5 inch disk or IBM-compatible CD a copy of all signed and stamped approved plans, exhibits, resolutions, and all submittal materials and other documentation pertaining to approval of this application in PDF format for electronic archiving. The applicant may elect to have the City send out the documents for scanning at the applicant's expense.

PASSED AND ADOPTED THIS 17th day of March, 2009 by the following Roll Call Vote:

AYES:

NOES:

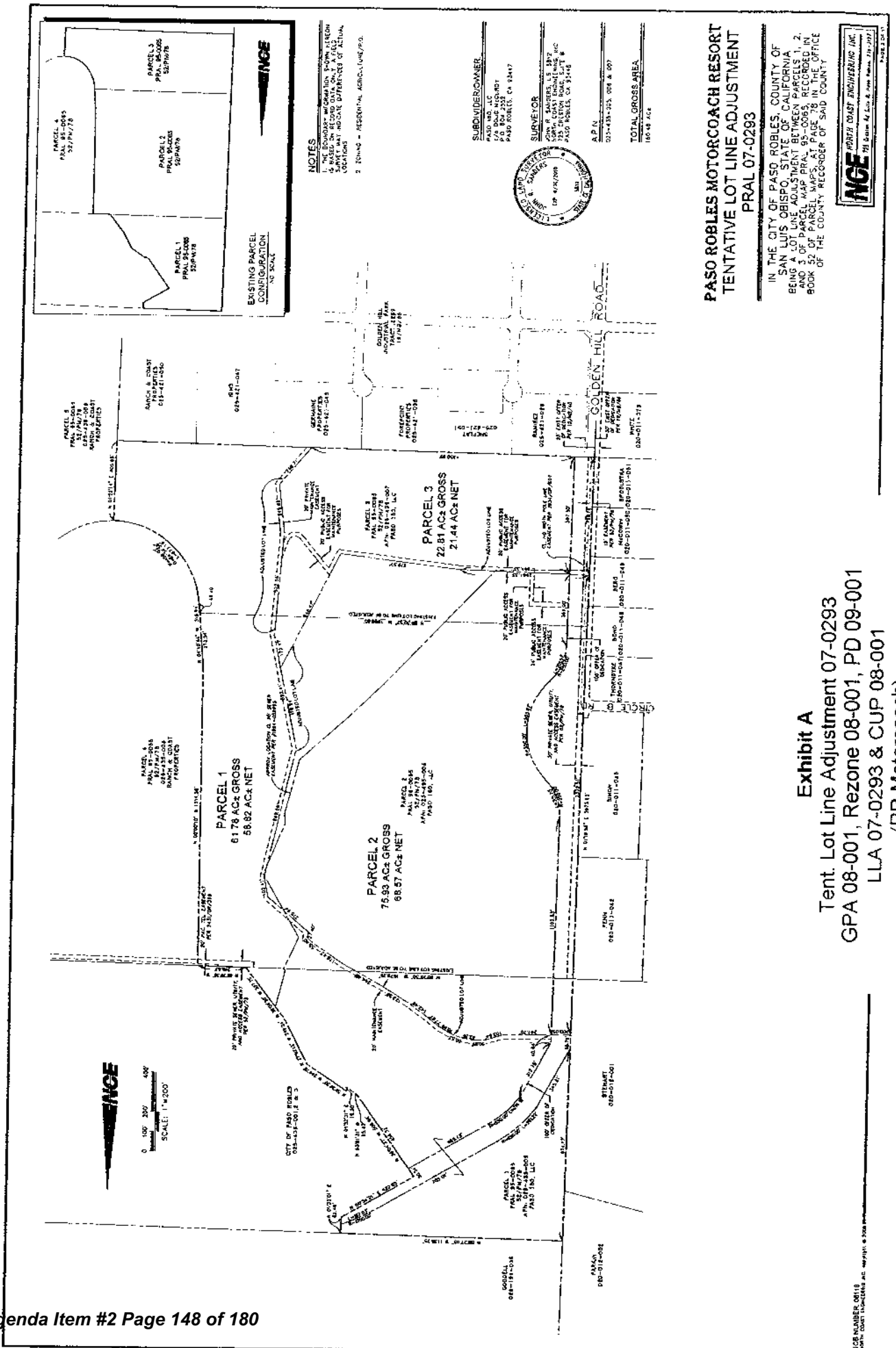
ABSENT:

ABSTAIN:

MAYOR DUANE PICANCO

ATTEST:

CATHY DAVID, DEPUTY CITY CLERK



NOTES
 1. THE BOUNDARY INFORMATION SHOWN HEREON IS BASED ON THE SURVEY DATA AND FIELD LOCATIONS. SURVEY DATA MAY INDICATE DIFFERENCES OF ACTUAL LOCATIONS.
 2. ZONING - RESIDENTIAL AGRICULTURE (RPA).

SUBDIVIDER/OWNER
 PASO ROBLES, LLC
 P.O. BOX 233
 PASO ROBLES, CA 92547

SURVEYOR
 JOHN R. SANDERS, L.S. 1812
 325 FORTY EIGHT ENGINEERING, INC.
 PASO ROBLES, CA 92546

A.P.N.
 023-433-023, 008 & 007

TOTAL GROSS AREA
 105.53 AC±



**PASO ROBLES MOTORCOACH RESORT
 TENTATIVE LOT LINE ADJUSTMENT
 PRAL 07-0293**

IN THE CITY OF PASO ROBLES, COUNTY OF SAN LUIS OBISPO, STATE OF CALIFORNIA
 BEING A TENTATIVE LOT LINE ADJUSTMENT BETWEEN PARCELS 1, 2, AND 3 OF PARCEL MAPS 08-001, PD 09-001 AND BOOK 52 OF PARCEL MAPS, AT PAGE 78, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

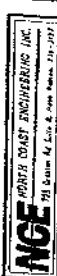


Exhibit A
 Tent. Lot Line Adjustment 07-0293
 GPA 08-001, Rezone 08-001, PD 09-001
 LLA 07-0293 & CUP 08-001
 (PR Motorcoach)

RESOLUTION NO. 09-

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF PASO ROBLES
AUTHORIZING THE REMOVAL 36 OAK TREES
(PASO ROBLES MOTORCOACH RESORT)

WHEREAS, PD 08-001 has been filed by North Coast Engineering on behalf of Paso 160, LLC to construct a 332 space RV resort on the 160-acre site located at the northern terminus of Golden Hill Road, North of Highway 46 East; and

WHEREAS, the 160-acre site is currently comprised of three parcels, the applicants are proposing to reconfigure the three parcels via Lot Line Adjustment 07-093, where Parcel 1 would be 58.8 acres, Parcel 2 would be 68.6 acres and Parcel 3 would be 21.4 acres; and

WHEREAS, PD 08-001 proposes to develop the Paso Robles Motorcoach project on Parcel 2; and

WHEREAS, an Arborist Report was prepared by A & T Arborists indicating that over 600 oak trees are located on the entire 160 acre site, approximately 336 oak trees were surveyed with the project; and

WHEREAS, an the report providing information on the characteristics and health of 336 trees; and

WHEREAS, the applicants along with the Arborist are requesting that the City Council allow the removal of 36 oak trees as a result of the development of the RV Park, including the extension of Golden Hill Road; and

WHEREAS, of the 36 trees proposed to be removed, 5 oak trees are completely dead, 9 trees need to be removed as a result of the construction of Golden Hill Road extension,

WHEREAS, according to Section 10.01.050.D, there are several factors that the City Council needs to review when considering the removal of a "healthy" oak tree. These factors along with Staff's analysis of each factor are listed below:

1. The condition of the oak tree with respect to its general health, status as a public nuisance, danger of falling, proximity to existing or proposed structures, interference with utility services, and its status as host for a plant, pest or disease endangering other species of trees or plants with infection or infestation;
A&T Arborists submitted a report along with a request for removal on January 2008. The report (Attachment 1) indicates that the trees on this site are considered old growth and as a result of not being maintained are in poor condition. As noted above 5 oaks are completely dead, most of the remaining 31 trees are in poor conditions.
2. The necessity of the requested action to allow construction of improvements or otherwise allow reasonable use of the property for the purpose for which it has been zoned. In this context, it shall be the burden of the person seeking the permit to demonstrate to the

Attachment 12

satisfaction of the director that there are no reasonable alternatives to the proposed design and use of the property. Every reasonable effort shall be made to avoid impacting oak trees, including but not limited to use of custom building design and incurring extraordinary costs to save oak trees;

The project has been designed to protect the remaining hundreds of trees. The trees proposed to be removed are in poor health and show signs of past failures. There are a few trees in good condition, these trees need to be removed as a result of the Golden Hill Road alignment. The remaining trees will be pruned and cared for to help prevent future failures.

3. The topography of land, and the potential effect of the requested tree removal on soil retention, water retention, and diversion or increased flow of surface waters. The director shall consider how either the preservation or removal of the oak tree(s) would relate to grading and drainage. Except as specifically authorized by the planning commission and city council, ravines, stream beds and other natural water-courses that provide a habitat for oak trees shall not be disturbed;

The removal of this tree would not result in negative effects on soil retention, water retention or surface water flows for the neighborhood.

4. The number, species, size and location of existing trees in the area and the effect of the requested action on shade areas, air pollution, historic values, scenic beauty and the general welfare of the city as a whole;

The remaining trees will be protected during construction and with operation of the RV resort. The remaining trees will be pruned and care for to help prevent future failures and long them health.

5. Good forestry practices such as, but not limited to, the number of healthy trees the subject parcel of land will support.

As described above, there are four other oak trees located on the site that will remain and be preserved. There would be sufficient area to plant replacement trees in the area where the subject tree is located, if allowed to be removed.

NOW, THEREFORE, BE IT RESOLVED, that the City Council of the City of El Paso de Robles does hereby:

1. Authorize the removal of thirty-six (36) Oak tree based on the January 2008 A&T Arborist Report indicating that the trees are in poor condition, or need to be removed for the construction of the Golden Hill Road extension. Except for dead trees, the trees shall be removed on a phase by phase basis;
2. Replacement trees shall be provided as required by the Oak Tree Ordinance at a ratio of 25-percent replacement of the diameter of trees removed. The 5 trees that are dead are not required to have replacement trees since the death of the trees was of natural causes.

PASSED AND ADOPTED by the City Council of the City of El Paso de Robles this 17th day of March 2009 by the following vote:

AYES:
NOES:
ABSTAIN:
ABSENT:

Duane Picanco, Mayor

ATTEST:

Dennis Fansler, Deputy City Clerk

A & T ARBORISTS

P.O. BOX 1311 TEMPLETON, CA 93465 (805) 434-0131



Tree Preservation Plan For Paso Robles Motorcoach Resort

Prepared by A & T Arborists and Vegetation Management

**Chip Tamagni
Certified Arborist #WE 6436-A**

A handwritten signature in black ink, appearing to read 'Chip Tamagni', written over the printed name and certification information.

**Steven Alvarez
Certified Arborist #WE 511-A**

A handwritten signature in black ink, appearing to read 'Steven Alvarez', written over the printed name and certification information.

PD # _____

Building Permit # _____

Exhibit A

Arborist Report

Project Description:

In section 10.01.010 section A Purpose and Intent of the Paso Robles Oak Tree Ordinance, it states:

“It is declared that the public interest and welfare requires that the city establish a program for the preservation of oak trees in order to maintain the heritage and characteristics of the City of El Paso de Robles (“The Pass of the Oaks”) as well as preserve the beauty and identity of the community”.

Preserving is defined as: protect from harm, maintain, to keep from spoiling. This project was designed with extensive effort being made to protect the healthy trees mainly through avoidance and minor encroachment into the critical root zones along with proper trimming, removal of the diseased/severely declining trees and extensively replanting new trees for the enjoyment of future generations.

The project involves three parcels totaling 160 acres located at the north end of Golden Hill Road and extending east to Huer Huero Creek in Paso Robles. Plans are to construct an upscale motor coach resort on parcel 2 with approximately 332 spaces. The property has literally hundreds of blue oaks (*Quercus douglasii*) scattered over the rolling hills along with valley oaks (*Quercus lobata*) located in the alluvial soils near the river. The area has been historically used for cattle grazing for approximately the past 70 years. This fact has resulted in very poor regeneration of new oaks. Cattle tend to compact the soil under the trees which proves difficult for regeneration. In addition, the cattle eat many of the acorns. The few seedlings that do sprout are readily consumed by the cattle and squirrels. The few young oaks on the property that have survived past the seedling stage have been repeatedly browsed from cattle and will most likely never grow to maturity with the current land use. Many studies have proven cattle grazing to be the number one cause of poor oak regeneration in California. There are many blue oaks on the property approaching 200 years old. Some are in relatively good condition considering they have never been maintained. Others have suffered major limb failures and decay has set in to the main trunks. Others have fallen over and died. Overall, the majority of the property is an aging blue oak woodland with very few trees in the 0-70 year age class.

With a property of this size, designing a project around the viable trees is of utmost importance. Instead of redesigning the project once trees have been given a condition rating, this project was purposely designed to preserve most of the higher quality trees. These trees can be trimmed thereby extending their lives by helping to prevent large failures resulting in less decay. In addition, regular maintenance and inspection will provide information to protect the oaks from insects and disease. Inversely, planning roads and buildings around trees with a short useful life expectancy is not productive long term stewardship of the land. Removing poor quality, potentially dangerous trees and replacing them with young trees is recommended. Once decay or cavities are present, trees becomes hazardous to targets under the tree (ie. building, pedestrian, roadway).

Before any conceptual plans were designed for the project, all the oak trees that were either bordering or in the middle of the project were surveyed as to location, size

and drip line. A & T Arborists visited each individual tree and established a condition rating from one to ten (described below). The results were turned over to North Coast Engineering. We recommended putting all efforts into designing around the higher conditioned trees and potentially removing (with re-planting mitigations) the lower rated trees. The current plan reflects that effort.

Approximately 336 trees were surveyed with the total number of trees on the property approaching 600. Current plans call for the removal of 24 blue oaks (five of which are dead) with a total diameter of 724 inches requiring mitigation and one live oak measuring 38 inches. Dead trees or stumps account for 141 diameter inches. Replacement requirements per section 10.01.050 F dictates 181 diameter inches of replacement blue oaks to be replanted on the site. Without intervention of cattle, the new trees will help re-establish an age class to the area which it is currently lacking.

A design concept for the Golden Hill Road extension has also been developed with the project. Extensive efforts have been made to design the road to meander the two lane arterial around as many trees as possible. Nine additional trees will have to be removed for Golden Hill Road construction. Total diameter of those trees is 247 inches, therefore requiring 62 inches of replacement trees.

The project has many spaces that have oak trees. All parking areas for motorcoaches will be constructed outside of the critical root zones. Most cut and fill grading will be outside of the critical root zones. Where cuts are necessary near trees and proper slopes cannot be maintained, retaining walls will be constructed at the edge of the critical root zones to protect the trees.

We suggest that a field trip with city council members, city staff, planning commission members, etc. would be beneficial in the review of this project. All potentially removed trees could be viewed in the field and their specific deficiencies can be discussed and observed.

Specific Mitigations Pertaining to the Project:

First, a pre-construction meeting is absolutely mandatory for this project for all contractors involved. A copy of this report should be given to all construction managers and signed. Each manager shall keep a sign-off sheet for all employees identifying that they have been trained in all applicable mitigation measures. Every member of any construction crew shall be trained. Because most of the trees are going to be protected with regard to critical root zone encroachment all standard mitigations below shall be well understood.

This project is planned to be completed in phases, therefore installing tree protection fencing for the entire project at one time is not required. Before a phase is started, fencing shall be installed at the edge of all critical root zones (or lines of encroachment) prior to any grading. Each phase of fencing shall be inspected by the project arborist and a letter to the City Planning Department shall be on file prior to issuance of grading permits.

In order to preserve the protected trees on site, Class II trimming (described below) is strongly recommended of all centrally located trees (ie. trees within individual spaces,

next to roads, etc.) We feel trimming will not only enhance the aesthetics of the project but will primarily help to insure the lifespan of the trees by lowering risk of limb failures. Pruning should commence as soon as possible.

The term "critical root zone" or CRZ is an imaginary circle around each tree. The radius of this circle (in feet) is equal to the diameter (in inches) of the tree. For example, a 10 inch diameter tree has a critical root zone with a ten foot radius from the tree. Working within the CRZ usually requires mitigations and/or monitoring by a certified arborist.

Of the few impacted trees, most have a minimal impact of between 5 and 10 percent. Impacts consist of grading for roads or trenching for retaining walls. As specified on the spreadsheet, these activities shall be monitored by the project arborist and documented on a tree by tree basis as to the activity and whether roots were encountered and properly pruned.

Some of the largest CRZ impacts will be to trees #1350, 1351, and 1352. An existing farm road is located in the path of the new road. The new road width will be 24 feet wide requiring additional cut on the uphill side of the road and fill down slope. Trees #1351 and #1352 shall require an application of plant growth regulator three to six months prior to grading. Standard monitoring and root pruning by the project arborists shall also be required. Tree #1350 has existing fill at its base from when the old farm road was cut. Some additional fill will be deposited within the critical root zone. A tree well is required if there is any potential for soil to be placed up against the trunk. The tree well can be constructed with either a gravity block wall or pressure treated wood. Tree #1283 is planned to have a 25% CRZ impact due to trenching for a retaining wall. This tree will require some additional mitigations including anti fungal treatment, insecticide treatment and plant growth regulator. On a case by case basis, the arborists may decide one or more of the above treatments may be required for additional impacted trees depending on the size and number of impacted roots during construction. Other factors including current tree health will be considered. All treatments will be documented on the city required monitoring forms and reported to the San Luis Obispo County Agricultural Commissioner on a monthly basis.

Specific Mitigations of Golden Hill Road Extension

Just south of tree #1086, a drainage pipe is planned under the road that will deliver runoff under the road near trees #1118, #1119 and #1147. This area has historically served as a natural drainage basin. Plans are to continue to utilize this natural basin with a regulated outflow during high runoff periods. There does not appear to be any substantial changes to water build up in that area. Several trees may require clearance pruning prior to road grading to avoid any branch tearing. There are some potential areas where fill will encroach into drip lines. The arborists may, in development of final plans, require gravity walls to keep soil away from the trunks of saved trees. There will be a series of retaining walls constructed near trees #1192, #1207, and #1208. All three of these trees will require arborist monitoring for root pruning and chemical mitigations to reduce stress on those trees.

All trees potentially impacted by this project are numbered and identified on the grading plan, in the field with an aluminum tag, and the spreadsheet. Tree protection fencing is shown on the grading plan. In the field, oak trees to be removed have red tape attached to the tag. Both critical root zones and approximate drip lines are outlined on the plans.

If pruning is necessary for building, road, or driveway clearance, removal of limbs larger than 6 inches in diameter will require a city approved permit along with a deposit paid in advance (to the City of Paso Robles). The city will send out a representative to approve or deny the permit. Only 25% of the live crown may be removed.

Tree Rating System

A rating system of 1-10 was used for visually establishing the general health and condition of each tree on the spreadsheet. The rating system is defined as follows:

<u>Rating</u>	<u>Condition</u>
0	Deceased
1	Evidence of massive past failures, extreme disease and is in severe decline.
2	Evidence of multiple past failures, cavities, disease and/or insect infestations. Overall reflects trees in decline.
3	Some past failures, some pests or structural defects that may be mitigated by class IV pruning.
4	May have had minor past failures, excessive deadwood or minor structural defects that can be mitigated with Class II, Class I pruning.
5	Relatively healthy tree with little visual, structural and/or pest defects and problems. Maintenance would some Class I pruning.
6	Healthy tree that probably can be left in its natural state with some minor dead wood removal and Class I pruning.
7-9	Has had proper arboricultural pruning and attention or have no apparent structural defects.
10	Specimen tree with perfect shape, structure and foliage in a protected setting (i.e. park, arboretum).

Aesthetic quality on the spreadsheet is defined as follows:

- **poor** - tree has little visual quality either due to severe suppression from other trees, past pruning practices, location or sparse foliage
- **fair** - visual quality has been jeopardized by utility pruning/obstructions or partial suppression and overall symmetry is average
- **good** - tree has good structure and symmetry either naturally or from prior pruning events and is located in an area that benefits from the trees position
- **excellent** - tree has great structure, symmetry and foliage and is located in a premier location. Tree is not over mature.

Standard Mitigation Measures:

The following standard mitigation measures/methods must be fully understood and followed by anyone working within the critical root zone of any native tree. Any necessary clarification will be provided by us (the arborists) upon request.

It is the responsibility of the **owner or project manager** to provide a copy of this tree protection plan to any and all contractors and subcontractors that work within the critical root zone of any native tree and confirm they are trained in maintaining fencing, protecting root zones and conforming to all tree protection goals. It is highly recommended that each contractor sign and acknowledge this tree protection plan.

Any future changes (within the critical root zone) in the project will need Project Arborist review and implementation of potential mitigation measures before any said changes can proceed.

Airspading: Airspading involves using highly compressed pushed through a small nozzle to dislodge and remove soil from around roots without damaging them. Airspading is primarily used for utility trenching within the critical root zone. Soil is blown out of the trench then conduit is routed under the exposed roots and re-buried leaving the roots intact. Airspading is also used during excavation to clean off roots for proper pruning. Airspading can be used for exploratory trenching to determine whether or not roots are in the vicinity for future design options.

Arborist Monitoring: An arborist shall be present for selected activities (trees identified on spreadsheet and items bulleted below). The monitoring does not necessarily have to be continuous but observational at times during these activities. It is the responsibility of the **owner(s) or their designee** to inform us prior to these events so we can make arrangements to be present. All monitoring will be documented on the field report form which will be forwarded to the project manager and the City of Paso Robles Planning Department. This is a large project requiring extensive grading activities. The project arborists may require a full time monitor during certain periods of construction if the volume dictates it. At a minimum, the following will be required:

- pre-construction fence placement inspection for each phase
- all grading and trenching identified on the spreadsheet
- a minimum of three days per week during construction phases
- any other encroachment the arborist feels necessary

Chip Mulch: All areas within the critical root zone of the trees that can be fenced shall receive a 4-6" layer of chip mulch to retain moisture, soil structure and reduce the effects of soil compaction.

Construction Materials And Waste: No liquid or solid construction waste shall be dumped on the ground within the critical root zone of any native tree. The critical root zone areas are not for storage of materials either.

Equipment Operation: Vehicles and all heavy equipment shall not be driven under the trees, as this will contribute to soil compaction. Also there is to be no parking of equipment or personal vehicles in these areas. All areas behind fencing are off limits unless pre-approved by the arborist.

Existing Surfaces: The existing ground surface within the critical root zone of all oak trees shall not be cut, filled, compacted or pared, unless shown on the grading plans **and** approved by the arborist.

Exposed Roots: Any exposed roots shall be re-covered the same day they were exposed. If they cannot, they must be covered with burlap or another suitable material and wetted down 2x per day until re-buried.

Fencing: The proposed fencing shall be shown in orange ink on the grading plan. It must be a minimum of 4' high chain link, snow or safety fence staked (with t posts 8 feet on center) at the edge of the critical root zone or line of encroachment for each tree or group of trees. The fence shall be up before any construction or earth moving begins for each phase. The owner shall be responsible for maintaining an erect fence throughout the construction period. The arborist(s), upon notification, will inspect the fence placement once it is erected. After this time, fencing shall not be moved without arborist inspection/approval. If the orange plastic fencing is used, a minimum of four zip ties shall be used on each stake to secure the fence. All efforts shall be made to maximize the distance from each saved tree. Weather proof signs shall be permanently posted on the fences every 50 feet, with the following information:

Tree Protection Zone
No personnel, equipment,
materials, and vehicles are
allowed
Do not remove or re-position
this fence without calling:
A & T Arborists
434-0131

Fertilization and Cultural Practices: As the project moves toward completion, the arborist(s) may suggest either fertilization and/or mycorrhiza applications that will benefit tree health. Mycorrhiza offers several benefits to the host plant, including faster growth, improved nutrition, greater drought resistance, and protection from pathogens.

Grading Within The Critical Root Zone: Grading should not encroach within the critical root zone unless authorized. Grading should not disrupt the normal drainage pattern around the trees. Fills should not create a ponding condition and excavations should not leave the tree on a rapidly draining mound.

Landscape: All landscape within the critical root zone shall consist of drought tolerant or native varieties. Lawns shall be avoided. All irrigation trenching shall be routed around critical root zones, otherwise above ground drip-irrigation shall be used. It is the owner's responsibility to notify the landscape contractor regarding this mitigation.

Paving Within The Critical Root Zone: Pervious surfacing is preferred within the critical root zone of any oak tree with more than 35% CRZ encroachment. If pavers are required, the areas are outlined on the grading plans. Areas that require a cut deeper than 24 inches are not normally candidates for pavers as the roots are usually pruned, therefore there are none to protect. Areas that require minimal grading (<6 inches) or fill >35% of the CRZ are more suitable. Pavers must be interlocking with a minimum of 10% void space backfilled with pea gravel. Geo textile fabric shall be permeable. Depending on use within the CRZ, pavers may or may not be required. Vertical perforated tubing with drainage grates may be required instead. Unless changes are made during the construction phase, the arborists feel there will be no pavers required at this time.

Pre-Construction Meeting: An on-site pre-construction meeting with the Arborist(s), Owner(s), Planning Staff, and the earth moving team shall be required for this project. Prior to final occupancy, a letter from the arborist(s) shall be required verifying the health/condition of all impacted trees and providing any recommendations for any additional mitigation. The letter shall verify that the arborist(s) were on site for all grading and/or trenching activity that encroached into the critical root zone of the selected native trees, and that all work done in these areas was completed to the standards set forth above.

Pruning Class 4 pruning includes-Crown reduction pruning shall consist of reduction of tops, sides or individual limbs. A trained arborist shall perform all pruning. No pruning shall take more than 25% of the live crown of any native tree. Any trees that may need pruning for road/home clearance shall be pruned **prior** to any grading activities to avoid any branch tearing.

Soil Aeration Methods: Soils within the critical root zone that have been compacted by heavy equipment and/or construction activities must be returned to their original state before all work is completed. Methods include water jetting, adding organic matter, and boring small holes with an auger (18" deep, 2-3' apart with a 2-4" auger) and the application of moderate amounts of nitrogen fertilizer. The arborist(s) shall advise.

Trenching Within Critical Root Zone: All trenching within the critical root zone of native trees shall be **hand dug**. All major roots shall be avoided whenever possible. All exposed roots larger than 1" in diameter shall be clean cut with sharp pruning tools and not left ragged. A **Mandatory** meeting between the arborists and grading contractor(s) must take place prior to work start.

Utility Placement: All utilities, sewer and storm drains shall be placed down the roads and driveways and when possible outside of the critical root zones. The arborist shall supervise trenching within the critical root zone. **All trenches in these areas shall be exposed by air spade or hand dug with utilities routed under/over roots larger than 3 inches in diameter.**

The attached spreadsheet includes trees listed by number, species and multiple stems if applicable, scientific name, diameter at breast height (4.5'), condition (scale from poor to excellent), status (avoided, impacted, removed, exempt), percent of critical root zone

impacted, mitigation required (fencing, root pruning, monitoring), construction impact (trenching, grading), recommended pruning, aesthetic value and individual tree notes along with canopy spread.

If all the above mitigation measures are followed, we feel there will be no long-term significant impacts to the remaining oak trees.

Please let us know if we can be of any future assistance to you for this project.

Steven G. Alvarez
Certified Arborist #WC 0511


Chip Tamagni

Certified Arborist #WE 6436-A



TREE PROTECTION SPREAD SHEET GOLDEN HILL 1

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	
1046	VO	Q. lobata	36	2	A	0%	NONE	fencing	NO	II	good	nesting holes, deadwood	NS
1001	BO	Q. doug.	46	1	A	0%	NONE	fencing	NO		poor	massive failures	AS
1004	BO	Q. doug.	42	2	A	0%	NONE	fencing	NO		fair	past failures	AS
1005	BO	Q. doug.	28	7	A	0%	NONE	fencing	NO	II	excl.	to be in vineyard	AS
1006	BO	Q. doug.	36	2	A	0%	NONE	fencing	NO		fair	massive failures	AS
1008	BO	Q. doug.	38	3	A	0%	NONE	fencing	NO		fair	past failures, mistletoe	AS
1007	BO	Q. doug.	34	2	A	0%	NONE	fencing	NO	II	fair	new and old failures	AS
1033	BO	Q. doug.	34	4	A	0%	NONE	fencing	NO		good		AS
1032	BO	Q. doug.	6	3	A	0%	NONE	fencing	NO		fair	suppressed	AS
1031	BO	Q. doug.	13	6	A	0%	NONE	fencing	NO		good		AS
1030	BO	Q. doug.	26	4	A	0%	NONE	fencing	NO		good	decay at base	AS
1029	BO	Q. doug.	23	4	A	0%	NONE	fencing	NO		good	mistletoe, v crotch	AS
1028	BO	Q. doug.	17	3	A	0%	NONE	fencing	NO		good	bad crotch	AS
1027	BO	Q. doug.	15	5	A	0%	NONE	fencing	NO		good	mistletoe	AS
1026	BO	Q. doug.	15	4	A	0%	NONE	fencing	NO		fair	deadwood	AS
1025	BO	Q. doug.	6	5	A	0%	NONE	fencing	NO		good		AS
1023	BO	Q. doug.	8	2	A	0%	NONE	fencing	NO		poor	mistletoe	AS
1024	BO	Q. doug.	9	3	A	0%	NONE	fencing	NO		fair	mistletoe	AS
1022	BO	Q. doug.	16	4	A	0%	NONE	fencing	NO		good	deadwood	AS
1021	BO	Q. doug.	16	4	A	0%	NONE	fencing	NO		good		AS

8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING,
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PRESCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 12 = FIELD NOTES
 13 = NORTH SOUTH/EAST WEST CANOPY SPREAD IS AS SHOWN TO SCALE ON PLANS

TREE PROTECTION SPREAD SHEET GOLDEN HILL 2

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1020	BO	Q. doug.	14	6	A	0%	NONE	fencing	NO		good		AS
1019	BO	Q. doug.	12	5	A	0%	NONE	fencing	NO		good	canker on trunk	AS
1018	BO	Q. doug.	24	5	A	0%	NONE	fencing	NO		good	deadwood	AS
1015	BO	Q. doug.	4	2	A	0%	NONE	fencing	NO		poor	suppressed, mistletoe	AS
1016	BO	Q. doug.	9	1	A	0%	NONE	fencing	NO		poor	highly suppressed, mistl.	AS
1017	BO	Q. doug.	15	2	A	0%	NONE	fencing	NO		fair	mistletoe, bad crotch	AS
1014	BO	Q. doug.	14	3	A	0%	NONE	fencing	NO		fair	mistletoe	AS
1009	BO	Q. doug.	40	2	A	0%	NONE	fencing	NO		fair	cavities, failures	AS
1002	BO	Q. doug.	22	4	A	0%	NONE	fencing	NO		good	ground squirrels	AS
1003	BO	Q. doug.	24	3	A	0%	NONE	fencing	NO		good	stressed	AS
1010	BO	Q. doug.	32	2	A	0%	NONE	fencing	NO		excel.	massive failure, cavity	AS
1011	BO	Q. doug.	27	3	A	0%	NONE	fencing	NO		fair	dieback	AS
1013	BO	Q. doug.	28	3	A	0%	NONE	fencing	NO		good	cavities, failures	AS
1012	BO	Q. doug.	47	3	A	0%	NONE	fencing	NO		excel.	cavities, failures	AS
1034	BO	Q. doug.	33	4	A	0%	NONE	fencing	NO		excel.	past failures	AS
1035	BO	Q. doug.	27	5	A	0%	NONE	fencing	NO		excel.		AS
1036	BO	Q. doug.	40	2	R	100%	GR	NONE	NO		good	conks, nesting holes	AS
1131	BO	Q. doug.	10	5	A	0%	NONE	fencing	NO		excel.		AS
1133	BO	Q. doug.	25	5	A	0%	NONE	fencing	NO		good		AS
1132	BO	Q. doug.	37	4	A	0%	NONE	fencing	NO		fair	mistletoe	AS

1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 2 = TREE TYPE: COMMON NAME IE: W.O. = WHITE OAK
 3 = SCIENTIFIC NAME
 4 = TRUNK DIAMETER @ 46"
 5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING.
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PERSCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/EAST WEST CANOPY SPREAD

TREE PROTECTION SPREAD SHEET GOLDEN HILL 3

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1130	BO	Q. doug	37	4	A	0%	NONE	fencing	NO		good	cavity	AS
1129	BO	Q. doug	20	4	A	0%	NONE	fencing	NO		good	none	AS
1128	BO	Q. doug	12	4	A	0%	NONE	fencing	NO		fair	suppressed	AS
1039	BO	Q. doug	23	4	A	0%	NONE	fencing	NO		good	undermined roots	AS
1038	BO	Q. doug	12	2	A	0%	NONE	fencing	NO		poor	mostly suppressed	AS
1037	BO	Q. doug	23	3	A	0%	NONE	fencing	NO	II	excel.	cavity	AS
1044	BO	Q. doug	32	6	A	0%	NONE	fencing	NO	II	excel.	none	AS
1043	BO	Q. doug	35	4	A	0%	NONE	fencing	NO	II	excel.	40% lean south	AS
1042	BO	Q. doug	25	5	A	0%	NONE	fencing	NO	II	excel.	none	AS
1041	BO	Q. doug	20	3	A	0%	NONE	fencing	NO	II	good	major cavity	AS
1040	BO	Q. doug	21	5	A	0%	NONE	fencing	NO		good	small deadwood	AS
1063	BO	Q. doug	24	5	A	0%	NONE	fencing	NO		good	suppressed	AS
1064	BO	Q. doug	35	4	A	0%	NONE	fencing	NO		fair	past failures	AS
1065	BO	Q. doug	10	4	A	0%	NONE	fencing	NO		fair	major deadwood	AS
1066	BO	Q. doug	33	3	A	0%	NONE	fencing	NO	II	good	nesting holes past fails	AS
1068	BO	Q. doug	28	5	A	0%	NONE	fencing	NO	II	good	good structure	AS
1067	BO	Q. doug	33	3	I	10%	TR	F,RP,M	YES	II	fair	stressed appearance	AS
1062	BO	Q. doug	27	5	A	0%	NONE	fencing	NO	II	excel.	massive past failure	AS
1070	BO	Q. doug	30	2	A	0%	NONE	fencing	NO		fair	none	AS
1081	BO	Q. doug	30	3	A	0%	NONE	fencing	NO		good	past failures	AS

8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING,
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PRESCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/ EAST WEST CANOPY SPREAD

#1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 #2 = TREE TYPE: COMMON NAME IE. W.O. = WHITE OAK
 #3 = SCIENTIFIC NAME
 #4 = TRUNK DIAMETER @ 4"
 #5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 #6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 #7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE

TREE PROTECTION SPREAD SHEET GOLDEN HILL 4

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1072	BO	Q. doug.	40	2	I	10%	GR	F,RP,M	YES	II	fair	past failure, cavity	AS
1073	BO	Q. doug.	19	4	A	0%	NONE	fencing	NO		fair		AS
1069	BO	Q. doug.	20	5	A	0%	NONE	fencing	NO		good		AS
1306	BO	Q. doug.	55	5	A	0%	NONE	fencing	NO		excel.	cavity	AS
1307	BO	Q. doug.	31	3	A	0%	NONE	fencing	NO		good	past failure	AS
1305	BO	Q. doug.	32	2	I	10%	GR	F,RP,M	YES		fair	past failure	AS
1333	BO	Q. doug.	36	4	A	0%	NONE	fencing	NO		good	nesting holes, cankers	AS
1308	BO	Q. doug.	26	5	A	0%	NONE	fencing	NO		good	v crotch	AS
1332	BO	Q. doug.	17	6	A	0%	NONE	fencing	NO		good		AS
1304	BO	Q. doug.	41	3	R	100%	GR	NONE	NO		good	nesting holes	AS
1309	BO	Q. doug.	28	3	A	0%	NONE	fencing	NO		good	nesting holes	AS
1300	BO	Q. doug.	26	6	A	0%	NONE	fencing	NO	II	excel.		AS
1301	BO	Q. doug.	32	2	R	100%	GR	NONE	NO		good	soon to fail, cavity	AS
1302	BO	Q. doug.	38	5	A	0%	NONE	fencing	NO		good		AS
1303	BO	Q. doug.	25	4	A	0%	NONE	fencing	NO		good	suppressed	AS
1310	BO	Q. doug.	36	5	A	0%	NONE	fencing	NO		good		AS
1311	BO	Q. doug.	14	3	A	0%	NONE	fencing	NO		fair	mistletoe	AS
1312	BO	Q. doug.	14	4	A	0%	NONE	fencing	NO		good	mistletoe	AS
1330	BO	Q. doug.	34	7	I	<5%	GR	fencing	NO		excel.		AS
1075	BO	Q. doug.	23	4	A	0%	NONE	fencing	NO		good	dieback	AS

1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 2 = TREE TYPE: COMMON NAME (E.W.O.) = WHITE OAK
 3 = SCIENTIFIC NAME
 4 = TRUNK DIAMETER @ 4'
 5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING.
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PRUNING CLASS: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/EAST WEST CANOPY SPREAD

TREE PROTECTION SPREAD SHEET GOLDEN HILL 5

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1074	BO	Q. doug	25	4	A	0%	NONE	fencing	NO		good	SUPPRESSED	AS
1076	BO	Q. doug	35	4	A	0%	NONE	fencing	NO		good	CAVITY, FAILURES	AS
1078	BO	Q. doug	24	4	A	0%	NONE	fencing	NO	II	excel.	PAST FAILURES	AS
1077	BO	Q. doug	47	6	I	10%	GR	F,RP,M	YES	II	excel.	HEAVY END WEIGHT	AS
1079	BO	Q. doug	34	2	R	100%	GR	NONE	NO		fair	PAST FAILURES, CAVITY	AS
1080	BO	Q. doug	31	3	I	5%	GR	F,RP,M	YES	II	fair	PAST FAILURES, CAVITY	AS
1082	BO	Q. doug	28	2	R	100%	GR	NONE	NO		fair	MAJ LIMB LOSS LEAN	AS
1081	BO	Q. doug	24	5	A	0%	NONE	fencing	NO	II	excel.	10% LEAN EAST	AS
1083	BO	Q. doug	34	2	R	100%	GR	NONE	NO		fair	MAJ LIMB LOSS LEAN	AS
1084	BO	Q. doug	35	2	R	100%	GR	NONE	NO		fair	MAJ LIMB LOSS LEAN	AS
1057	BO	Q. doug	42	1	R	100%	GR	NONE	NO		poor	STAGE HEAD	AS
1056	BO	Q. doug	27	0	R	100%	GR	NONE	NO		dead	DEAD TREE	AS
1055	BO	Q. doug	30	3	I	5%	GR	F,RP,M	YES	II	fair	TWIG DIE BACK	AS
1047	BO	Q. doug	22	4	A	0%	NONE	fencing	NO	II	good	HEAVY MIS. TOE	AS
1050	BO	Q. doug	24	3	I	10%	GR	F,RP,M	YES	II	fair	15% LEAN SOUTH	AS
1049	BO	Q. doug	28	4	A	0%	NONE	fencing	NO	II	fair	MIS. TOE M/L/L	AS
1048	BO	Q. doug	50	1	R	100%	GR	NONE	NO		poor	MAJ CRACK @ TRUNK	AS
1045	BO	Q. doug	30	5	A	0%	NONE	fencing	NO	II	excel.	10% LEAN SOUTH	AS
1051	BO	Q. doug	35	2	A	0%	NONE	fencing	NO	II	poor	50% TREE BROKE OFF	AS
1053	BO	Q. doug	29	4	A	0%	NONE	fencing	NO	II	good	LARGE BEE.H IN CAVITY	AS

8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING.
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PERSCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/EAST WEST CANOPY SPREAD

#1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 #2 = TREE TYPE: COMMON NAME (E.W.O. = WHITE OAK)
 #3 = SCIENTIFIC NAME
 #4 = TRUNK DIAMETER @ 4'
 #5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 #6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 #7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE

TREE PROTECTION SPREAD SHEET GOLDEN HILL 6

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1054	BO	Q. doug.	27	2	A	0%	NONE	fencing	NO		fair	nesting holes, cavity	AS
1052	BO	Q. doug.	29	3	A	0%	NONE	fencing	NO	II	fair	suppressed by 1053	AS
1060	BO	Q. doug.	40	2	R	40%	GR	NONE	NO		poor	cavity at base, limb loss	AS
1061	BO	Q. doug.	28	4	I	5%	GR	F,RP,M	YES	II	fair	good structure	AS
1058	BO	Q. doug.	22	3	A	0%	NONE	fencing	NO	II	fair	mistletoe, cavities	AS
1059	BO	Q. doug.	35	4	I	5%	GR	F,RP,M	NO	II	good	mistletoe, good structure	AS
1115	BO	Q. doug.	24	4	A	0%	NONE	fencing	NO	II	good	twig dieback	AS
1116	BO	Q. doug.	29	5	A	0%	NONE	fencing	NO	II	excel.	good structure	AS
1127	BO	Q. doug.	28	1	A	0%	NONE	fencing	NO		poor	major limb loss	AS
1126	BO	Q. doug.	26	1	A	0%	NONE	fencing	NO		poor	major limb loss	AS
1117	BO	Q. doug.	32	5	A	0%	NONE	fencing	NO	II	excel.	good structure	AS
1125	BO	Q. doug.	28	3	A	0%	NONE	fencing	NO		fair	utility pruned	AS
1124	BO	Q. doug.	22	3	A	0%	NONE	fencing	NO		fair	suppressed by 1124	AS
1123	BO	Q. doug.	27	4	A	0%	NONE	fencing	NO		good	mistletoe	AS
1122	BO	Q. doug.	24	4	A	0%	NONE	fencing	NO		good	wire at base	AS
1113	BO	Q. doug.	19	4	A	0%	NONE	fencing	NO	II	good	twig dieback	AS
1112	BO	Q. doug.	19	4	A	0%	NONE	fencing	NO	II	good	mistletoe	AS
1111	BO	Q. doug.	29	3	A	0%	NONE	fencing	NO	II	fair	major limb loss	AS
1110	BO	Q. doug.	26	3	I	15%	GR	F,RP,M	YES	II	fair	major limb loss	AS
1104	BO	Q. doug.	37	2	R	25%	TR	NONE	NO		poor	major limb loss	AS

1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 2 = TREE TYPE: COMMON NAME (E.V.O.) = WHITE OAK
 3 = SCIENTIFIC NAME
 4 = TRUNK DIAMETER @ 4ft
 5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING,
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PERSCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/ EAST WEST CANOPY SPREAD

TREE PROTECTION SPREAD SHEET GOLDEN HILL 7

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1090	BO	Q. doug.	27	4	A	0%	NONE	fencing	NO	II	fair	lean 10 deg. South	AS
1085	BO	Q. doug.	44	1	R	0%	hazard	NONE	NO		poor	major limb loss	AS
1086	BO	Q. doug.	20	2	I	15%	GR	F,R,P,M	YES		fair	mistletoe, dieback	AS
1089	BO	Q. doug.	20	5	A	0%	NONE	fencing	NO	II	excel.	good structure	AS
1087	BO	Q. doug.	27	1	R	100%	GR	NONE	NO		poor	major limb loss	AS
1118	BO	Q. doug.	25	4	A	0%	NONE	fencing	NO	II	good	twig dieback	AS
1114	BO	Q. doug.	20	2	A	0%	NONE	fencing	NO	II	fair	mistletoe, bad structure	AS
1119	BO	Q. doug.	29	5	A	0%	NONE	fencing	NO		excel.	good structure	AS
1147	BO	Q. doug.	29	5	A	0%	NONE	fencing	NO		excel.	healthy, good structure	AS
1121	BO	Q. doug.	14	5	A	0%	NONE	fencing	NO		excel.	road side	AS
1120	BO	Q. doug.	15	3	A	0%	NONE	fencing	NO		fair	wire @ base	AS
1146	BO	Q. doug.	15	3	A	0%	NONE	fencing	NO		fair	wire @ base	AS
1145	BO	Q. doug.	18	4	A	0%	NONE	fencing	NO		good	good structure	AS
1148	BO	Q. doug.	17	4	A	0%	NONE	fencing	NO	II	good	good structure	AS
1149	BO	Q. doug.	25	4	R	100%	GR	NONE	NO		excel.	good structure	AS
1088	BO	Q. doug.	24	4	I	10%	GR	F,R,P,M	YES	II	excel.	good structure	AS
1094	BO	Q. doug.	27	2	R	100%	GR	NONE	NO		poor	nesting holes, bee hive	AS
1095	BO	Q. doug.	37	2	R	100%	GR	NONE	NO		fair	bad structure	AS
1102	BO	Q. doug.	28	2	R	100%	GR	NONE	NO		good	windswept north, cavity	AS
1102	BO	Q. doug.	32	4	A	0%	NONE	fencing	NO	II	good	good structure	AS

8 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 9 = TREE TYPE: COMMON NAME IE. W.O. = WHITE OAK
 10 = SCIENTIFIC NAME
 11 = TRUNK DIAMETER @ 4'6"
 12 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 13 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 14 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 9 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING.
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PRESCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = NORTH SOUTH EAST WEST CANOPY SPREAD

TREE PROTECTION SPREAD SHEET GOLDEN HILL 8

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1313	BO	Q. doug.	46	4	A	0%	NONE	fencing	NO	II	excel.	cavities, past failures	AS
1317	BO	Q. doug.	7	3	A	0%	NONE	fencing	NO		good	mistletoe	AS
1316	BO	Q. doug.	5	4	A	0%	NONE	fencing	NO		good		AS
1314	BO	Q. doug.	8	4	A	0%	NONE	fencing	NO		good		AS
1315	BO	Q. doug.	10	4	A	0%	NONE	fencing	NO		good	mistletoe	AS
1322	BO	Q. doug.	29	4	I	10%	TR	F,RP,M	YES	II	good	past failures	AS
1323	BO	Q. doug.	30	4	I	10%	TR	F,RP,M	YES	II	excel.	poor crotches	AS
1299	BO	Q. doug.	23	5	I	10%	TR	F,RP,M	YES	II	good	deadwood	AS
1105	BO	Q. doug.	39	5	A	0%	NONE	fencing	NO	II	excel.		AS
1106	BO	Q. doug.	31	5	I	5%	TR	F,RP,M	YES	II	excel.	deadwood	AS
1107	BO	Q. doug.	28	4	I	5%	TR	F,RP,M	YES	II	good	stressed	AS
1298	BO	Q. doug.	51	4	A	0%	NONE	fencing	NO	II	excel.	past failures	AS
1294	BO	Q. doug.	25	4	A	0%	NONE	fencing	NO	II	good		AS
1324	BO	Q. doug.	29	4	I	5%	GR	F,RP,M	YES	II	good	past failures	AS
1293	BO	Q. doug.	38	4	A	0%	NONE	fencing	NO	II	good	past failures	AS
1281	BO	Q. doug.	31	4	I	5%	GR	F,RP,M	YES	II	good	cavity, splits	AS
1280	BO	Q. doug.	34	5	A	0%	NONE	fencing	NO		good		AS
1282	BO	Q. doug.	25	5	A	0%	NONE	fencing	NO		good		AS
1284	BO	Q. doug.	31	4	I	5%	GR	F,RP,M	YES		good	past failures	AS
1325	BO	Q. doug.	11	6	A	0%	NONE	fencing	NO		good		AS

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 2 = TREE TYPE: COMMON NAME (E.W.O. = WHITE OAK)
 3 = SCIENTIFIC NAME
 4 = TRUNK DIAMETER @ 4'
 5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING,
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PERSCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/EAST WEST CANOPY SPREAD

12/23/2008

TREE PROTECTION SPREAD SHEET GOLDEN HILL 9

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1321	BO	Q. doug.	15	6	A	0%	NONE	fencing	NO		good		AS
1320	BO	Q. doug.	46	3	A	0%	NONE	fencing	NO		good	deadwood	AS
1319	BO	Q. doug.	36	2	A	0%	NONE	fencing	NO		good	past failure, cavity	AS
1283	BO	Q. doug.	42	6	I	25%	GR	F,RP,M	YES	IV	excel.		AS
1285	BO	Q. doug.	24	4	I	5%	GR	F,RP,M	YES	II	good	cavity	AS
1278	BO	Q. doug.	38	0	R	0%	NONE	NONE	NO		dead	dead stump	AS
1277	BO	Q. doug.	29	4	A	0%	NONE	fencing	NO	II	good	cavity	AS
1219	BO	Q. doug.	23	6	A	0%	NONE	fencing	NO		excel.		AS
1222	BO	Q. doug.	27	2	R	0%	hazard	NONE	NO		fair	massive cavity	AS
1223	BO	Q. doug.	25	6	I	5%	GR	F,RP,M	YES	II	excel.		AS
1221	BO	Q. doug.	30	4	A	0%	NONE	fencing	NO	II	fair	stressed	AS
1220	BO	Q. doug.	33	2	A	0%	NONE	fencing	NO		fair	cavity, nesting holes	AS
1165	BO	Q. doug.	24	2	A	0%	NONE	fencing	NO		fair	cavity, past failures	AS
1166	BO	Q. doug.	28	5	A	0%	NONE	fencing	NO		good		AS
1164	BO	Q. doug.	29	5	A	0%	NONE	fencing	NO		excel.	mistletoe	AS
1163	BO	Q. doug.	38	2	A	0%	NONE	fencing	NO		fair	past failure	AS
1279	BO	Q. doug.	37	2	A	0%	NONE	fencing	NO		poor	past failure	AS
1295	BO	Q. doug.	26	5	A	0%	NONE	fencing	NO		excel.		AS
1296	BO	Q. doug.	21	5	A	0%	NONE	fencing	NO	II	excel.		AS
1158	BO	Q. doug.	37	3	A	0%	NONE	fencing	NO	II	fair	past failure	AS

8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING.
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PERSCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/ EAST WEST CANOPY SPREAD

#2 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 #3 = TREE TYPE: COMMON NAME (E.W.O.) = WHITE OAK
 #4 = SCIENTIFIC NAME
 #5 = TRUNK DIAMETER @ 4"
 #6 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 #7 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 #8 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE

TREE PROTECTION SPREAD SHEET GOLDEN HILL 10

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1157	BO	Q. doug.	27	3	A	0%	NONE	fencing	NO	II	fair	failures, cavity	AS
1098	BO	Q. doug.	28	0	R	100%	GR	NONE	NO		dead	dead	AS
1097	BO	Q. doug.	37	2	R	100%	GR	NONE	NO		fair	cavity, nesting holes	AS
1100	BO	Q. doug.	16	3	A	0%	NONE	fencing	NO		fair	stressed	AS
1099	BO	Q. doug.	31	6	A	0%	NONE	fencing	NO	II	excel.		AS
1167	BO	Q. doug.	26	2	A	0%	NONE	fencing	NO		fair	cavity, past failures	AS
1168	BO	Q. doug.	30	3	A	0%	NONE	fencing	NO		good	cavity at base	AS
1169	BO	Q. doug.	20	4	A	0%	NONE	fencing	NO		good	stressed	AS
1177	BO	Q. doug.	22	4	A	0%	NONE	fencing	NO		good	mistletoe	AS
1171	BO	Q. doug.	28	2	A	0%	NONE	fencing	NO		fair	cavity, failures	AS
1172	BO	Q. doug.	25	3	A	0%	NONE	fencing	NO		fair	mistletoe, stressed	AS
1173	BO	Q. doug.	20	4	A	0%	NONE	fencing	NO		good		AS
1176	BO	Q. doug.	18	3	A	0%	NONE	fencing	NO		fair	stressed	AS
1175	BO	Q. doug.	28	4	A	0%	NONE	fencing	NO		fair	suppressed	AS
1174	BO	Q. doug.	21	3	A	0%	NONE	fencing	NO		good	nesting holes, cavity	AS
1213	BO	Q. doug.	18	6	A	0%	NONE	fencing	NO		good		AS
1226	BO	Q. doug.	33	5	A	0%	NONE	fencing	NO		excel.	mistletoe	AS
1224	BO	Q. doug.	34	5	A	0%	NONE	fencing	NO	II	excel.	excessive end weight	AS
1225	VO	Q. lobata	38	2	R	100%	GR	NONE	NO		fair	massive failure	AS
1214	VO	Q. lobata	27	4	A	0%	NONE	fencing	NO		good		AS

1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 2 = TREE TYPE: COMMON NAME (E,W,O) = WHITE OAK
 3 = SCIENTIFIC NAME
 4 = TRUNK DIAMETER @ 45"
 5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING,
 10 = ARBORIST MONITORING REQUIRED, YES/NO
 11 = PRESCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/EAST WEST CANOPY SPREAD

TREE PROTECTION SPREAD SHEET GOLDEN HILL 11

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1215	BO	Q. doug.	25	2	A	0%	NONE	fencing	NO		good	soon to fail, cavity	AS
1216	BO	Q. doug.	33	3	I	30%	GR	F,RP,M	YES		excel.	decay, nesting holes	AS
1286	BO	Q. doug.	40	5	A	0%	NONE	fencing	NO		excel.		AS
1289	VO	Q. lobata	24	4	A	0%	NONE	fencing	NO		good	suppressed	AS
1288	VO	Q. lobata	27	4	A	0%	NONE	fencing	NO		good	suppressed	AS
1287	VO	Q. lobata	21	4	A	0%	NONE	fencing	NO		good	suppressed	AS
1290	VO	Q. lobata	21	4	A	0%	NONE	fencing	NO		good	suppressed	AS
1201	BO	Q. doug.	44	2	R	100%	GR	NONE	NO		fair	past failure, stressed	AS
1206	BO	Q. doug.	15	1	A	0%	NONE	fencing	NO		poor	past failures, cracks	AS
1200	BO	Q. doug.	19	4	A	0%	NONE	fencing	NO		good	leaning	AS
1205	BO	Q. doug.	31	4	I	5%	GR	F,RP,M	YES		good	past failures	AS
1204	BO	Q. doug.	26	2	R	100%	GR	NONE	NO		fair	cavity, pst failures	AS
1202	BO	Q. doug.	35	3	A	0%	NONE	fencing	NO		good	cavities, stressed	AS
1203	BO	Q. doug.	25	4	A	0%	NONE	fencing	NO		good	cavity	AS
1208	BO	Q. doug.	23	4	I	20%	GR	F,RP,M	YES		good	past failures	AS
1207	BO	Q. doug.	21	3	R	100%	GR	NONE	NO		fair	past failures, cavity, mist.	AS
1194	BO	Q. doug.	28	4	I	15%	GR	F,RP,M	YES		good	cavity	AS
1195	BO	Q. doug.	11	5	A	0%	NONE	fencing	NO		good		AS
1192	BO	Q. doug.	18	5	A	0%	NONE	fencing	NO		good		AS
1193	BO	Q. doug.	37	4	I	15%	GR	F,RP,M	YES		excel.	nesting holes	AS

8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING.
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PERSCRIBED PRUNING: CLASS 1-4
 12= AESTHETIC VALUE
 12 = FIELD NOTES
 13= NORTH SOUTH/ EAST WEST CANOPY SPREAD

#2 = TREE # MOSTLY CLOCKWISE FROM DUE NORTH
 = TREE TYPE, COMMON NAME (E.W.O.= WHITE OAK)
 3= SCIENTIFIC NAME
 4 = TRUNK DIAMETER @ 4"
 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE

TREE PROTECTION SPREAD SHEET GOLDEN HILL 12

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST. STATUS	CRZ % IMPACT	CONST. IMPACT	MITIGATION PROPOSAL	MONT. REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS/EW
1196	BO	Q. doug.	19	5	A	0%	NONE	fencing	NO		good	mistletoe	AS
1197	BO	Q. doug.	7	3	A	0%	NONE	fencing	NO		fair	suppressed	AS
1198	BO	Q. doug.	10	5	A	0%	NONE	fencing	NO		good		AS
1200	BO	Q. doug.	22	2	A	0%	NONE	fencing	NO		poor	caonks, past failure	AS
1209	BO	Q. doug.	19	2	A	0%	NONE	fencing	NO		poor	stressed	AS
1211	BO	Q. doug.	30	2	A	0%	NONE	fencing	NO		fair	past failure, cavity, mist.	AS
1212	BO	Q. doug.	14	5	A	0%	NONE	fencing	NO		good		AS
1189	BO	Q. doug.	16	4	A	0%	NONE	fencing	NO		good	mistletoe, suppressed	AS
1190	BO	Q. doug.	22	5	A	0%	NONE	fencing	NO		good		AS
1191	BO	Q. doug.	22	4	A	0%	NONE	fencing	NO		fair	cavity	AS
1188	BO	Q. doug.	19	3	A	0%	NONE	fencing	NO		fair	cavity	AS
1179	BO	Q. doug.	28	2	A	0%	NONE	fencing	NO		good	cavity at crotch	AS
1178	BO	Q. doug.	25	4	A	0%	NONE	fencing	NO		good	cavity, mistletoe	AS
1180	BO	Q. doug.	26	6	I	5%	GR	F,RP,M	YES	II	excel.	mistletoe	AS
1181	BO	Q. doug.	23	5	R	100%	GR	NONE	NO		excel.		AS
1183	BO	Q. doug.	19	5	A	0%	NONE	fencing	NO		excel.		AS
1184	BO	Q. doug.	5	3	A	0%	NONE	fencing	NO		poor	suppressed	AS
1185	BO	Q. doug.	18	4	A	0%	NONE	fencing	NO		good	suppressed	AS
1182	BO	Q. doug.	19	5	A	0%	NONE	fencing	NO		good		AS
1162	BO	Q. doug.	19	5	A	0%	NONE	fencing	NO		good		AS

1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 2 = TREE TYPE: COMMON NAME (E.W. O.= WHITE OAK)
 3= SCIENTIFIC NAME
 4 = TRUNK DIAMETER @ 4'
 5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TREENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING.
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PRESCRIBED PRUNING: CLASS 1-4
 12= AESTHETIC VALUE
 13= FIELD NOTES
 13= NORTH SOUTH/EAST WEST CANOPY SPREAD

TREE PROTECTION SPREAD SHEET GOLDEN HILL 13

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	GRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1186	BO	Q. doug.	18	2	R	100%	GR	NONE	NO		poor	crown decay	AS
1187	BO	Q. doug.	27	5	A	0%	NONE	fencing	NO	II	excel.		AS
1170	BO	Q. doug.	22	5	A	0%	NONE	fencing	NO	II	excel.		AS
1161	BO	Q. doug.	7	5	A	0%	NONE	fencing	NO		excel.		AS
1160	BO	Q. doug.	22	3	I	10%	GR	F,R,P,M	YES		fair	nesting holes, mistletoe	AS
1159	BO	Q. doug.	28	4	A	0%	NONE	fencing	NO		good	cavities	AS
1217	BO	Q. doug.	25	5	A	0%	NONE	fencing	NO	II	good	epicormic growth	AS
1156	BO	Q. doug.	23	6	I	5%	GR	F,R,P,M	YES	II	excel.		AS
1154	BO	Q. doug.	27	3	A	0%	NONE	fencing	NO	II	good	nesting holes	AS
1153	BO	Q. doug.	32	3	A	0%	NONE	fencing	NO	II	good	cavity, past failures	AS
1218	BO	Q. doug.	22	0	R	100%	GR	NONE	NO		dead	failed this year	AS
1151	BO	Q. doug.	26	6	A	0%	NONE	fencing	NO	II	excel.		AS
1297	BO	Q. doug.	41	5	A	0%	NONE	fencing	NO	II	excel.	excessive end weight	AS
1109	BO	Q. doug.	26	5	A	0%	NONE	fencing	NO	II	excel.		AS
1108	BO	Q. doug.	25	5	A	0%	NONE	fencing	NO	II	excel.	mistletoe	AS
1101	BO	Q. doug.	35	5	A	0%	NONE	fencing	NO	II	excel.	mistletoe	AS
1150	BO	Q. doug.	23	4	A	0%	NONE	fencing	NO	II	good	mistletoe	AS
1095	BO	Q. doug.	18	3	A	0%	NONE	fencing	NO	II	poor	very stressed	AS
1152	BO	Q. doug.	31	3	A	0%	NONE	fencing	NO	II	fair	mistletoe, past failure	AS
1135	BO	Q. doug.	21	4	A	0%	NONE	fencing	NO	II	fair	mistletoe, stressed	AS

8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING.
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PRESCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/ EAST WEST CANOPY SPREAD

#2 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 = TREE TYPE: COMMON NAME (E.W.O. = WHITE OAK)
 = SCIENTIFIC NAME
 = TRUNK DIAMETER @ 46"
 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE

TREE PROTECTION SPREAD SHEET GOLDEN HILL 14

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS
1155	BO	Q. doug.	21	4	A	0%	NONE	fencing	NO	II	fair	deadwood	AS
1329	BO	Q. doug.	32	2	R	100%	GR	NONE	NO		poor	major limb loss, cavity	AS
1328	BO	Q. doug.	36	1	R	100%	GR	NONE	NO		poor	major limb loss, cavity	AS
1320	BO	Q. doug.	46	3	A	0%	NONE	fencing	NO		fair	short tree	AS
1319	BO	Q. doug.	36	2	A	0%	NONE	fencing	NO		poor	major limb loss, cavity	AS
1318	BO	Q. doug.	32	1	A	0%	NONE	fencing	NO	II	poor	major limb loss, cavity	AS
1327	BO	Q. doug.	45	1	R	100%	GR	NONE	NO		poor	cavities, previous failures	AS
1321	BO	Q. doug.	15	4	A	0%	NONE	fencing	NO		fair	small tree, good structure	AS
1093	BO	Q. doug.	30	4	A	0%	NONE	fencing	NO	II	fair	limb loss, stressed	AS
1092	BO	Q. doug.	30	4	A	0%	NONE	fencing	NO	II	good	good structure	AS
1134	BO	Q. doug.	23	4	A	0%	NONE	fencing	NO	II	good	good structure	AS
1144	BO	Q. doug.	26	5	A	0%	NONE	fencing	NO		excel.	good structure	AS
1145	BO	Q. doug.	18	4	A	0%	NONE	fencing	NO		fair	twig dieback	AS
1143	BO	Q. doug.	12	3	A	0%	NONE	fencing	NO		fair	barbed wire @ base	AS
1136	BO	Q. doug.	23	5	A	0%	NONE	fencing	NO	II	excel.	good canopy	AS
1137	BO	Q. doug.	18	4	I	5%	GR	F,R,P,M	YES	II	good	mistletoe	AS

1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 2 = TREE TYPE: COMMON NAME IE W.O. = WHITE OAK
 3 = SCIENTIFIC NAME
 4 = TRUNK DIAMETER @ 4'6"
 5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING,
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PERSCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/ EAST WEST CANOPY SPREAD

TREE PROTECTION SPREAD SHEET GOLDEN HILL 15

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1138	BO	Q. doug.	25	3	I	5%	GR	F,RP,M	YES	II	fair	nesting holes, past failures	AS
1139	BO	Q. doug.	18	3	I	5%	GR	F,RP,M	YES	II	fair	mistletoe, suppressed	AS
1140	BO	Q. doug.	30	3	I	5%	GR	F,RP,M	YES	II	good	major past failure	AS
1141	BO	Q. doug.	22	3	A	0%	NONE	fencing	NO		fair	suppressed, wire@base	AS
1142	BO	Q. doug.	12	3	A	0%	NONE	fencing	NO		good	wire @base	AS
1153	BO	Q. doug.	22	5	A	0%	NONE	fencing	NO	II	excel.	good structure	AS
1333	VO	Q. lobata	37	4	A	0%	NONE	fencing	NO		good	v crotch	AS
1334	VO	Q. lobata	15	4	A	0%	NONE	fencing	NO		fair	very suppressed	AS
1335	VO	Q. lobata	10	3	A	0%	NONE	fencing	NO		fair	very suppressed	AS
1336	VO	Q. lobata	18	5	A	0%	NONE	fencing	NO		good	good structure	AS
1337	VO	Q. lobata	24	4	A	0%	NONE	fencing	NO		good	good structure	AS
1338	VO	Q. lobata	9	4	A	0%	NONE	fencing	NO		fair	deadwood	AS
1339	VO	Q. lobata	19	4	A	0%	NONE	fencing	NO		good	suppressed	AS
1340	VO	Q. lobata	27	4	A	0%	NONE	fencing	NO		good	deadwood	AS
1341	VO	Q. lobata	12	3	A	0%	NONE	fencing	NO		good	deadwood, some failures	AS
1342	VO	Q. lobata	21	4	A	0%	NONE	fencing	NO		fair	very suppressed	AS
1343	VO	Q. lobata	19	3	A	0%	NONE	fencing	NO		good	good structure	AS
1344	VO	Q. lobata	13	3	A	0%	NONE	fencing	NO		fair	stressed	AS
1345	VO	Q. lobata	59	3	A	0%	NONE	fencing	NO		fair	stressed	AS
1346	VO	Q. lobata	22	5	A	0%	NONE	fencing	NO		good	past failures	AS
1346	VO	Q. lobata	22	5	A	0%	NONE	fencing	NO		good	deadwood	AS

#1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 #2 = TREE TYPE; COMMON NAME IE W.O. = WHITE OAK
 #3 = SCIENTIFIC NAME
 #4 = TRUNK DIAMETER @ 46"
 #5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 #6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 #7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 #8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 #9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING.
 #10 = ARBORIST MONITORING REQUIRED: YES/NO
 #11 = PRESCRIBED PRUNING: CLASS 1-4
 #12 = AESTHETIC VALUE
 #13 = FIELD NOTES
 #14 = NORTH SOUTH/ EAST WEST CANOPY SPREAD

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TREE PROTECTION SPREAD SHEET GOLDEN HILL 16

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1347	VO	Q. lobata	8	3	A	0%	NONE	fencing	NO		fair	very suppressed	AS
1348	VO	Q. lobata	24	4	A	0%	NONE	fencing	NO		good	deadwood	AS
1349	VO	Q. lobata	20	3	A	0%	NONE	fencing	NO		fair	major deadwood	AS
1350	VO	Q. lobata	28	4	I	20%	FILL	F,M	YES		good	deadwood	AS
1352	VO	Q. lobata	40	4	I	15%	GR	F,RP,M	YES		excel.	some failures	AS
1351	VO	Q. lobata	28	4	I	20%	GR	F,RP,M	YES		good	deadwood	AS
1380	VO	Q. lobata	21	5	A	0%	NONE	fencing	NO		good	deadwood	AS
1096	BO	Q. doug.	23	3	R	100%	GR	NONE	NO		fair	deadwood	AS
1053a	BO	Q. doug.	26	0	R	100%	GR	NONE	NO		poor	dead stump	AS

1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 2 = TREE TYPE: COMMON NAME (E,W,O.= WHITE OAK)
 3 = SCIENTIFIC NAME
 4 = TRUNK DIAMETER @ 4'
 5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING,
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PRESCRIBED PRUNING CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/ EAST WEST CANOPY SPREAD

12/23/2008

TREE PROTECTION SPREADSHEET
GOLDEN HILL 17

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1353	BO	Q. doug.	28	6	I	10%	GR	F,RP,M	YES		good	good structure	AS
1354	VO	Q. lobata	31	5	A	0%	NONE	fencing	NO		good	some suppression	AS
1355	BO	Q. doug.	22	4	A	0%	NONE	fencing	NO		good		AS
1356	VO	Q. lobata	18	4	A	0%	NONE	fencing	NO		good	minor suppression	AS
1357	BO	Q. doug.	20	3	A	0%	NONE	fencing	NO		good	suppressed	AS
1358	BO	Q. doug.	25	4	A	0%	NONE	fencing	NO		good		AS
1359	BO	Q. doug.	19	4	A	0%	NONE	fencing	NO		good		AS
1360	VO	Q. lobata	31	1	A	0%	NONE	fencing	NO		poor	failures	AS
1361	BO	Q. doug.	20	1	A	0%	NONE	fencing	NO		fair	very suppressed	AS
1362	VO	Q. lobata	42	1	A	0%	NONE	fencing	NO		fair	failures	AS
1363	BO	Q. doug.	9	1	A	0%	NONE	fencing	NO		fair	very suppressed	AS
1364	VO	Q. lobata	58	4	A	0%	NONE	fencing	NO		good	major deadwood	AS
1365	VO	Q. lobata	12	1	A	0%	NONE	fencing	NO		poor	suppressed	AS
1366	VO	Q. lobata	34	3	A	0%	NONE	fencing	NO		fair	suppressed and lean	AS
1367	BO	Q. doug.	13	3	A	0%	NONE	fencing	NO		fair	suppressed	AS
1368	BO	Q. doug.	27	5	I	5%	GR	F,RP,M	YES	II	good	good structure	AS
1369	BO	Q. doug.	23	3	I	5%	GR	F,RP,M	YES	II	fair	poor shoot growth	AS
1370	BO	Q. doug.	37	3	A	0%	NONE	fencing	NO	II	good	limb loss	AS
1371	BO	Q. doug.	21	3	A	0%	NONE	fencing	NO		fair		AS
1372	BO	Q. doug.	8	2	A	0%	NONE	fencing	NO		poor	suppressed	AS

8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING.
10 = ARBORIST MONITORING REQUIRED: YES/NO
11 = PRESCRIBED PRUNING, CLASS 1-4
12 = AESTHETIC VALUE
12 = FIELD NOTES
13 = NORTH SOUTH/EAST WEST CANOPY SPREAD

#2 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
#3 = TREE TYPE: COMMON NAME (E.W.O. = WHITE OAK)
#4 = SCIENTIFIC NAME
#5 = TRUNK DIAMETER @ 4'6"
#6 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
#7 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
#8 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE

TREE PROTECTION SPREADSHEET GOLDEN HILL 18

1	2	3	4	5	6	7	8	9	10	11	12	13	14
TREE #	TREE SPECIES	SCIENTIFIC NAME	TRUNK DBH	TREE CONDITION	CONST STATUS	CRZ % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	AESTH. VALUE	FIELD NOTES	NS EW
1373	BO	Q. doug.	40	4	A	0%	NONE	fencing	NO	II	poor	limb loss, cankers	AS
1374	BO	Q. doug.	25	3	A	0%	NONE	fencing	NO	II	poor	suppressed, stressed	AS
1375	VO	Q. lobata	34	3	A	0%	NONE	fencing	NO	II	poor	cankers, nesting holes	AS
1376	BO	Q. doug.	16	4	A	0%	NONE	fencing	NO		good		AS
1377	BO	Q. doug.	8	3	A	0%	NONE	fencing	NO		poor	very suppressed	AS
1378	BO	Q. doug.	22	4	A	0%	NONE	fencing	NO		good	40 degree lean	AS
1379	BO	Q. doug.	16	3	A	0%	NONE	fencing	NO		good	suppressed	AS
1380	BO	Q. doug.	40	2	A	0%	NONE	fencing	NO		poor	massive limb loss	AS
1381	BO	Q. doug.	33	3	A	0%	NONE	fencing	NO		good	past failures	AS
1382	BO	Q. doug.	30	3	A	0%	NONE	fencing	NO		good	past failures	AS
1383	BO	Q. doug.	28	5	A	0%	NONE	fencing	NO		good	good structure	AS
1220	BO	Q. doug.	33	4	A	0%	NONE	fencing	NO		good	treehouse in tree	AS

1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
 2 = TREE TYPE: COMMON NAME (E.W.O. = WHITE OAK)
 3 = SCIENTIFIC NAME
 4 = TRUNK DIAMETER @ 45"
 5 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
 6 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
 7 = CRZ: PERCENT OF IMPACTED CRITICAL ROOT ZONE
 8 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
 9 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTPRUNING,
 10 = ARBORIST MONITORING REQUIRED: YES/NO
 11 = PERSCRIBED PRUNING: CLASS 1-4
 12 = AESTHETIC VALUE
 13 = FIELD NOTES
 14 = NORTH SOUTH/ EAST WEST CANOPY SPREAD

PROOF OF PUBLICATION

LEGAL NEWSPAPER NOTICES

PLANNING COMMISSION/CITY COUNCIL PROJECT NOTICING

Newspaper: Tribune

Date of Publication: February 5, 2009

Project: Notice of Intent to Adopt a Mitigated Negative Declaration and the associated General Plan Amendment, Rezone, Planned Development, Conditional Use Permit and Lot Line Adjustment. (Paso Robles Motorcoach)

I, Lonnie Dolan, employee of the Community Development Department, Planning Division, of the City of El Paso de Robles, do hereby certify that this notice is a true copy of a published legal newspaper notice for the above named project.

Signed: [Signature]
Lonnie Dolan

forms/newsaffi.691

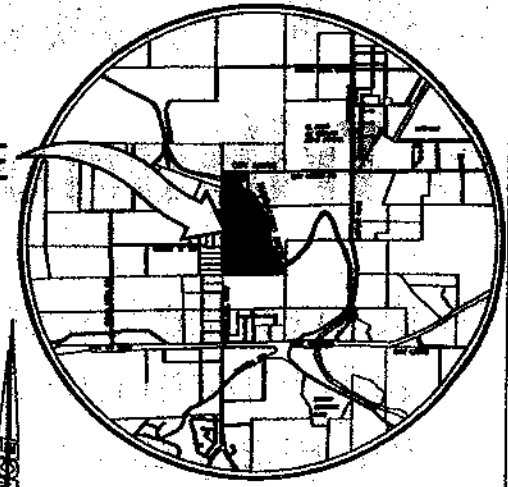
CITY OF EL PASO DE ROBLES
NOTICE OF PUBLIC HEARING
NOTICE OF INTENT TO ADOPT A MITIGATED
NEGATIVE DECLARATION

NOTICE IS HEREBY GIVEN that the City Council of the City of El Paso de Robles will hold a Public Hearing on Tuesday, March 17, 2009, at 7:30 p.m. at the City of El Paso de Robles, 1000 Spring Street, Paso Robles, California, in the City Council Chambers, to consider adoption of a General Plan Amendment, Rezone, Planned Development, Conditional Use Permit and Lot Line Adjustment and the associated Mitigated Negative Declaration in accordance with the provisions of the California Environmental Quality Act (CEQA) for the following project:

The Paso Robles Motorcoach project is proposed to be located at the northern end of Golden Hill Road, see attached location map. The Paso Robles Motorcoach project consists of the following applications:

- General Plan Amendment 09-001: a request to amend the land use designation from Agriculture (AG) to Parks & Open Space (POS) for Parcel 2.
- Rezone 09-001: a request to change the zoning district from Residential Agriculture (RA) to Parks & Open Space (POS), for Parcel 2, and to change the zoning district for Parcels 1 and 3, from Residential Agriculture (RA) to Agriculture (AG).
- Planned Development 08-001 & Conditional Use Permit 08-001: a request to establish a 332-space Recreational Vehicle (RV) Resort on Parcel 2.
- Lot Line Adjustment PR 07-0293: Request to adjust the property lines of the existing three parcels to correspond with the proposed project.

SITE



VICINITY MAP
NO SCALE

Vicinity Map
GPA 09-001, RZ09-001,
PD 08-001, CUP 08-001, LLA 07-0293
(Paso Robles Motorcoach)

The public review period for the Mitigated Negative Declaration (MND) is February 9, 2009 through March 17, 2009. The proposed MND may be reviewed at the Community Development Department, 1000 Spring Street, Paso Robles, California, on the City's website at www.prcity.com. Copies may be purchased for the cost of reproduction.

Written comments on the proposed General Plan Amendment, Rezone, Planned Development, Conditional Use Permit, Lot Line Adjustment and corresponding MND may be mailed to the Community Development Department, 1000 Spring Street, Paso Robles, CA 93446, provided that the comments are received prior to the time of the public hearing. Oral comments may be made at the hearing. Should you have any questions regarding this application, please call Darren Nash at (805) 237-3870.

If you challenge this application in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the City Council at or prior to the public hearing.

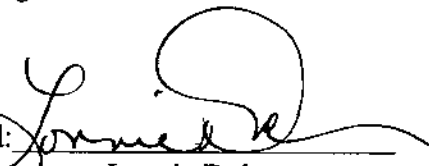
Darren Nash
Associate Planner
February 5, 2009
Agenda Item #2 Page 179 of 180
6796790

**AFFIDAVIT
OF MAIL NOTICES**

PLANNING COMMISSION/CITY COUNCIL PROJECT NOTICING

I, Lonnie Dolan, employee of the City of El Paso de Robles, California, do hereby certify that the mail notices have been processed as required for General Plan Amendment 09-001, Rezone, 09-001, Planned Development 08-001, Conditional Use Permit 08-001, Lot Line Adjustment PRAL 07-0293 (PR Motorcoach Resort) on this 20th day of February 2009.

City of El Paso de Robles
Community Development Department
Planning Division

Signed: 
Lonnie Dolan

forms\mailaffi.691